



**CITY OF CRYSTAL LAKE**  
**AGENDA**  
**CITY COUNCIL**  
**REGULAR MEETING**  
City of Crystal Lake  
100 West Woodstock Street, Crystal Lake, IL  
City Council Chambers  
September 7, 2021  
7:00 p.m.

1. **Call to Order**
2. **Roll Call**
3. **Pledge of Allegiance**
4. **Approval of Minutes – August 17, 2021 Regular City Council Meeting**
5. **Accounts Payable**
6. **Public Presentation**  
*The public is invited to make an issue oriented comment on any matter of public concern not otherwise on the agenda. The public comment may be no longer than 5 minutes in duration. Interrogation of the City staff, Mayor or City Council will not be allowed at this time, nor will any comment from the Council. Personal invectives against City staff or elected officials are not permitted.*
7. **Mayor's Report**
8. **City Council Reports**
9. **Consent Agenda**
  - a. **Coronavirus Local Fiscal Recovery Fund Conflict of Interest Policy**
  - b. **Class 16 Temporary Liquor License Request – Knights of Columbus Fr. McCormick Council #3880, Parish Music Festival, St. Thomas the Apostle Church, 451 West Terra Cotta Avenue, September 11, 2021**
  - c. **Class 16 Temporary Liquor License Request – Knights of Columbus Council #10678, St. Elizabeth Ann Seton Church, 1023 McHenry Avenue, October 9, 2021**
10. **60 Kent Avenue, Alvarez – Annexation Public Hearing and Rezoning upon annexation to R-2 Single Family Residential**
11. **Quarry Cable Park Events and Live Music Consideration**
12. **Adoption of the Crystal Lake and Pingree Road Stations Strategic Action Plan**
13. **Bid Award – Sewer Lining Services**
14. **Bid Award – 2021 Sidewalk Cutting Program**
15. **Bid Rejection and Award – Water & Wastewater Laboratory Services**
16. **Bid Rejections and Award – HVAC Duct Cleaning**
17. **Proposal Award – Consultant Selection for Preliminary Design Engineering Services for new Deep Well 20 and modifications to Water Treatment Plant 4**

- 18. Proposal Award – Consultant Selection for Preliminary Engineering Services for the Re-establish Crystal Creek Project**
- 19. Intergovernmental Funding Agreement for the Pingree Train Station Non-Structural Facility Repairs**
- 20. Board and Commission Appointment – Sustainability Committee**
- 21. Council Inquiries and Requests**
- 22. Adjourn to Executive Session for the purpose of discussing matters of pending and probable litigation, the sale, purchase or lease of real property, collective bargaining and personnel**
- 23. Reconvene to Regular Session**
- 24. Adjourn**

*If special assistance is needed in order to participate in a City of Crystal Lake public meeting, please contact Melanie Nebel, Executive Assistant, at 815-459-2020, at least 24 hours prior to the meeting, if possible, to make arrangements.*



**Agenda Item No: 9a**

**City Council  
Agenda Supplement**

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<b>Meeting Date:</b>	September 7, 2021
<b>Item:</b>	Coronavirus Local Fiscal Recovery Fund Conflict of Interest Policy
<b>Staff Recommendation:</b>	A motion adopting an Ordinance pertaining to the Coronavirus Local Fiscal Recovery Fund
<b>Staff Contact:</b>	Jodie Hartman, Director of Finance

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**Background:**

On March 11, 2021, President Biden signed the American Rescue Plan Act or 2021 (ARPA) into law. This stimulus package provides for \$1.9 trillion in relief relating to the Coronavirus Pandemic. Of the \$1.9 trillion, \$130.2 billion is slated for the Local Fiscal Recovery Fund, the first stimulus package to provide direct funds to local governments throughout the country. The City of Crystal Lake will receive a per capita distribution through the State of Illinois later this year for the first half of the allocation, with the second distribution one year later.

In order to receive Local Fiscal Recovery Fund awards available under the American Rescue Plan Act (ARPA), units of local government are required to complete and file with the Illinois Department of Commerce and Economic Opportunity (DCEO) a “U.S. Department of the Treasury Coronavirus Local Fiscal Recovery Fund Award Terms and Condition” form (Terms and Conditions), as well an “Assurance of Compliance with Civic Rights Requirements”. The required documents must be filed by September 30, 2021 in order to claim the funds.

The Illinois Municipal League (IML) has recommended adoption of an ordinance authorizing the filing of these documents and adopting the federal conflict of interest provisions applied by the U.S. Treasury to the ARPA funds. Legal review concurs with formally adopting the conflict of interest components of the federal Terms and Conditions. The City maintains a comprehensive conflict of interest policy, however, the City’s policy language does not mirror exactly that of the federal policy. According to the Terms and Conditions document, the City “must maintain a conflict of interest policy consistent with 2 CFR §200.318 and that such conflict of interest policy is applicable to each activity funded with the ARPA moneys”. In the interest of protecting the City and ensuring full compliance, the following ordinance has been recommended per the recommendation of IML and the City’s legal counsel.

**Votes Required to Pass:**

Simple majority

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**CITY OF CRYSTAL LAKE**

**ORDINANCE NO. \_\_\_\_\_**

**AN ORDINANCE PERTAINING TO THE CORONAVIRUS  
LOCAL FISCAL RECOVERY FUND**

**WHEREAS**, the City of Crystal Lake in McHenry County, Illinois, (the “*City*”) is an Illinois municipality that is eligible for funds through the Coronavirus Local Fiscal Recovery Fund through the U.S. Department of the Treasury (the “*Treasury*”) pursuant to Section 9901 of the American Rescue Plan Act of 2021, P.L. 117-2 (“*ARPA*”); and

**WHEREAS**, as a local government recipient of financial support through ARPA, the City is required to utilize the financial support received from Treasury for the specific purposes and in compliance with the terms and conditions required by ARPA and Treasury regulations implementing ARPA; and

**WHEREAS**, Treasury requires the recipients of Coronavirus Local Fiscal Recovery Funds maintain conflict of interest policies consistent with 2 C.F.R. § 200.318(c) (the “*Federal Conflict of Interest Rules*”); and

**WHEREAS**, although the City has conflict of interest policies in place that reflect the Federal Conflict of Interest Rules, the City Council of the City has determined that it is advisable, necessary, and in the best interest of the City to adopt expressly the Federal Conflict of Interest Rules with respect to the use of the Coronavirus Local Fiscal Recovery Funds; and

**WHEREAS**, in order to participate in and receive the funding pursuant to ARPA, the City is required to execute and deliver the “Coronavirus Local Fiscal Recovery Fund Award Terms and Conditions” (the “*Terms and Conditions*”), the Assurances of Compliance with Civil



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Rights Requirements (the “*Assurances*”), and various other documents as set forth in the website [[NEU Portal \(illinois.gov\)](#)] of the Illinois Department of Commerce and Economic Opportunity (collectively, the “*Required Documentation*”), and the City Council desires to authorize and ratify, as appropriate, the execution and delivery of such Required Documentation;

NOW, THEREFORE, BE IT ORDAINED, BY THE MAYOR AND CITY COUNCIL OF THE CITY OF CRYSTAL LAKE, MCHENRY COUNTY, STATE OF ILLINOIS as follows:

SECTION 1: Recitals. The foregoing recitals shall be and are hereby incorporated as findings of fact as if said recitals were fully set forth herein.

SECTION 2: Approval of Execution and Delivery of Required Documentation. The City Council hereby approves the execution and delivery on behalf of the City the Required Documentation (including without limitation the Terms and Conditions and Assurances) in substantially the forms provided by the Illinois Department of Commerce and Economic Opportunity; any action heretofore taken in conformity with this approval is hereby ratified. The City Council further authorizes (and ratifies any prior action of the following persons consistent with this authorization) the Mayor, City Clerk, Deputy City Clerk, City Manager, or Finance Director of the City (or any of them individually or in combination) to execute the Required Documentation on behalf of the City.

SECTION 3: Adoption of Conflict of Interest Policy in Connection with the Use of Coronavirus Local Fiscal Recovery Funds. The City Council hereby adopts the following conflict of interest provisions consistent with the Federal Conflict of Interest Rules that shall apply to all activities and expenditures funded through the Coronavirus Local Fiscal Recovery Fund Award:

1. No officer, official, employee, or agent of the City may participate in the selection, award, or administration of a contract supported by a Federal award if he or she has a real or apparent conflict of interest. Such a conflict of interest would arise when the

**DRAFT**

- officer, official, employee, or agent, any member of his or her immediate family, his or her partner, or an organization which employs or is about to employ any of the parties indicated herein, has a financial or other interest in or a tangible personal benefit from a firm considered for a contract.
2. The officers, officials, employees, or agents of the City may neither solicit nor accept gratuities, favors, or anything of monetary value from contractors or parties to subcontracts. However, nominal interests (as reflected in the statutory exceptions set forth in 50 ILCS 105/3 or 65 ILCS 5/3.1-55-10) or the gift is an unsolicited item of nominal value, consistent with the State Officials and Employees Ethics Act, 5 ILCS 430/1 *et seq.*, shall not be a violation of this policy.
  3. The violation of these standards of conduct may result in disciplinary action for violations of such standards by officers, officials, employees, or agents of the City in accordance with applicable law, including the policies, employment contracts, contracts for services or collective bargaining agreements of the City.
  4. The City shall award contracts only to responsible contractors possessing the ability to perform successfully under the terms and conditions of a proposed procurement. Consideration will be given to such matters as contractor integrity, compliance with public policy, record of past performance, and financial and technical resources.
  5. The City shall disclose in writing to Treasury any potential conflict of interest affecting the awarded funds in accordance with 2 C.F.R. § 200.112.

SECTION 4: Severability. If any provision of this Ordinance or application thereof to any person or circumstances is ruled unconstitutional or otherwise invalid, such invalidity shall not affect other provisions or applications of this Ordinance that can be given effect without the invalid application or provision, and each invalid provision or invalid application of this Ordinance is severable.

Section 5: Effective Date. This Ordinance shall be in full force and effect upon its passage and approval in accordance with applicable law.

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PASSED THIS \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_\_.

AYES: \_\_\_\_\_

NAYS: \_\_\_\_\_

ABSENT: \_\_\_\_\_

APPROVED THIS \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_\_.

\_\_\_\_\_  
Mayor Haig Haleblian

ATTEST:

\_\_\_\_\_  
City Clerk Nick Kachiroubas



**Agenda Item No: 9b**

**City Council  
Agenda Supplement**

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**Meeting Date:**

September 7, 2021

**Item:**

Class 16 Temporary Liquor License Request –  
Knights of Columbus Fr. McCormick Council #3880

**Staff Recommendation:**

Motion to approve issuance of a Class 16 Temporary  
Liquor License to the Knights of Columbus Fr. McCormick  
Council #3880

**Staff Contact:**

Eric T. Helm, Deputy City Manager

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**Background:**

The City has received a request from the Knights of Columbus Fr. McCormick Council #3880 for the issuance of a Class 16 Temporary Liquor License in order to sell beer and wine at their Parish Music Festival being held at the St. Thomas the Apostle Church Parking Lot at the corner of Oak Street and Route 176, located at 451 West Terra Cotta Avenue, on Saturday, September 11, 2021 from 4:30 p.m. to 9:00 p.m.

Section 329-5-P of the City Code permits the issuance of a Class 16 Temporary Liquor License for the retail sale of beer and wine for consumption upon the premises specified in the license where sold. The license shall be issued to not-for-profit corporations qualified to do business in the State of Illinois. The license shall be for a period not to exceed three (3) days, and shall be issued only for special events sponsored by the not-for-profit corporation requesting the license.

The Council approved this event in 2019. A Certificate of Insurance naming the City of Crystal Lake as an additional insured must be submitted as a condition for the issuance of the Temporary Liquor License.

**Votes Required to Pass:**

Simple majority



**Agenda Item No: 9c**

**City Council  
Agenda Supplement**

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**Meeting Date:**

September 7, 2021

**Item:**

Class 16 Temporary Liquor License Request –  
Knights of Columbus Council #10678

**Staff Recommendation:**

Motion to approve issuance of a Class 16 Temporary  
Liquor License to the Knights of Columbus Council  
#10678

**Staff Contact:**

Eric T. Helm, Deputy City Manager

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**Background:**

The City has received a request from the Knights of Columbus Council #10678 for the issuance of a Class 16 Temporary Liquor License in order to sell beer and wine at their Oktoberfest event being held at the St. Elizabeth Ann Seton Church Community Center, located at 1023 McHenry Avenue, on Saturday, October 9, 2021 from 5:00 p.m. to 11:00 p.m.

Section 329-5-P of the City Code permits the issuance of a Class 16 Temporary Liquor License for the retail sale of beer and wine for consumption upon the premises specified in the license where sold. The license shall be issued to not-for-profit corporations qualified to do business in the State of Illinois. The license shall be for a period not to exceed three (3) days, and shall be issued only for special events sponsored by the not-for-profit corporation requesting the license.

The Knights of Columbus have met all application requirements and the Council has approved this event in the past.

**Votes Required to Pass:**

Simple majority



**Agenda Item No: 10**

**City Council  
Agenda Supplement**

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**Meeting Date:** September 7, 2021

**ANNEXATION PUBLIC HEARING**

**Item:** REPORT OF THE PLANNING & ZONING COMMISSION

**Request:** Rezoning upon annexation to R-2 Single-Family Residential

**Petitioner:** Jorge Alvarez  
60 Kent Ave

**Recommendation:** Motion to adopt the annexation Ordinance and approve the Planning and Zoning Commission (PZC) recommendation and adopt an Ordinance for rezoning upon annexation to the R-2 Single-Family Residential District at 60 Kent Avenue.

**Staff Contact:** Kathryn Cowlin, Director of Community Development

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**Background:**

- The property is currently zoned R-1 Single-Family Residential in McHenry County. This property is part of a pocket of unincorporated property that is wholly surrounded by city limits. The adjacent properties within the city are zoned R-2 Single-Family Residential, which is the requested zoning district.
- The petitioner has future plans to construct a single-family home on the property and wishes to connect to city utilities. To connect to city utilities, the property must be annexed.
- Request: The petitioner is requesting to rezone the property upon annexation to R-2 Single-Family Residential.

**PZC Highlights:**

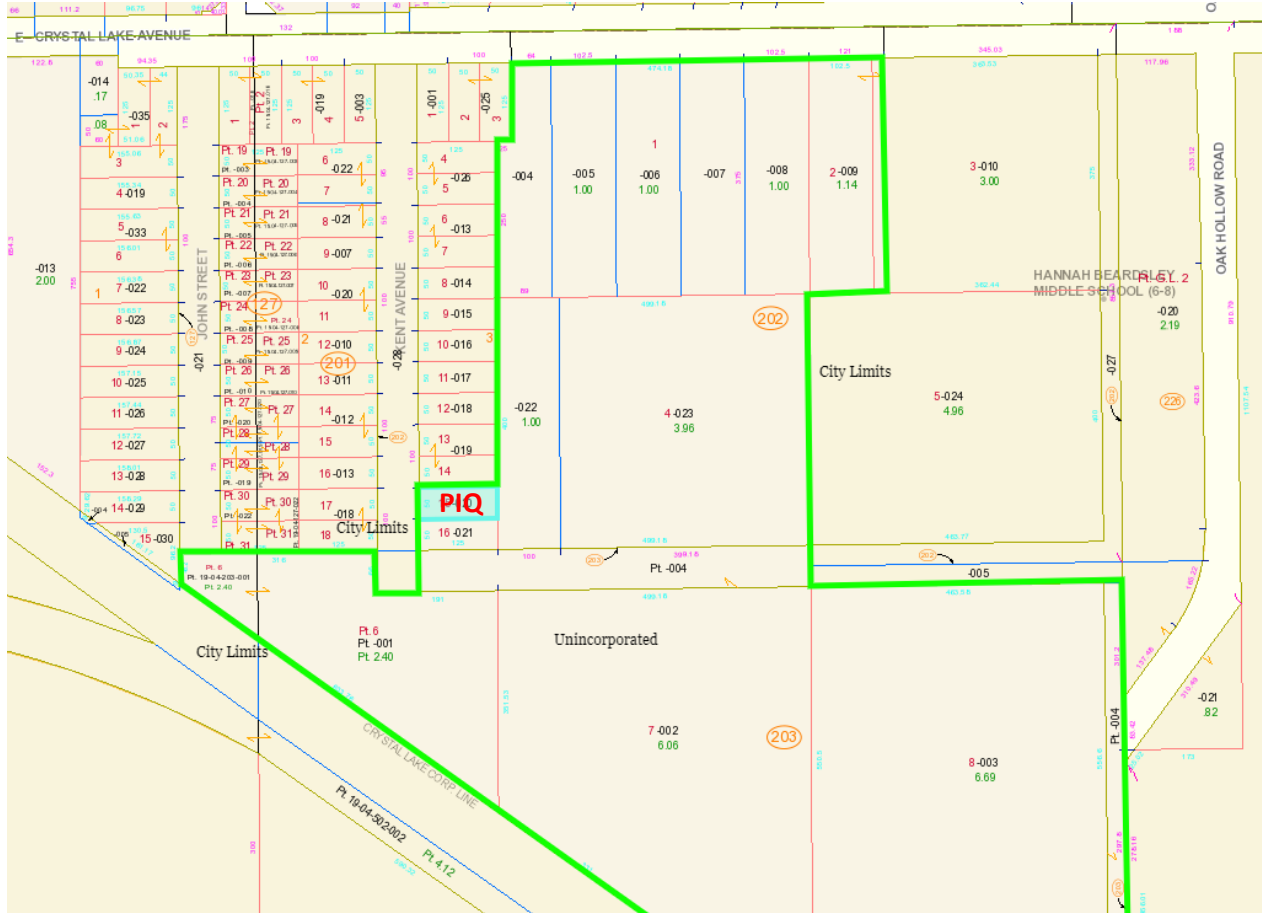
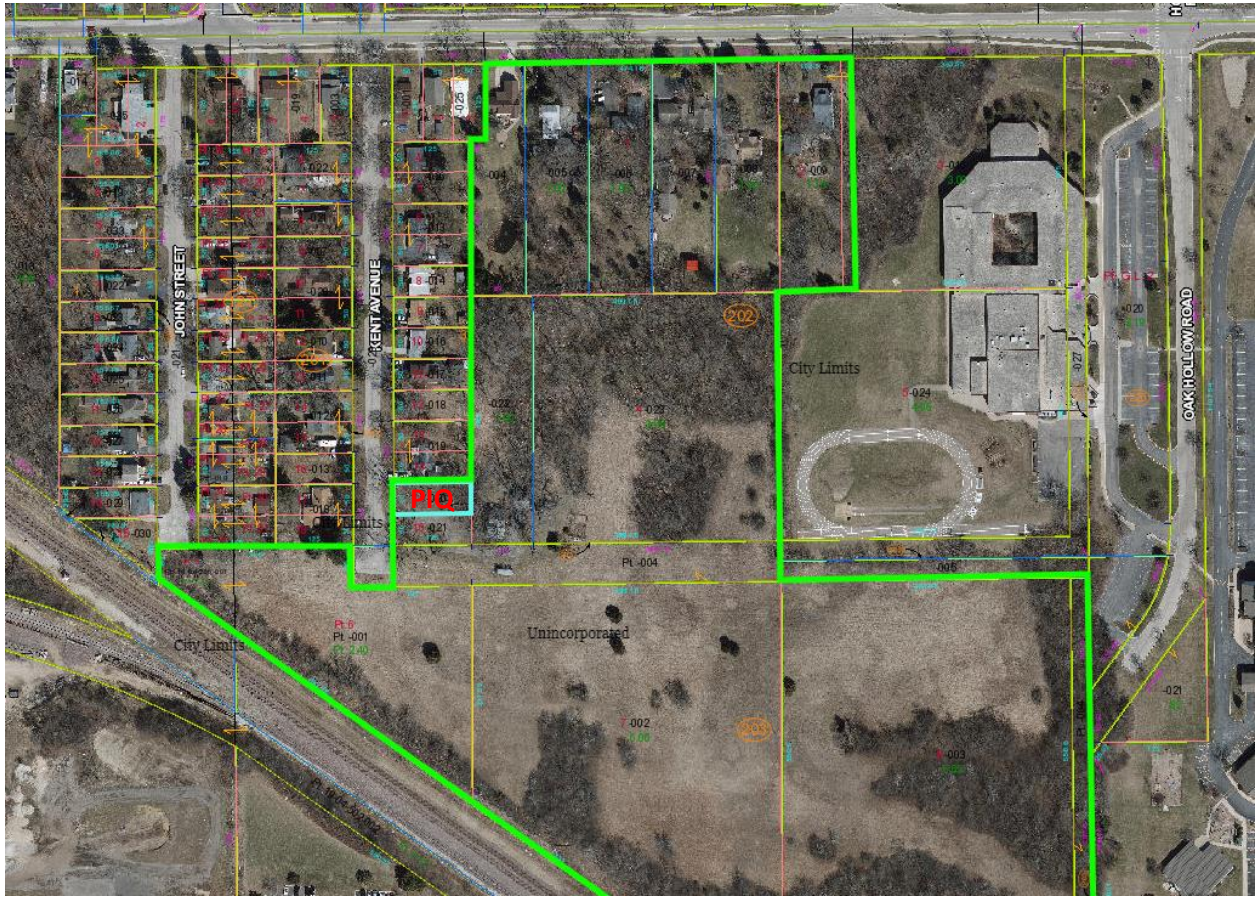
- The petitioner reviewed the request to rezone the property to R-2 Single-Family Residential upon annexation.
- The PZC stated that the petition met the Findings of Fact.

The PZC recommended **approval (6-0)** of the petitioner's request.

**Votes Required to Pass:** A simple majority



60 Kent Ave – Annexation  
PIQ Map





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Ord. No. xxxx  
File No. xxx



**The City of Crystal Lake Illinois**

**AN ORDINANCE ANNEXING CERTAIN TERRITORY TO  
THE CITY OF CRYSTAL LAKE, McHENRY COUNTY, ILLINOIS**

WHEREAS, a written Petition, signed by the legal owners of all land within the territory hereinafter described, has been filed with the City Clerk of the City of Crystal Lake, McHenry County, Illinois, requesting that said territory be annexed to the City of Crystal Lake; and

WHEREAS, the said territory is not within the corporate limits of any municipality but is contiguous to the City of Crystal Lake; and

WHEREAS, said territory is not part of any public library district; and

WHEREAS, legal notices regarding the intention of the City to annex said territory have been sent to all public bodies required to receive such notice by State statute; and

WHEREAS, all Petitions, documents and other necessary legal requirements are in full compliance with the requirements of the Statutes of the State of Illinois, specifically 65 ILCS 5/7-1-8; and

WHEREAS, annexation of the territory by the City is also authorized under, and is in compliance with the requirements of, Section 7-1-10 of the Illinois Municipal Code, 65ILCS 5/7-1-10; and

**DRAFT**

Ord. No. xxxx  
File No. xxx

WHEREAS, it is in the best interests of the City of Crystal Lake that said territory be annexed thereto.

NOW, THEREFORE, BE IT ORDAINED BY THE MAYOR AND CITY COUNCIL OF THE CITY OF CRYSTAL LAKE, McHENRY COUNTY, ILLINOIS, as follows:

SECTION I: The foregoing recitals are hereby incorporated into and made a part of this Ordinance as if fully set forth.

SECTION II: That the territory legally described in Exhibit “A” attached hereto and made a part hereof, be and the same is hereby annexed to the City of Crystal Lake, McHenry County, Illinois.

SECTION III: That the City Clerk of the City of Crystal Lake is hereby directed to record in the Office of the Recorder of Deeds of McHenry County, Illinois, being the County in which the aforesaid annexed territory is situated, a certified copy of this Ordinance together with an accurate map of the territory hereby annexed, said map being attached hereto and made a part hereof and labeled Exhibit "B". Additionally, the City Clerk is authorized and directed to cause a certified copy of this Ordinance, together with an accurate map to be filed in the Office of the County Clerk of McHenry County, Illinois.

SECTION IV: That this Ordinance shall be in full force and effect from and after its passage and approval.

AYES:

NAYS:

ABSENT:

PASSED this 7th day of September, 2021.

APPROVED by me this 7th day of September, 2021.

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Ord. No. xxxx  
File No. xxx

City of Crystal Lake, an  
Illinois municipal corporation

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Haig Haleblian, Mayor

SEAL

ATTEST:

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Nick Kachiroubas, City Clerk

Passed: September 7, 2021  
Approved: September 7, 2021

**DRAFT**

Ord. No. xxxx  
File No. xxx

**CERTIFICATION**

I, NICK KACHIROUBAS, do hereby certify that I am the duly elected, authorized and acting City Clerk of the City of Crystal Lake, County of McHenry, and State of Illinois, and that as such City Clerk I am the keeper of the records and minutes of the proceedings of the Mayor and Councilmembers to the said City.

I do hereby certify that the following "AN ORDINANCE ANNEXING CERTAIN TERRITORY TO THE CITY OF CRYSTAL LAKE, McHENRY COUNTY, ILLINOIS" is a true and correct copy of Ordinance Number \_\_\_\_\_ duly passed and approved at a regular meeting of said Mayor and Council; held on the 4th day of February 2020, at which time \_\_\_ councilmembers were present and \_\_\_ councilmembers were absent.

Motion was made by Councilmember \_\_\_\_\_ and seconded by Councilmember \_\_\_\_\_ that the following Ordinance be passed and approved. Upon roll-call vote, \_\_\_ councilmembers voted AYE and \_\_\_\_\_ councilmembers voted NAY; whereupon said Ordinance was declared duly passed and was thereupon approved by the Mayor.

\_\_\_\_\_  
CITY CLERK

(SEAL)

Prepared by:

City of Crystal Lake  
Community Development  
100 W. Woodstock Street  
Crystal Lake, IL 60014

After recording, return to:

City of Crystal Lake  
Community Development  
100 W. Woodstock Street  
Crystal Lake, IL 60014

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File No. xxx

**EXHIBIT "A"**  
**LEGAL DESCRIPTION**

LOT 15 IN BLOCK 3 OF CRYSTAL HEIGHTS SUBDIVISION, A SUBDIVISION IN SECTION 4, TOWNSHIP 43 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED JANUARY 16, 1928, AS DOCUMENT NO. 81577, IN BOOK 6 OF PLATS, PAGE 14, IN MCHENRY COUNTY, ILLINOIS.

COMMONLY KNOWN AS 60 KENT AVEUNE, CRYSTAL LAKE, ILLINOIS 60014. PIN 19-04-202-020

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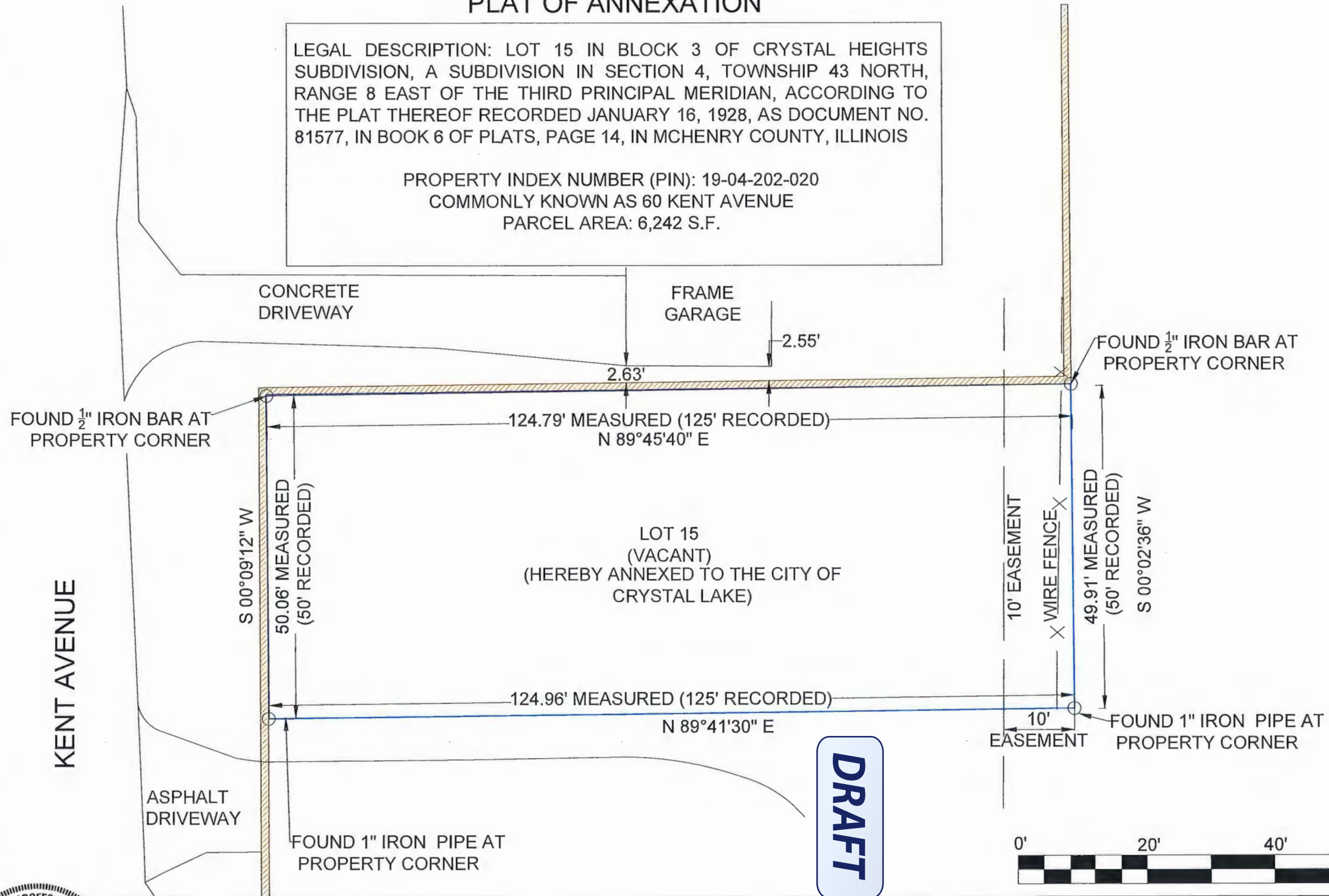
Ord. No. xxxx  
File No. xxx

EXHIBIT "B"  
PLAT OF ANNEXATION

# PLAT OF ANNEXATION

LEGAL DESCRIPTION: LOT 15 IN BLOCK 3 OF CRYSTAL HEIGHTS SUBDIVISION, A SUBDIVISION IN SECTION 4, TOWNSHIP 43 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED JANUARY 16, 1928, AS DOCUMENT NO. 81577, IN BOOK 6 OF PLATS, PAGE 14, IN MCHENRY COUNTY, ILLINOIS

PROPERTY INDEX NUMBER (PIN): 19-04-202-020  
COMMONLY KNOWN AS 60 KENT AVENUE  
PARCEL AREA: 6,242 S.F.



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APPROVED:  
*[Signature]*  
(REGISTERED PROFESSIONAL ENGINEER)  
DATED:  
8/30/21



LEGEND	
	NEW BOUNDARY LINE
	EXISTING CITY OF CRYSTAL LAKE CORPORATE LIMITS

PREPARED BY: City of Crystal Lake Community Development 100 West Woodstock St Crystal Lake, Illinois 60014	
DRAWN BY:	KL
CHECKED BY:	KC
COMPLETION DATE:	8/17/2021

City of  
**Crystal Lake**  
Illinois

SHEET 1 OF 1

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Ord. No. xxxx  
File No. xxx



**The City of Crystal Lake Illinois**

**AN ORDINANCE ZONING CERTAIN PROPERTY**  
**“R-2” SINGLE-FAMILY RESIDENTIAL DISTRICT**

WHEREAS, Jorge Alvarez (hereinafter, the “Owner”) is the owner of property legally described in Exhibit A, attached hereto (hereinafter, the “Subject Property”); and

WHEREAS, pursuant to an application filed by the Owner, the Planning and Zoning Commission of the City of Crystal Lake, pursuant to notice duly published on August 2, 2021 in the Northwest Herald, held a public hearing at 7:00 p.m., on August 18, 2021 at City Hall at 100 W. Woodstock Street, Crystal Lake, Illinois to consider the proposed zoning of the Subject Property to “R-2” Single-Family Residential District; and

WHEREAS, on August 18, 2021, the Planning and Zoning Commission, having fully heard and considered the testimony of all those present at the public hearing who wished to testify, made findings of fact as required by law and recommended to the Mayor and City Council of the City of Crystal Lake that the zoning of the Property be changed to “R-2” Single-Family Residential District, all as more specifically set forth in that certain Report of the Planning and Zoning Commission in Case #PLN-2021-00130; and

WHEREAS, the City Council of the City of Crystal Lake, pursuant to notice duly published on August 24, 2021 in the Northwest Herald, held a public hearing at 7:00 p.m., on September 7,



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File No. xxx

2021 at City Hall at 100 W. Woodstock Street, Crystal Lake, Illinois to consider the proposed annexation of the subject property; and

WHEREAS, said territory has been duly annexed by ordinance to the City of Crystal Lake; and

WHEREAS, said territory is to be zoned “R-2” Single-Family Residential District zoning; and

WHEREAS, it is in the best interests of the City of Crystal Lake that the property legally described herein below be classified and zoned as indicated.

NOW THEREFORE, BE IT ORDAINED BY THE MAYOR AND CITY COUNCIL OF THE CITY OF CRYSTAL LAKE, McHENRY COUNTY, ILLINOIS, as follows:

SECTION I: Recitals: The foregoing recitals are hereby incorporated by reference as though fully set forth herein.

SECTION II: That the Subject Property is hereby zoned and classified “R-2” Single-Family Residential District.

SECTION III: That the City Clerk is hereby directed to amend the official zoning map of the City of Crystal Lake and all pertinent records of the City of Crystal Lake to show the zoning classification of the above-described property in accordance with the provisions of this Ordinance, as provided by law.

SECTION IV: That this Ordinance shall be in full force and effect from and after its passage, approval and publication as provide by law.

**DRAFT**

Ord. No. xxxx  
File No. xxx

AYES:

NAYS:

ABSENT:

PASSED this 7th day of September, 2021.

APPROVED by me this 7th day of September, 2021.

City of Crystal Lake, an  
Illinois municipal corporation

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Haig Haleblian, Mayor

SEAL

ATTEST:

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Nick Kachiroubas, City Clerk

Passed: September 7, 2021  
Approved: September 7, 2021

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Ord. No. xxxx  
File No. xxx

**EXHIBIT A**

LOT 15 IN BLOCK 3 OF CRYSTAL HEIGHTS SUBDIVISION, A SUBDIVISION IN SECTION 4, TOWNSHIP 43 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED JANUARY 16, 1928, AS DOCUMENT NO. 81577, IN BOOK 6 OF PLATS, PAGE 14, IN MCHENRY COUNTY, ILLINOIS.

COMMONLY KNOWN AS 60 KENT AVEUNE, CRYSTAL LAKE, ILLINOIS 60014. PIN 19-04-202-020



**#2021-130**  
**60 Kent Avenue – Rezoning/Annexation**  
**Project Review for Planning and Zoning Commission**

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<b><u>Meeting Date:</u></b>	August 18, 2021
<b><u>Request:</u></b>	Rezoning upon annexation to R-2 Single-Family Residential.
<b><u>Location:</u></b>	60 Kent Avenue
<b><u>Acreage:</u></b>	Approximately 6,250 square feet (0.15 acres)
<b><u>Existing Zoning:</u></b>	R-1 Single-Family Residential (McHenry County)
<b><u>Requested Zoning:</u></b>	R-2 – Single-Family Residential
<b><u>Surrounding Properties:</u></b>	North: R-2 – Single-Family Residential South: A-1 – Agriculture (McHenry County) East: A-1 – Agriculture (McHenry County) West: R-2 – Single-Family Residential
<b><u>Staff Contact:</u></b>	Katie Cowlin (815.356.3798)

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**Background:**

- **Existing Use:** The subject property is a vacant lot.
- The lot is part of the Crystal Heights Subdivision and is considered a legal non-conforming lot due to lot area and width.

**Development Analysis:**

**General:**

- **Request:** The petitioner is requesting to rezone the property upon annexation to R-2 Single-Family Residential.
- **Land Use:** The Comprehensive Land Use map shows the area as Urban Residential which is an appropriate land use designation.
- **Zoning:** The property is currently zoned R-1 Single-Family Residential in McHenry County. This property is part of a pocket of unincorporated property that is wholly surrounded by city limits. The adjacent properties within the city are zoned R-2 Single-Family Residential, which is being requested.

- The petitioner has future plans to construct a single-family home on the property and wishes to connect to city utilities. To connect to city utilities, the property had to be annexed.

**Comprehensive Land Use Plan 2030 Vision Summary Review:**

The Comprehensive Plan designates the subject property as Urban Residential, which allows for existing and future single-family residential uses. The following goal is applicable to this request:

Land Use - Residential

**Goal: Encourage a diversity of high quality housing in appropriate locations throughout the city that supports a variety of lifestyles and invigorates community character.**

This can be accomplished with the following supporting action:

**Supporting Action:** Preserve and enhance the character and livability of existing residential area with architectural and development guidelines. Promote safe, clean and well-maintained housing by encouraging regular repair and maintenance of housing.

**Findings of Fact:**

**REZONING**

- The property is currently zoned R-1 Single-Family Residential in McHenry County. Property annexed into the City of Crystal Lake comes in as E- Estate zoning.
- The property would be rezoned to the City's R-2 – Single-Family Residential zoning district upon annexation.

Criteria for Rezoning:

(a) The existing uses and zoning of nearby property.

*Meets*                       *Does not meet*

(b) The extent to which property values are diminished by a particular zoning classification or restriction.

*Meets*                       *Does not meet*

(c) The extent to which the destruction of property value of a petitioner property owner promotes the health, safety, morals or general welfare of the public.

*Meets*                       *Does not meet*

(d) The relative gain to the public as opposed to the hardship imposed on a petitioning property owner.

*Meets*                       *Does not meet*

(e) The suitability of the subject property for its zoned purposes.

*Meets*                       *Does not meet*

(f) The length of time the property has been vacant as zoned, considered in the context of land development in the area.

*Meets*                       *Does not meet*

(g) The Comprehensive Plan designation and the current applicability of that designation.

*Meets*                       *Does not meet*

(h) The evidence or lack of evidence, of community need for the use proposed.

*Meets*                       *Does not meet*

**Recommended Conditions:**

If a motion to recommend approval of the petitioner's request is made, the following conditions are recommended:

1. Approved plans, reflecting staff and advisory board recommendations, as approved by the City Council:
  - A. Application (Alvarez, dated 07/21/2021)
  - B. Plat of Survey (Luco Surveying, dated 04/16/2018)
  
2. The petitioner shall address all of the review comments and requirements of the Fire Rescue and Community Development Departments.



**City of Crystal Lake  
Development Application  
Ownership Sign-off Acknowledgement Form**

The following information is related to a development application. As the owner of the property in question, I (we) acknowledge that the information provided in the submittal was reviewed and approved.

Owner Information

Name: Jorge Alvarez

Address: 3511 E. Crystal Lake Ave,  
Crystal Lake, IL 60014

Phone: [REDACTED]

E-mail: [REDACTED]

Project Name & Description: Annexation and Re-Zoning of 60 Kent Ave,  
Lot 15, PIN: 19-04-202-020, Into Crystal Lake,  
(currently unincorporated)

Project Address/Location: 60 Kent Ave. Crystal Lake, IL 60014

Signature

[Handwritten Signature]  
Jorge Alvarez

7-21-21

Owner: Print and Sign name

Date

NOTE: If the property is held in a trust, the trust officer must sign this petition as owner. In addition, the trust officer must provide a letter that names all beneficiaries of the trust.



**CRYSTAL LAKE PLANNING AND ZONING COMMISSION  
WEDNESDAY, AUGUST 18, 2021  
HELD AT THE CRYSTAL LAKE CITY COUNCIL CHAMBERS**

The meeting was called to order by Vice Chairman Esposito at 7:00 p.m.

Mr. Esposito called the meeting to order. On roll call, members Atkinson, Esposito, Gronow, Jouron, Repholz and Skluzacek were present. Chairman Greenman was absent.

Kathryn Cowlin, Director of Community Development was present from Staff.

Mr. Esposito said this meeting is being recorded for broadcast and future playback on the City's cable channel. He led the group in the Pledge of Allegiance.

**APPROVE MINUTES OF THE AUGUST 4, 2021 PLANNING AND ZONING COMMISSION MEETING**

Mr. Jouron moved to approve the minutes from the August 4, 2021 regular Planning and Zoning Commission meeting as presented. Mr. Atkinson seconded the motion. On roll call, members Atkinson, Esposito, Gronow, Jouron, Skluzacek and Repholz voted aye. Motion passed.

**2021-130 60 KENT AVENUE – REZONING UPON ANNEXATION** – PUBLIC HEARING

Rezoning upon annexation to R-2 Single-Family Residential.

Jorge Alvarez, petitioner, was present to represent the request.

Mr. Alvarez shared he purchased the vacant lot and would like to annex to the city so he can connect to city utilities when he builds a house on the property. The rezoning to R-2 Single-Family would allow him to build a home.

The Vice Chairman opened the public hearing. There was no one in the public wishing to speak on the agenda item. The public hearing was closed.

Mr. Skluzacek asked whose home was behind the lot. Mr. Alvarez stated that is owned by the school district and is vacant. It was there when the school district bought all the vacant land to the south and east.

Mr. Esposito stated the request meets the findings of fact.

Mr. Gronow made a motion to approve the Rezoning upon annexation to R-2 Single-Family Residential at 60 Kent Avenue.



Mr. Skluzacek seconded the motion. On roll call, members Atkinson, Esposito, Gronow, Jouron, Repholz and Skluzacek voted aye. Motion passed 6-0.

**REPORT FROM PLANNING**

Ms. Cowlin reviewed the items that were on the previous City Council meeting agenda and the items that are scheduled for the upcoming PZC meeting.

**COMMENTS FROM THE COMMISSION**

Mr. Esposito asked that the TOD Plan be shared with the HPC.

Mr. Esposito thanked the Commissioners for being good to work with over the years. Mr. Jouron thanked Mr. Esposito for his years of service and for always making meetings easier by being a great person.

Mr. Jouron made a motion to adjourn the meeting. Mr. Skluzacek seconded the motion. On voice vote, all members voted aye. The meeting was adjourned 7:08 p.m.

**PUBLIC NOTICES**

Court at the McHenry County Government Center, 2200 North Seminary Avenue, Woodstock, Illinois, 60098, or with the representative, or both.

Copies of claims filed with the Clerk must be mailed or delivered to the representative and to his attorney within ten days after it has been filed.

Katherine M. Keefe  
Clerk of the Circuit Court

(Published in Northwest Herald August 24, 31, September 7, 2021) 1913157

**PUBLIC NOTICE**

STATE OF ILLINOIS )  
COUNTY OF MCHENRY)SS  
BEFORE THE CORPORATE  
AUTHORITIES OF  
THE CITY OF CRYSTAL LAKE  
MCHENRY COUNTY,  
ILLINOIS

IN THE MATTER OF THE  
PETITION OF  
JORGE ALVAREZ FOR  
APPROVAL OF AN INITIAL  
ZONING CLASSIFICATION  
FOR A PARCEL OF LAND TO  
BE ANNEXED TO THE CITY  
OF CRYSTAL LAKE, ILLINOIS

**NOTICE OF PUBLIC  
HEARING ON PETITION FOR  
THE ANNEXATION OF  
CERTAIN PROPERTY INTO  
THE CITY OF  
CRYSTAL LAKE, MCHENRY  
COUNTY, ILLINOIS**

PLEASE TAKE NOTICE THAT, pursuant to 65 ILCS 5/11-15.1-1 et seq., the Corporate Authorities of the City of Crystal Lake, Illinois will conduct a public hearing on a proposed annexation relating to the territory described below on September 7, 2021 at 7:00 p.m. at the Crystal Lake City Hall, 100 West Woodstock Street, Crystal Lake, Illinois. Following such hearing, said Corporate Authorities may consider action on such annexation and, pursuant to Section 7-1-8 of the Illinois Municipal Code, 65 ILCS 5/7-1-8, the annexation of the territory described below.

Said annexation consists of the parcels totaling approximately 0.15 acres and legally described as follows:

LOT 15 IN BLOCK 3 OF CRYSTAL HEIGHTS SUBDIVISION, A SUBDIVISION IN

**PUBLIC NOTICES**

SECTION 4, TOWNSHIP 43 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED JANUARY 16, 1928, AS DOCUMENT NO. 81577, IN BOOK 6 OF PLATS, PAGE 14, IN MCHENRY COUNTY, ILLINOIS.

COMMONLY KNOWN AS 60 KENT AVEUNE, CRYSTAL LAKE, ILLINOIS 60014. PIN 19-04-202-020

The Crystal Lake Rural Fire Protection provides fire protection. If the territory described above is annexed, fire protection and library services will be provided to such territory by the City of Crystal Lake.

Anyone with questions or seeking additional information regarding the annexation should contact Planning & Economic Development at 815-356-3605 or [planning@crystallake.org](mailto:planning@crystallake.org). All persons interested in commenting on the proposed annexation shall have the right to be heard at the time herein prescribed. Such hearing may be continued to another date without further notice.

Nick Kachiroubas,  
City Clerk,  
City of Crystal Lake

(Published in the Northwest Herald on August 24, 2021) 1912384

**PUBLIC NOTICE**

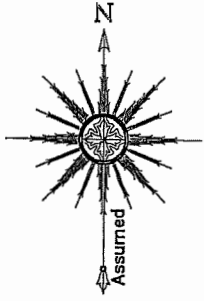
**Notice of Intent to  
Dispose of Abandoned  
and/or Unclaimed Property**  
The following amount must be received at Doc's River Road Storage, at 2121 S. River Rd., McHenry, IL 60051 by August 31, 2021, or the contents of storage units and/or property will be disposed of:

Lyle Hillman, 172 Hollywood, Waterloo, IA 50701; 24' Pontoon boat, \$210.00

Tom Cooper, 155 Captain Hooke, Lakemoor, IL 60051; Paving Equipment, \$600.00

Paul Heyden II, 6600 W Devon #2S Chicago, IL 60631; Fishing boat, \$180.00

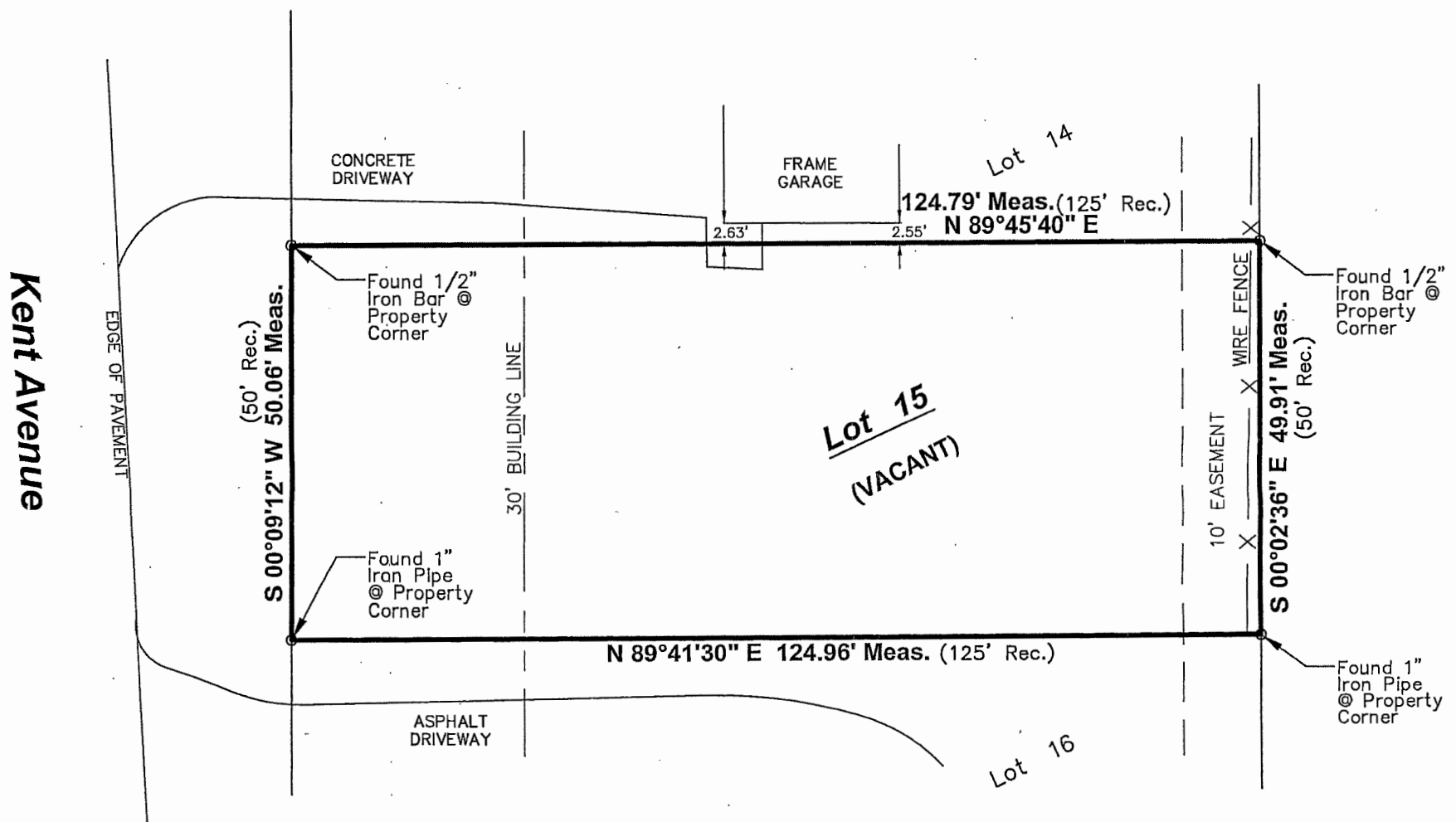
(Published in the Northwest Herald August 17, 24, 2021) 1911086



# Plat of Survey

LEGAL DESCRIPTION: LOT 15 IN BLOCK 3 OF CRYSTAL HEIGHTS SUBDIVISION, A SUBDIVISION OF PART OF LOT 2 OF THE NORTHEAST 1/4 AND LOT 2 OF THE NORTHWEST 1/4 OF SECTION 4, TOWNSHIP 43 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED JANUARY 16, 1928, AS DOCUMENT NO. 81577, IN BOOK 6 OF PLATS, PAGE 14, IN MCHENRY COUNTY, ILLINOIS.

PARCEL AREA: 6,242 S.F.



THIS DOCUMENT IS NOT VALID WITHOUT THE SURVEYOR'S SEAL.

STATE OF ILLINOIS } s.s.  
COUNTY OF MCHENRY }

IN MY PROFESSIONAL OPINION, AND BASED ON MY OBSERVATIONS, I HEREBY CERTIFY THAT THE PLAT HEREON DRAWN IS A TRUE REPRESENTATION OF SAID SURVEY. THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

DATED THIS 17th DAY OF April, A.D. 2018

*Jeffrey A. Spirek*  
JEFFREY A. SPIREK  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3898  
REGISTERED AGENT FOR LUCO CONSTRUCTION COMPANY  
LICENSE TO BE RENEWED NOVEMBER 30, 2018

NOTES:  
\*COMPARE THE DESCRIPTION ON THIS PLAT WITH YOUR DEED AND REPORT ANY DIFFERENCE, IMMEDIATELY.  
\*ALL DIMENSIONS ARE GIVEN IN FEET AND DECIMAL PARTS THEREOF.  
\*BEARINGS AS SHOWN ARE ASSUMED AND ARE SO INDICATED FOR INTERIOR ANGLES ONLY.  
\*BUILDING LINES AND EASEMENTS AS SHOWN ARE BASED SOLELY ON PROVIDED DOCUMENTS.  
\*UTILITIES WITHIN EASEMENTS ARE NOT SHOWN HEREON. UNDERGROUND UTILITIES, INCLUDING, BUT NOT LIMITED TO CONDUITS AND CABLES (IF ANY) HAVE NOT BEEN SHOWN HEREON.

LEGEND	
●	Set IP
○	Fnd IP
×	Fnd "X"
⊗	Fnd P.K.
□-□-□	Wood Fence
×-×-×	CL Fence
▒	Concrete
▨	Wood Deck
▩	Brick
☆	STREET LIGHT
⊙	B-BOX
⊕	WATER VALVE VAULT
⊗	FIRE HYDRANT
⊘	MANHOLE
⊙	STORM MANHOLE
⊕	JUNCTION BOX
⊖	POWER POLE
⊕	ELECTRIC TRANSFORMER



**Luco Surveying**  
Professional Design Firm No. 184.004829  
54 Lou Avenue, Crystal Lake, IL 60014  
Phone: 815.526.3974 Email: lucoland@gmail.com

Common Address: 60 Kent Avenue, Crystal Lake, IL  
PIN # 19-04-202-020

Job Number: 18 - 45508 Drawn By: JAC

Client: Gottomoller Reference: Shields

Field Work Completion: 04-16-2018

Revisions:



## Agenda Item No: 11

### City Council Agenda Supplement

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<b>Meeting Date:</b>	September 7, 2021
<b>Item:</b>	Quarry Cable Park Events and Live Music Consideration
<b>Staff Recommendation:</b>	Motion to approve the Special Event application from the Quarry Cable Park for use of the Three Oaks Recreation Area for an outdoor music event on Saturday, September 25, 2021 or Saturday, October 9, 2021 from 5:00 p.m. to 11:00 p.m. and the Merry Market event on Friday and Saturday, November 5 and 6, 2021.
<b>Staff Contact:</b>	Eric Helm, Deputy City Manager Nick Hammonds, Assistant to the City Manager

---

#### **Background:**

The Quarry Cable Park has requested a Special Event permit to host events at their location at the Three Oaks Recreation Area. Pursuant to section 5.08 (l) of the Concessionaire and Property Use Agreement, the Quarry Cable Park shall receive Special Event approval for all events involving “tournaments, special advertising displays, live bands, special promotions, outside vendors, night use, and/or large groups of people that could exceed the available parking spaces...The review of these events will be reviewed on a case-by-case basis.” The following special events are proposed in fall 2021.

Event	Date	New / Recurring
Country Themed Event	Saturday, September 25, or Saturday, October 9	New
Merry Market	Friday and Saturday, November 5 and 6	Recurring

All events and all related activities will be contained in the designated premises outlined in the Property Use Agreement. The Fire Rescue Department and Three Oaks Recreation Area staff have reviewed and approved the following events proposed by the Quarry Cable Park. **Capacity restrictions and social distancing will be in compliance with state and local requirements depending on the status of Covid-19 at the time of each event.**

#### Saturday, September 25 or Saturday, October 9, 2021: Outdoor Country Music Event

This new proposed event is to be held on the outdoor patio on Saturday, September 25, 2021 or Saturday, October 9, 2021 from 5:00 p.m. to 11:00 p.m. This event will include country themed food and drinks, live music, and may include a mechanical bull. The Quarry Cable Park will provide the liability insurance for the mechanical bull prior to the event. The Fire Rescue Department reviewed the specifications for the bull and found no issues. Anticipated attendance for the event will be similar to past events at the Quarry of 200 to 300 people. Since the event is proposed for late September or early October when many Three Oaks amenities have closed for the season, parking will not be an issue.

### Friday, November 5, and Saturday, November 6, 2021: Merry Market

This event is proposed for Friday, November 5, and Saturday, November 6, 2021. Previous event attendance was 130 people. This event will be similar to the events held in December 2018 and 2019, featuring artists with hand-crafted holiday gifts and accessories.

### **Staff Conditions for All Events**

City staff from the Fire Rescue and Police Departments have reviewed the request and do not have any concerns regarding the events, providing the following conditions are met:

1. Follow all masking and social distancing recommendations as required by federal, state and local authorities. The City will have discretion to cancel an event if the City determines that Covid-19 cases and restrictions warrant cancellation.
2. The Quarry Cable Park & Grille shall coordinate with the Crystal Lake Fire Rescue and Police Departments, and Three Oaks Recreation Area staff regarding assistance during each event, including coordinating with the Fire Rescue Department regarding occupancy limits.
3. Provide a layout for each event showing the locations of the various event activities.
4. Provide and maintain emergency access (boat launch) to the North Lake for rescue purposes during the entire event.
5. Provide 20-foot access lane(s) for emergency vehicles to gain access to the building, Fire Department Connection, and the event site.
6. Fire hydrants located on or near the property must remain unobstructed and “No Parking” signs posted.
7. Provide a site plan at least 30 days prior to the event for any tents/canopies, if used, to ensure compliance.
8. Any barricades used to restrict traffic or pedestrians must be easily moveable or manned by event staff should an emergency occur during the event hours.
9. Provide adequate lighting for participants during night hours.
10. If a standby ambulance is being requested, contact the Fire Department at least 30 days prior to the event to discuss scheduling and associated fees.
11. All debris created by the event shall be cleaned up during and after the event.
12. Petitioner must adhere to the required City insurance provisions.
13. In the case of inclement weather, alternate dates can be approved by the City Manager.
14. Liability insurance for the rental and use of a mechanical bull shall be provided to the City prior to the event.

The applicants have been made aware of these recommended conditions and will attend the City Council meeting to answer any questions.

### **Votes Required to Pass:**

Simple majority vote of the City Council.



## Agenda Item No: 12

### City Council Agenda Supplement

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<b><u>Meeting Date:</u></b>	September 7, 2021
<b><u>Item:</u></b>	The Crystal Lake & Pingree Road Stations Strategic Action Plan.
<b><u>Recommendation:</u></b>	Motion to adopt the Ordinance and approve the Planning and Zoning Commission (PZC) recommendation and adopt the Crystal Lake & Pingree Road Stations Strategic Action Plan.
<b><u>Staff Contact:</u></b>	Kathryn Cowlin, Director of Community Development Elizabeth Maxwell, City Planner

---

#### **Background:**

- The City applied for and was awarded a grant for Local Technical Assistance through the Regional Transportation Authority's (RTA) grant program for a Transit Oriented Development (TOD) plan. The grant pays 80% of the cost of the plan.
- Through a competitive bidding process, Houseal Lavigne with Fish Transportation Group was selected as the consultant for the project.
- On May 18<sup>th</sup>, the draft plan was presented to the City Council for review. These comments, along with the comments received from the steering committee, the public open house, online feedback, and the Planning and Zoning Commission were reviewed and changes were made to the plan.
- Request: Adopt the final plan of the Crystal Lake & Pingree Road Stations Strategic Action Plan. The Strategic Action Plan is a guidebook for future development strategies. It also establishes direction for improvements to pedestrian, bicycle and vehicle infrastructure.

#### **PZC Highlights:**

The following discussion took place during the Planning and Zoning Commission hearing:

- The PZC was supportive of the solutions and design examples presented in the plan.
- The PZC was excited for future redevelopment opportunities near the two stations.

The PZC recommended **approval (5-0)** of the plan.

**Votes Required to Pass:** A simple majority vote

**DRAFT**

Ord. No.  
File No.



**AN ORDINANCE ADOPTING THE  
CRYSTAL LAKE & PINGREE ROAD STATIONS STRATEGIC ACTION PLAN**

WHEREAS, pursuant to its home rule powers, the Illinois Municipal Code, and other applicable authority, the City is authorized to regulate land uses within the City and does so pursuant to the City’s Unified Development Ordinance, set forth as Chapter 650 of the Crystal Lake City Code; and

WHEREAS, pursuant to the Local Technical Assistance Grant process in conjunction with a consulting firm, the City has drafted a transit oriented development plan, the “Crystal Lake & Pingree Road Stations Strategic Action Plan (the “Strategic Action Plan”) which creates a vision for development and redevelopment surrounding the two Metra train stations in Crystal Lake; and

WHEREAS, the Planning and Zoning Commission of the City of Crystal Lake, pursuant to notice duly published on August 12, 2021 in the Northwest Herald, held a public hearing at 7:00 p.m., on September 1, 2021 at City Hall at 100 W. Woodstock Street, Crystal Lake, Illinois to consider the proposed Strategic Action Plan; and

WHEREAS, on September 1, 2021, the Planning and Zoning Commission, having fully heard and considered the testimony of all those present at the public hearing who wished to testify, made a recommendation to the Mayor and City Council of the City of Crystal Lake that the proposed Strategic Action Plan be approved; and

WHEREAS, the Mayor and City Council, having considered such recommendation and the minutes from the Planning and Zoning Commission, have found and determined that adopting the Strategic Action Plan, as set forth in this Ordinance will be in the best interests of the City and its residents;

NOW, THEREFORE, BE IT ORDAINED BY THE MAYOR AND CITY COUNCIL OF THE CITY OF CRYSTAL LAKE, McHENRY COUNTY, ILLINOIS, as follows:

**SECTION I: Recitals:** The foregoing recitals are repeated and incorporated as though fully set forth herein.



**DRAFT**

Ord. No.  
File

**SECTION II. Adoption of of Strategic Plan.** The Crystal Lake & Pingree Road Stations Strategic Action Plan, attached hereto is hereby adopted.

**SECTION III:** All ordinances or parts of ordinances in conflict herewith are hereby repealed.

**SECTION IV:** That the City Clerk be and is hereby directed to amend all pertinent records of the City of Crystal Lake to show the approval of the Crystal Lake & Pingree Road Stations Strategic Action Plan, in accordance with the provisions of this Ordinance, as provided by law.

**SECTION IV:** That this Ordinance shall be in full force and effect from and after its passage, approval and publication as provided by law.

DATED at Crystal Lake, Illinois, this 7<sup>th</sup> day of September, 2021.

CITY OF CRYSTAL LAKE, an  
Illinois Municipal Corporation

By: \_\_\_\_\_  
Haig Haleblian, MAYOR

SEAL

ATTEST:

\_\_\_\_\_  
Nick Kachiroubas, CITY CLERK

PASSED: September 7, 2021  
APPROVED: September 7, 2021

Published in pamphlet form by the authority of the Mayor and City Council of the City of Crystal Lake.



# CRYSTAL LAKE & PINGREE ROAD STATIONS **STRATEGIC ACTION PLAN**

Draft - 2021/8/27

**DRAFT**  
for Review



# ACKNOWLEDGMENTS

## THANKS TO THOSE WHO PARTICIPATED

A special thank you goes to everyone who participated in the planning process for the Crystal Lake & Pingree Road Stations Strategic Action Plan. This Plan was made possible by the contributions and insights of the residents, business owners, property owners, developers, representatives from various groups and organizations in the City and local and regional transportation agencies, Steering Committee members, City Council, and Planning & Zoning Commission.

### Project Funding

The Crystal Lake & Pingree Road Stations Strategic Action Plan was funded by the Regional Transportation Authority's Community Planning Program.

For more information about this program, visit <http://rtachicago.org/plans-programs/grants-projects/community-planning>

## CITY COUNCIL

- Mayor Haig Haleblan
- Council Member Ellen Brady
- Council Member Cathy Ferguson
- Council Member Brett Hopkins
- Council Member Cameron Hubbard
- Council Member Mandy Montford
- Council Member Ian Philpot

## PLANNING & ZONING COMMISSION

- Jeff Greenman, Chair
- Vince Esposito, Vice-Chair
- Bob Atkinson
- Bill Gronow
- Jim Jouron
- Kathy Repholz
- Allan Skluzacek

## STEERING COMMITTEE MEMBERS

- Alex Devlin, RTA
- Michael Horsting, RTA
- Mary Donner, PACE
- David Kralik, Metra
- Rick Mack, Metra
- Anthony Mattingly, Metra
- Scott Hennings, McHenry County Division of Transportation
- Diana Kenney, Downtown Crystal Lake
- Nate Wynsma, Lexington Homes
- Eberhard Veit, McHenry Bicycle Advocates
- Jason Herbster, Crystal Lake Park District

## CONSULTANT TEAM



With technical assistance from  
**Fish Transportation Group**







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**DRAFT**  
for Review

**DRAFT**  
for Review



CHAPTER 1  
**INTRODUCTION**

**DRAFT**  
for Review





The Crystal Lake and Pingree Road Metra stations are important economic drivers for the City of Crystal Lake. The Metra stations can be a catalyst to attract residents and economic investments into the City, and the City needs a roadmap to best utilize the Metra stations' potential. Thus, the City has embarked on a 12-month planning process to establish a Stations Strategic Action Plan. This document creates a long-term vision and policy framework for the City to implement to enhance both areas into successful, vibrant, and unique destinations to live and work. In essence, the Plan identifies where the Metra Station Areas are now, where they want to be in the future, and how should they get there.

## PURPOSE OF THE STATIONS STRATEGIC ACTION PLAN

The Crystal Lake and Pingree Road Stations Strategic Action Plan establishes a shared vision for areas surrounding the Crystal Lake and Pingree Road Metra Stations. The Plan identifies strategies to guide future development, promote a multi-modal environment, create welcoming and unique destinations, and maximize the potential of undeveloped and underutilized land. Ultimately, the Plan illustrates how the Crystal Lake and Pingree Road Station Areas should evolve over the next 10 years and provides strategies for achieving that vision.

## ORGANIZATION OF THE PLAN

The Plan is composed of the following chapters:

### CHAPTER 1: STATION AREAS SNAPSHOT

This chapter introduces the premise of the Plan and provides an overview of the planning process, planning area, community engagement, review of past plans and studies, and takeaways from an analysis on the residential, commercial, office, and industrial markets, existing land use, and current zoning at the time the plan was created.

### CHAPTER 2: VISION, GOALS, AND OBJECTIVES

This chapter consists of a forward-looking vision statement for the Crystal Lake and Pingree Road Station Areas that describes the desired outcome to be achieved through implementation of the Plan. The vision statements are supported by goals and objectives that guide implementation of the Plan to achieve the desired outcomes for both Station Areas.

### CHAPTER 3: LAND USE AND DEVELOPMENT

This chapter includes land use recommendations for both Station Areas. This chapter also provides a toolbox of urban design best practices and illustrates their applicability to development within both Station Areas.



## CHAPTER 4: TRANSPORTATION

This chapter includes a series of strategies that address transportation issues and improvements within and around both Station Areas, including vehicular, transit, and active modes of transportation. These strategies are intended to promote a multi-modal environment in both Station Areas.

## CHAPTER 5: REDEVELOPMENT OPPORTUNITY SITES

This chapter presents illustrative concepts that visualize development potential and desired intensity and character of future development for three identified redevelopment opportunity sites. These models demonstrate how Station Area land use and transportation recommendations can be addressed at the site level and communicate desired direction for future development and can be used as a marketing tool to attract development interest to the subject sites.

## PLANNING PROCESS

To adequately guide the Crystal Lake and Pingree Road Station Areas for the next 20 years, the Plan is the product of a community-driven planning process that includes engagement with residents, business owners, local officials, City staff, staff from transportation and transit agencies (Metra, Pace, McHenry Department of Transportation, and Regional Transportation Authority (RTA)), and other key stakeholders.

## TASK 1: PROJECT INITIATION AND STEERING COMMITTEE PROJECT KICK-OFF

The planning process began with meetings with key RTA and City staff, elected officials, and the Steering Committee, which consists of local realtors, transportation and transit agency staff, bicycle advocates, Crystal Lake Park District staff, and the Downtown Crystal Lake's business association. These meetings enabled staff, officials, and Steering Committee members to identify issues and opportunities and the onset of the process.

## TASK 2: ANALYSIS OF EXISTING CONDITIONS AND REVIEW PAST PLANS

This task entailed the preparation of the Existing Conditions Report (ECR) based on existing datasets, field reconnaissance, surveys, past planning efforts and initiatives, market and demographic information, and community engagement. The ECR served as the initial foundation for the development of the Plan.

### Impacts of COVID-19

The Crystal Lake and Pingree Road Stations Strategic Action Plan was developed in the midst of the COVID-19 pandemic. Public engagement was conducted primarily online in response to social distancing requirements. While the full impact of the pandemic is not known, the vision and goals contained in this plan provide direction that is responsive to an evolving community.





### **TASK 3: LAND USE AND ZONING ANALYSIS**

This task established an overall “vision” for the Crystal Lake and Pingree Road Station Areas, along with preliminary goals and policies that provide focus and direction for subsequent planning activities. A future land use and transportation framework was developed to illustrate how the vision, goals, and policies manifest themselves within both Station Areas. Preliminary massing models were also developed to illustrate the recommended development program for the redevelopment opportunity sites. This task was undertaken concurrently with Task 4: Transportation and Accessibility Framework and Task 5: Market Analysis to ensure the recommendations were supported by transportation and market findings.

### **TASK 4: TRANSPORTATION AND ACCESSIBILITY FRAMEWORK**

The Transportation and Accessibility Framework identified strategies for enhancing pedestrian and bicycle mobility while ensuring convenient access for motorists and transit-passengers. The recommendations were aimed in promoting a more multi-modal environment for both Stations Areas.

### **TASK 5: MARKET ANALYSIS**

The market analysis utilized data collected in previous tasks to identify how each Station Area function in the context of the larger region. This included an examination of the residential, retail, office, and industrial market trends for both Station Area, the City as a whole, and the larger region. These findings ensured the Plan’s recommendations are grounded in market and economic realities.

### **TASK 6: IMPLEMENTATION STRATEGY**

The implementation strategy and action framework identified actions to be undertaken, determine prioritization, establish the role of the City and various partners and stakeholders in undertaking key actions, and identify specific implementation tasks.

### **TASK 7: PUBLIC OUTREACH AND PARTICIPATION**

Community outreach and citizen participation were an important part of the planning process. Residents, business owners, and other stakeholders participated in the planning process through a variety of tools including a virtual Community Vision Event, map social, an interactive project website, online questionnaires, key person interviews, and update meetings to the City’s Planning and Zoning Commission and City Council. This task occurred throughout the planning process.

### **TASK 8: DRAFT STATIONS STRATEGIC ACTION PLAN**

This step included the preparation of the draft of the Crystal Lake and Pingree Road Stations Strategic Action Plan, which was reviewed by City staff, the steering committee, and the community through a Community Open House. The review, discussion, and public feedback would inform revisions to the revised and final draft Plan.

### **TASK 9: FINAL STATIONS STRATEGIC ACTION PLAN**

This final step included the preparation and refinement of a final Plan, along with the City’s adoption process.



CHAPTER 2  
**STATION AREAS  
SNAPSHOT**

**DRAFT**  
for Review





## REGIONAL SETTING

The City of Crystal Lake is a community with a population of approximately 39,829 (per 2019 estimates from the U.S. Census). Located in the southeast portion of McHenry County, the City is situated at the northwestern edge of the Chicago metropolitan region. Crystal Lake is the largest community in McHenry County and is generally well-connected with the rest of the region, given that it has access to the regional highways, including the east-west U.S. Highway 14 and IL Route 31, which intersects with Interstate 90 about five miles south of the City. Metra's Union Pacific Northwest (UP-NW) Line also serves the City with two Metra stations, Crystal Lake and Pingree Road, traveling to the Ogilvie Transportation Center in downtown Chicago.

Founded by two communities that were established in the 1800s, the Village of Crystal Lake was situated by Crystal Lake while the Village of Nunda was located at the present Crystal Lake Downtown area that acted as a commercial hub for the first direct rail connection from Chicago into McHenry County. Both communities consolidated into the City of Crystal Lake in 1914. Today, the City of Crystal Lake remains an important hub of commerce in McHenry County.

Downtown Crystal Lake is still a vibrant destination and business environment. Commercial uses outside of Downtown are concentrated along U.S. Highway 14 and South Virginia Rd, while light industrial, offices, and medical offices are mostly situated along Congress Parkway and by Pingree Road Metra station. The City's residential neighborhoods surrounding Downtown Crystal Lake are well-developed and more urban in nature, since they are situated within traditional grid network, and they enjoy easy access to some of the City's well-known destinations, including Veterans Acres Park, Downtown Crystal Lake, and Crystal Lake. Residential neighborhoods farther from Downtown Crystal Lake tend to be more suburban as they are usually situated within subdivisions.



## STATION AREA

The Plan provides recommendations for two identified Station Areas:

- **Crystal Lake Station Area:** Quarter-mile radius around the Crystal Lake Metra station.
- **Pingree Road Station Area:** Half-mile radius around the Pingree Road Metra station.

The Crystal Lake Station Area focuses on Downtown Crystal Lake and the surrounding residential neighborhoods. This area is generally built-out except for several parcels and blocks that are identified as potential redevelopment sites in the final section of this report. In contrast, the Pingree Road Station Area functions more as a business park with a park-and-ride commuter facility with residential subdivisions to the north and a concentration of offices, medical offices, and light industrial uses to the south. A series of vacant lots and Metra's excess property by the Pingree Road Metra station are potential development opportunities. The following sections provide greater and more in-depth analysis of both Station Areas.





## COMMUNITY ENGAGEMENT

Community outreach efforts were conducted throughout the planning process to facilitate discussion with a diverse range of community stakeholders and gain an understanding of local needs and aspirations. This included the use of virtual events and online tools that allowed community members to effectively participate and share their feedback during the COVID-19 pandemic under related social distancing requirements. Residents, business community members, property owners, developers, service providers, officials, and other groups with a vested interest participated and shared their vision for the Crystal Lake and Pingree Road Station Areas. Together, the feedback helped guide the direction of the Plan and ensured recommendations and policies are responsive to the needs of the community.

The following is a list of community outreach efforts conducted throughout the planning process (all outreach were conducted virtually, except the Update Meetings to the City's Planning & Zoning Commission and City Council):

### STEERING COMMITTEE MEETING KICK-OFF MEETING & FOLLOW-UP INTERVIEWS

A Steering Committee kick-off meeting was held on July 2020 to introduce Steering Committee members about the planning process and opened discussions about the issues and opportunities in the Crystal Lake and Pingree Road Station Areas. The consultant team also conducted follow-up interviews with each Steering Committee member. These conversations touched on a variety of topics, such as land use, residential neighborhoods, economic development, regional transportation, Metra service, and trail access.

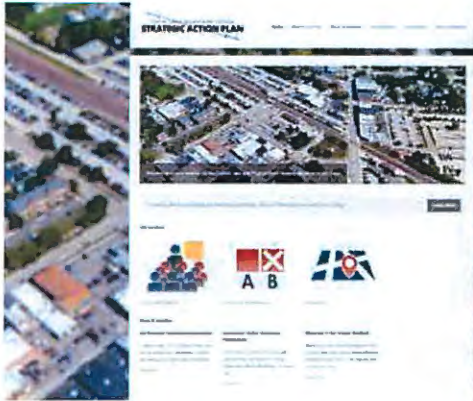
### STEERING COMMITTEE MEETING #2

In October 2020, the consultant team met with the Steering Committee members to review and discuss the Existing Conditions Report and selection of the three redevelopment opportunity sites. The discussion set the direction for the consultant team to develop preliminary land use and transportation framework and preliminary massing models to visualize site-specific land use and transportation recommendations.

### COMMUNITY VISION MEETING

In October 2020, the consultant team and the City hosted a virtual Community Vision Workshop on Zoom. The presentation introduced the Crystal Lake and Pingree Road Stations Strategic Action Plan, described what a transit-oriented development (TOD) is, explored TOD's key principles, examples, and importance to the Crystal Lake and Pingree Road Station Areas. The presentation also included a demonstration of Map.Social, which is an online platform that people can spatially map out their vision, issues, and opportunities of the Station Areas.





**STEERING COMMITTEE MEETING #3**

In March 2021, the consultant team met with the Steering Committee to review and discuss the draft vision and goals, preliminary land use and transportation framework and massing models for the selected redevelopment opportunity sites. The resulting discussion and feedback received provided directions for the development of the draft Crystal Lake & Pingree Road Stations Strategic Action Plan.

**PLANNING & ZONING COMMISSION AND CITY COUNCIL UPDATE MEETING (IN-PERSON)**

In April and May 2021, the consultant team provided an update meeting to the City's Planning & Zoning Commission and the City Council, respectively, to solicit their input regarding the Plan's land use and transportation recommendations, urban design considerations, and the redevelopment opportunity sites' massing models. The feedback received were used to further refine the Plan's recommendations.

**PROJECT WEBSITE**

A project website was developed to support the planning process and keep the public updated on the project's progress. It included background information about the Plan, project news and announcements, meeting dates, documents, and online engagement tools. The website remained active throughout the development of the Stations Strategic Action Plan and acted as a 'hub' for information and engagement opportunities.

**MAP.SOCIAL**

Map.social is an online mapping tool that allows participants to pinpoint issues and opportunities within the Station Areas on their own personalized map. It was available to the public throughout the planning process on the project website.

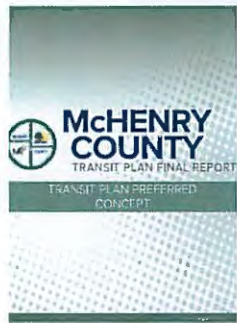
**ONLINE COMMUNITY SURVEY**

A Community Questionnaire was made available on the project website, designed to gather detailed feedback from residents and business community members in Crystal Lake.



## PAST PLANS & STUDIES

The following are the past research and studies reviewed in preparation of the Crystal Lake and Pingree Road Stations Strategic Action Plan. Plans include those adopted by the City of Crystal Lake as well as those that impact policy, planning, and development within both Station Areas. The Plan recognizes the value of past planning efforts and builds upon them as a component of the community's vision.



McHenry County  
Transit Plan (2019)



City of Crystal Lake  
Transportation  
Plan (2017)



Crystal Lake 2030  
Comprehensive  
Plan (2012)



Green Infrastructure  
Vision (2011)





## MARKET ANALYSIS

A market analysis was undertaken to examine past, current, and projected trends in the residential, commercial, office, and industrial real estate in Crystal Lake and the larger McHenry County market area. Market analysis findings were used to identify issues and opportunities in each real estate market and help inform the Plan's land use and development recommendations for the Crystal Lake and Pingree Road Station Areas.

## RESIDENTIAL

The Crystal Lake Residential Market area covers Crystal Lake and is roughly bounded by Lakewood (west), Fox River Grove (east), Ringwood (north), and Algonquin (south). This area covers where the bulk of new potential residents are likely to move from.







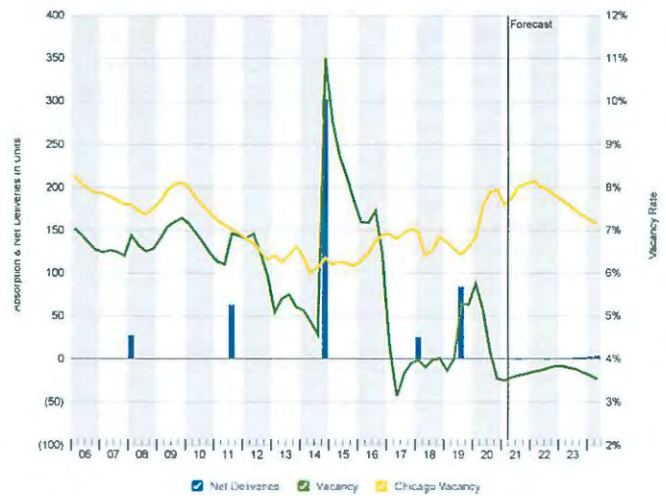
### Single-Family Attached Home Sales

Single-Family Attached Home refers to low-rise townhomes. Townhomes valued between \$100,000 and \$199,000 sold well among those attainable for most income levels, with an average of 50 days on the market. This represents the bulk (63% of total inventory) of the home sales in the market area. Townhomes valued lower than this price range spent about 70 days on the market, and townhomes at higher price ranges were in the market between 100 and 180 days. Few properties valued higher than \$350,000 (0.6% of total inventory) were sold within 25 days on the market. 1- and 2-bedroom units are the most popular type of townhomes, representing 64 percent of all townhome sales, and 3-bedrooms have the highest sell rate at 80% at an average price of \$187,000.

### Multifamily Sales (2-4 Bedroom Unit Buildings)

Based on available datasets, there were few multifamily buildings (78) relative to attached single-family homes (1,379). Because of this low sample size, there is more significant variation. Multifamily units priced between \$150,000 and \$199,000 were the most popular, representing 46 percent of all sales, and 69 percent of all sales were priced between \$150,000 and \$250,000. A little over half of all sold multifamily units were 3- and 4-bedroom units, with 4-bedroom units enjoying a 25 percent higher sale rate than 3-bedrooms do, making them unusually competitive with for-sale townhomes.

Residential Net Deliveries & Vacancy



Source: CoStar



Source: Lennar.com

Multifamily Residential Sales Volume & Sale Price Per Unit (Inflation Adjusted)



Source: CoStar

### Multifamily Rentals

Recent trends indicated the multifamily rental market had a minor blip in sales and vacancy patterns. At 3.5 percent, multifamily rental's vacancy rate is at near historical lows. This is aided by very little activity in new constructions, with the total number of units almost unchanged since 2015. In addition, 3-bedroom units have experienced a shift from having one of the highest vacancy rates before 2020 to the lowest in the multifamily rental market at 1.6 percent in 2021. Since 2016, rents have been growing in the Crystal Lake-area market, while rents in Chicago have remained steady. These trends signal that the multifamily rental market may be ready to be tested again for new residential developments.

### Residential Market Summary

Demand is strongest for single-family for-sale townhouses (1- to 3-bedrooms) and rental apartments (studio to 2-bedrooms). The findings indicate a lower level of local and regional demand for for-sale condominiums. Still, the rental multifamily market may be ready for deliveries of new units after a period of inactivity.





## OFFICE

The Office Market shares the same market boundaries as the Retail market, based largely around highways and a travel-time of around 15 minutes from Crystal Lake. It is roughly bounded by Algonquin (south), Johnsburg (north), Woodstock (west), and Wauconda (east).



## Office Summary

In the Crystal Lake Office Market, the vacancy rate has increased from 9 percent to 10 percent between 2020 and mid-2021. This is considered a low-end of averages for the past 10 years. The available datasets and recent trends suggest that there are nominal deliveries of office space scheduled for the near future, as developers are evaluating how the increasing popularity of working-from-home would impact demand for office space.

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Crystal Lake & Pingree Road Stations Strategic Action Plan  
**Station Areas Snapshot**



## RETAIL

Over the last decade, the retail market and the way consumers shop have undergone a major transformation due to the growth of e-commerce, which has been attracting sales and gaining market share from brick-and-mortar stores. According to the U.S. Department of Commerce, the share of total retail sales that is captured by e-commerce increased from 2.9 percent in 2006 to 10.2 percent in the first quarter of 2019. For some items, such as books and electronics, the penetration of e-commerce sales is very high.

Unlike demand for residential units, demand for retail space is derived from the demand for retail goods and services. It is driven by population and household growth, disposable incomes, and spending patterns. These factors remain the key determinants of the sales potential of a retail development despite the rapid growth of e-commerce.

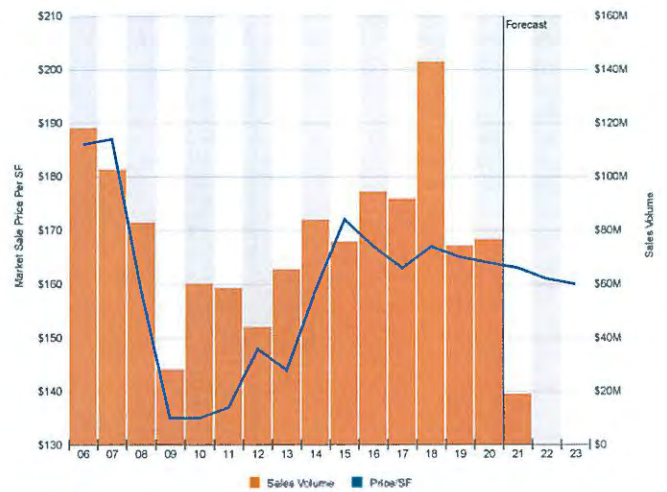
### Retail Market Area

The Crystal Lake Retail Market Area has the same market area as the City's Office Market has.

### Retail Summary

The Crystal Lake Retail Market remains in good health, characterized by relatively low vacancy rates and modest sales activity. Retail vacancy rates have, however, increased from five to six percent in 2020 while remaining near its 15-year historic lows. Rents per square foot have declined at a similar rate since 2015 but experienced a more severe drop-off since 2006, from which the market has not yet recovered.

Retail Sales Volume & Sale Price Per SF (Inflation Adjusted)



Source: CoStar





## INDUSTRIAL

Demand for warehouse and logistics space at the national level has significantly increased in recent years due to the rise of e-commerce. The Crystal Lake Industrial Market is the largest by land area, because its tenants typically are more location-flexible. The Market Area's approximate boundaries are McHenry (east), Marengo (west), Richmond (north), and Huntley (south).



## Industrial Summary

The datasets and trends suggest that the Crystal Lake Industrial Market is in healthy condition, with Warehousing & Logistics spaces being the main contributors. Since 2019, and throughout 2020, the Crystal Lake Industrial Market's vacancy rates had consistently matched its lowest levels in 20 years, despite being higher than of Chicago since 2011. Additionally, market rents have been steadily rising since 2011 from \$6.15/ft<sup>2</sup> foot to \$7.36/ft<sup>2</sup> when adjusted for inflation. It should be noted that rent per square foot has not been this high, adjusted for inflation, since 2004.

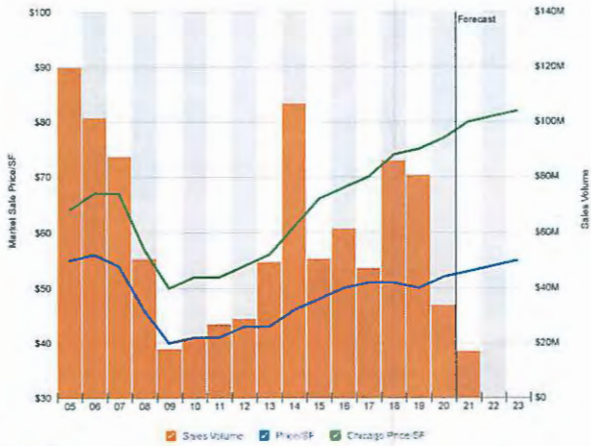
Logistics fared better than Specialized Industrial in terms of vacancies, despite both having similar share of the Industrial Market's total square footage. The vacancy rate for the former is five percent and 14 percent for the latter. Flex makes up 5 percent of the Industrial Market's total square footage, with a vacancy rate of 5 percent.

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Crystal Lake & Pingree Road Stations Strategic Action Plan  
Station Areas Snapshot



Industrial Sales Volume & Sale Price Per SF (Inflation Adjusted)



Source: CoStar

## EXISTING LAND USE & CURRENT ZONING

The following pages/spreads exhibit the existing land use and current zoning in the Crystal Lake and Pingree Road Station Areas. The former examines how land is utilized in both Station Areas, and the latter explores how the City's Unified Development Ordinance's zoning district influence development patterns in both Station Areas. **Refer to page 22 and 23 for the Existing Land Use maps and page 24 and 25 for the Current Zoning maps.**



EXISTING LAND USE

# CRYSTAL LAKE STATION AREA

Existing land use provides an inventory of every parcel in the Crystal Lake and Pingree Road Station Areas to identify how land is utilized within both Station Areas. The Station Areas' existing land uses have been broken into 12 distinct land use classifications, which together create an attractive Downtown environment for the Crystal Lake Station Area and a successful employment hub for the Pingree Road Station Area. This inventory represents data collected in September 2020.

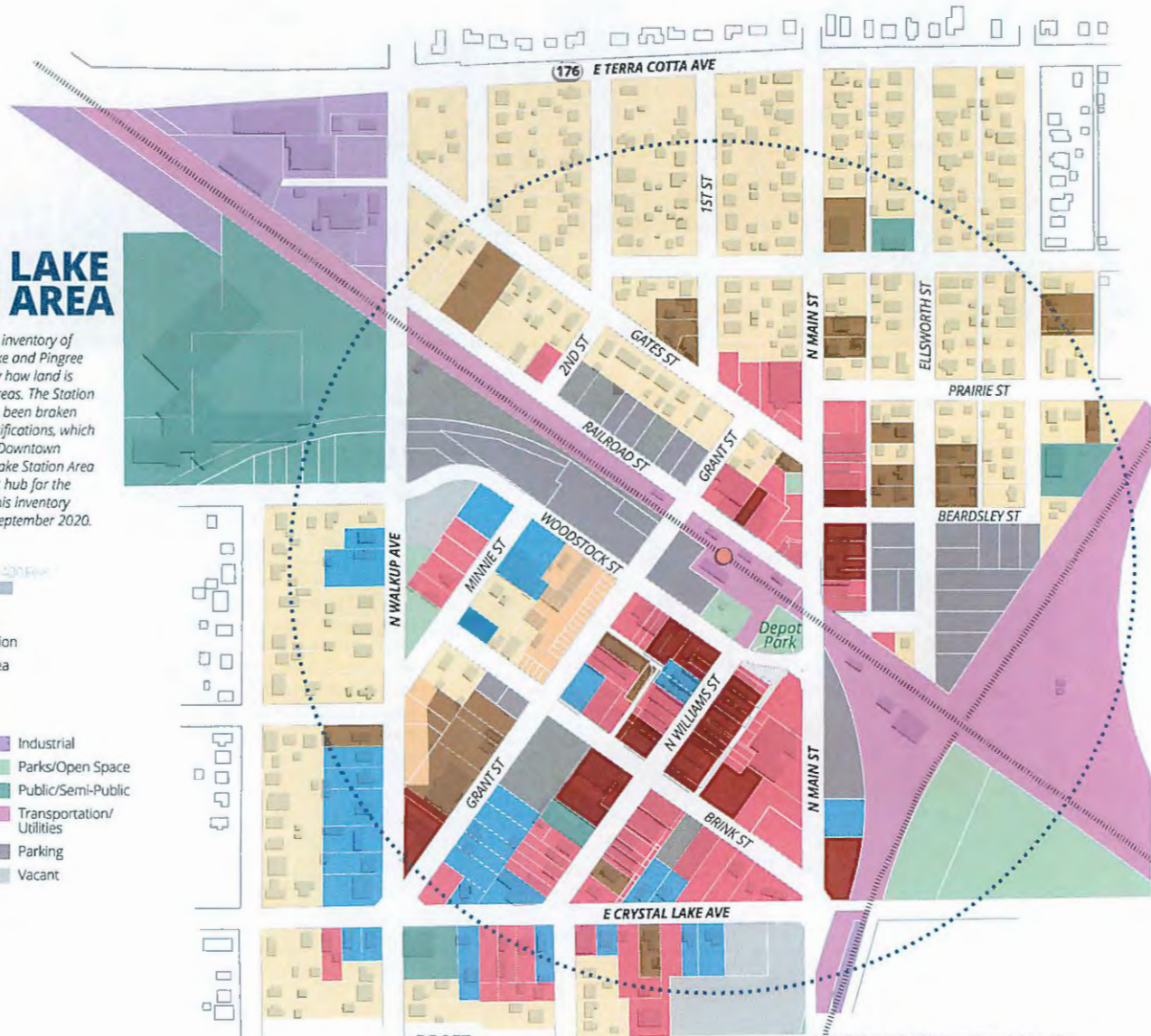


**Legend**

- Crystal Lake Metra Station
- Crystal Lake Station Area
- Railroad

**Existing Land Use**

- |                        |                          |
|------------------------|--------------------------|
| Single-family Detached | Industrial               |
| Single-family Attached | Parks/Open Space         |
| Multi-family           | Public/Semi-Public       |
| Commercial             | Transportation/Utilities |
| Office                 | Parking                  |
| Mixed-Use              | Vacant                   |



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Crystal Lake & Pingree Road Stations Strategic Action Plan  
Station Areas Snapshot

EXISTING LAND USE

# PINGREE ROAD STATION AREA

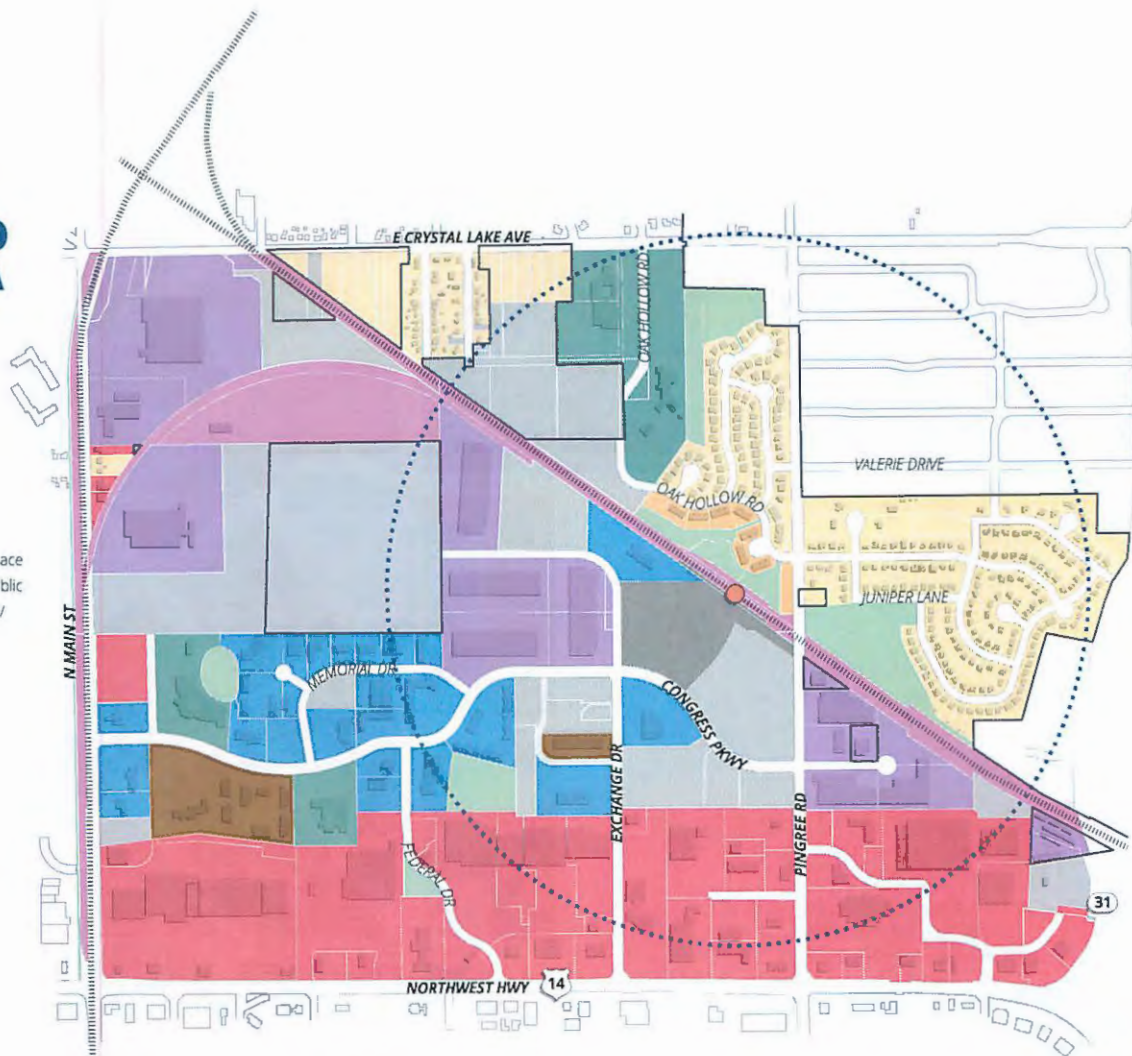


**Legend**

- Pingree Road Metra Station
- Pingree Road Station Area
- Railroad

**Future Land Use**

- |                        |                          |
|------------------------|--------------------------|
| Single-family Detached | Industrial               |
| Single-family Attached | Parks/Open Space         |
| Multi-family           | Public/Semi-Public       |
| Commercial             | Transportation/Utilities |
| Office                 | Vacant                   |





CURRENT ZONING

# CRYSTAL LAKE STATION AREA

Zoning and development controls have the power to shape and reshape communities by determining where specific land uses are permitted and the density of those uses, and by setting standards for setback, parking, landscaping, signs, and other standards that help to shape the visual attractiveness of a community or an area.

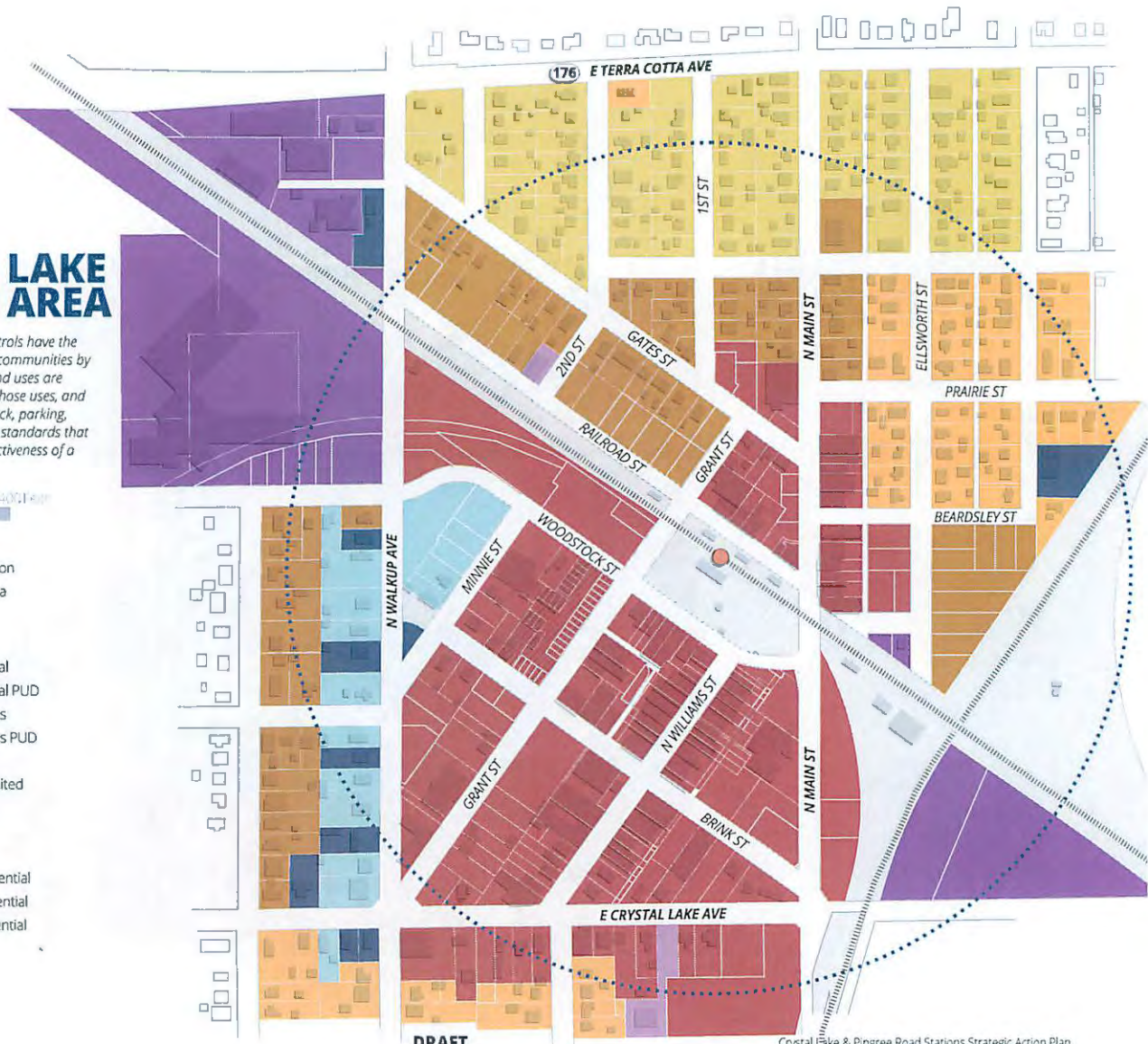


**Legend**

- Crystal Lake Metra Station
- Crystal Lake Station Area
- Railroad

**Current Zoning**

- B-2: General Commercial
- B-2: General Commercial PUD
- B-4: Mixed-Use Business
- B-4: Mixed-Use Business PUD
- M: Manufacturing
- M-L: Manufacturing Limited
- O: Office
- O: Office PUD
- R-O: Residential Office
- R-2: Single-Family Residential
- R-3A: Two-Family Residential
- R-3B: Multifamily Residential



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Crystal Lake & Pingree Road Stations Strategic Action Plan  
Station Areas Snapshot

CURRENT ZONING

# PINGREE ROAD STATION AREA

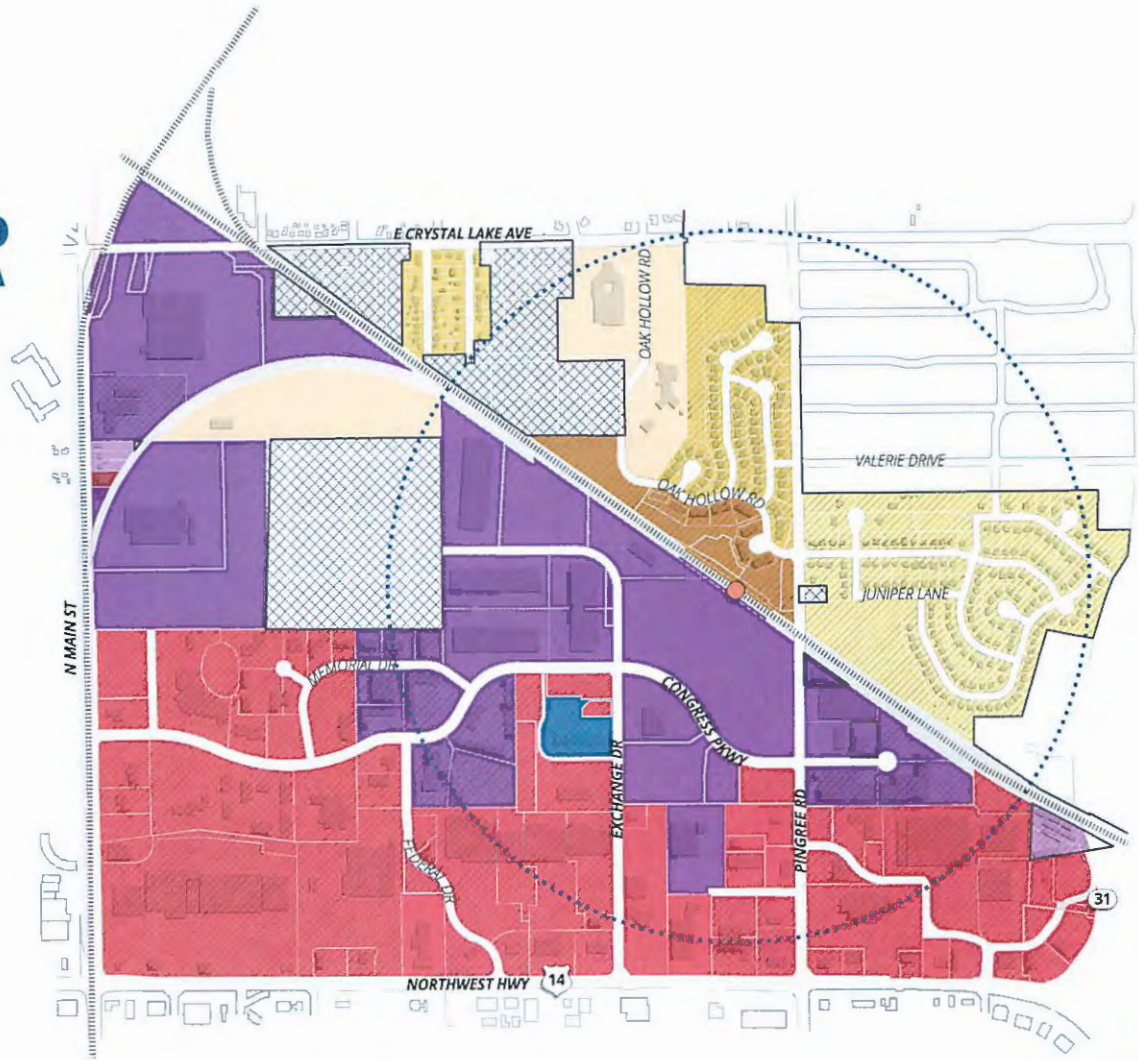


**Legend**

- Pingree Road Metra Station
- Pingree Road Station Area
- Railroad

**Current Zoning**

- B-2: General Commercial
- B-2: General Commercial PUD
- B-4: Mixed-Use Business
- County
- E: Estate
- M: Manufacturing
- M: Manufacturing PUD
- M-L: Manufacturing Limited
- M-L: Manufacturing Limited PUD
- O: Office PUD
- R-1: Single-Family Residential PUD
- R-2: Single-Family Residential
- R-2: Single-Family Residential PUD
- R-3B: Multifamily Residential PUD



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for Review





CHAPTER 3  
**VISION, GOALS, AND  
OBJECTIVES**

**DRAFT**  
for Review



The vision, goals, and objectives set the framework for the Crystal Lake and Pingree Road Stations Strategic Action Plan's recommendations. Together, they provide a guide for decision making and establish direction for new projects and policies. The vision, goals, and objectives are defined as follows:

**Vision** – Describes the desired outcome to be achieved through the implementation of the Plan. It is forward looking and describes what the community could be like if the goals and recommendations are achieved. Crystal Lake and Pingree Road Station Areas have their own Vision to reflect their distinct character and purpose.

**Goals** – The goals describe desired results toward which planning efforts should be directed. They are broad and long range. They represent an ambition to be sought and require the culmination of many smaller actions to be fully achieved.

**Objectives** – The objectives are specific and quantify the goals and set expectations for their achievement.

## VISION

### CRYSTAL LAKE STATION AREA

*The Crystal Lake Metra Station Area is a unique destination known for its thriving small businesses and a visually attractive and historic Downtown. Residential and mixed-use infill developments anchor prominent entrances to the area with local shops and restaurants. These businesses, along with unique cultural and entertainment venues attract visitors from around the region. New development complements the established character of the surrounding neighborhoods and provides quality housing options within walking distance to the Metra station. Being in Downtown provides residents with convenient access to everyday goods and services, which has also attracted more young professionals to Crystal Lake.*

### PINGREE ROAD STATION AREA

*The Pingree Road Metra Station Area has a growing business park that strengthens Crystal Lake's economic vitality. The Main Street Crossing site is incorporated into the City, fostering additional growth in employment and housing options for all ages. Streetscaping improvements give the Station Area a unique identity and complement transportation enhancements to foster a multimodal environment. Safe and direct connections are provided to community assets, such as Feinburg Park and the Prairie Path Trail. With easy access to employment, housing, and the US Route 14 retail and service corridor, the Pingree Road Metra Station Area is an attractive live-work environment for Crystal Lake residents.*





## GOALS & OBJECTIVES

### HOUSING

Provide a greater range of housing options for people at all life stages and background within proximity to the Crystal Lake and Pingree Road Metra stations.

#### Objectives

1. Update the City's codes to promote residential development within Downtown Crystal Lake and Pingree Road Station Area, through regulatory changes and coordination with developers, while ensuring new housing is compatible with adjacent development and contributes to the area's desired character and greater base of residents.
2. Encourage developers to develop multi-family housing complexes with common space amenities that are attractive to young professionals.

3. The City should play an active role in working with developers, investors, and local businesses to position redevelopment opportunity sites for new development. When resources are limited, City efforts should focus on these sites.
4. Proactively work with developers to ensure future (re)developments feature visually appealing facades, site design with strong relationship to the public street, and sufficient amount of transparency, as well as leveraging opportunities to create additional outdoor dining/public open spaces to create additional 'third places'.

### ECONOMIC DEVELOPMENT

#### Goal 1

Strengthen Downtown Crystal Lake as an attractive shopping, dining, and entertainment destination.

#### Objectives

1. Provide opportunities for outdoor seating and activity areas in Downtown Crystal Lake's public rights-of-way to generate additional social gathering places.
2. Explore opportunities to implement a direct and safe pedestrian walkway between the existing alleyway at N Williams St and the Main St as an amenity within future development.

3. Enhance a section of a street as a festival street, using special streetscape improvements, movable bollards, and lighting that temporarily repurpose the right-of-way to provide a space for community events and public gatherings.
4. Continue to coordinate with Metra to explore redevelopment opportunities in Metra commuter parking lots that align with the Land Use Plan.
5. Utilize the Plan as a tool to market the desired vision and development potential of the Downtown to the development community.



**Goal 2**

Enhance the Pingree Road Metra Station Area as an economic center and expand the City's employment and tax base.

**Objectives**

1. Continue to allow for a mix of office and light industrial uses within the Crystal Lake Business Center and the Main Street Crossing site with a focus on maintaining high quality design that contributes to the desired character of the Station Area.
2. As development occurs, incorporate the Main Street Crossing site into the City limits and work with the developer to arrange cost sharing agreements associated with extending and expanding needed infrastructure.
3. Explore opportunities with developers and the Union Pacific Railroad to implement a rail spur connection between the rail line and the northern portion of the Main Street Crossing site to better attract industrial/logistic establishments into the Pingree Road Station Area.

4. Utilize the Plan as a tool to market the desired vision and development potential for the Main Street Crossing site and infill opportunities within the Pingree Road Station Area.
5. Use the Plan to proactively communicate the City's expectations for well-designed development and utilize pre-application conferences with City Staff to minimize uncertainties in the development process.
6. As development occurs, ensure there are sufficient buffers and screening between residential and non-residential uses to improve the marketability of residential products, including senior living facilities.

7. Continue to allow non-traditional tenants, such as gym, wellness clinics, and indoor sports recreation centers within office and industrial buildings to improve resiliency against changing market trends.
8. Continue to coordinate with Metra on commuter parking utilization rates and explore opportunities to convert land reserved for Metra parking into additional development and open space opportunities.
9. Coordinate with Metra to ensure the Pingree Road Metra station's pedestrian underpass is well-maintained throughout the year to maintain safe access for pedestrian and bicyclists.

**TRANSPORTATION**

**Goal 1 - 2030 Comprehensive Plan**

Provide a comprehensive transportation system to serve current and future land uses safely and efficiently, considering travel by road, rail, public transportation, bicycle, and foot.

**Objectives**

1. Provide a balanced transportation system to ensure the safe and efficient movement of vehicles, pedestrians, and cyclists.
2. Enhance mobility and accessibility by providing a comprehensive and connected transportation network.





**Goal 2 - 2017 Crystal Lake Transportation Plan**

The City of Crystal Lake envisions a transportation system that corresponds to adjacent land uses and increases community safety, reduces congestion, and decreases environmental impact by providing safe and convenient travel along and across streets through a comprehensive, integrated transportation network for pedestrians, bicyclists, public transportation riders and drivers, and people of all ages and abilities.

**Objectives**

1. Existing components of Crystal Lake's transportation network should be improved in order to better accommodate the different users of the system.
2. Enhance sidewalk network and strengthen pedestrian facility standards to improve walkability.

3. Expand bicycle network and broaden bicycle facility standards to accommodate a range of users.
4. Make public transportation an interconnected part of the transportation network.
5. Promote alternative transportation and transportation safety through education, encouragement, and enforcement.
6. Establish design standards for transportation improvements to better complement and accommodate the surrounding land uses. Use of transportation network design elements can enhance safety and provide for multimodal accessibility through strategies that separate pedestrians and bicyclists from motor vehicle traffic as well as provide opportunities to separate local traffic from truck traffic and provide more travel choices through improved connectivity.

**Goal 3 - Complete Streets Policy**

In November 2019, the City of Crystal Lake adopted a Complete Streets Policy "to create a comprehensive, integrated, and connected transportation network that accommodates all users: bicyclists, pedestrians, transit users, and drivers of automobiles and freight vehicles." Specifically, the Complete Streets Policy is to address the needs of all users in the planning, design, approval, and implementation for all transportation network improvements, with a focus on providing safe and accessible accommodations for pedestrians, bicyclists, and transit riders.

**Objectives**

1. Expand, enhance, and connect the active transportation network and infrastructure.
2. Establish design guidelines addressing access, intersections, driveways, and transit stops to foster multimodal options.
3. Promote transit supportive design for future planning and when development opportunities are considered.





## URBAN DESIGN

### Goal

Strengthen the Crystal Lake and Pingree Road Metra Station Areas as unique, safe, and attractive destinations by establishing welcoming "third places," enhancing pedestrian and bicycle facilities, and updating streetscape elements.

### Objectives

1. Expand Downtown Crystal Lake's existing streetscape palette to future (re)developments within the Crystal Lake Station Area.
2. Prioritize streetscaping elements at Pingree Road Station Area's key intersections, including planned roundabouts, to make them safer for all modes of transportation and be welcoming gateways.
3. Prioritize development of additional publicly accessible open space elements as development occurs within the Station Areas in alignment with the Plan's urban design framework.
4. Utilize the City's Capital Improvement Program to proactively address gaps in the City's sidewalk and trail network and incorporate such enhancements into planned roadway improvements where possible.
5. Ensure the design and scale of future development in both Station Areas complements adjacent development.
6. Continue to enforce the City's Landscaping requirements to ensure parking lots and utilities are sufficiently screened from public rights-of-way.
7. Work with Downtown Crystal Lake Main Street and local artists in establishing a program to install permanent and temporary public art installations to welcome residents and visitors to Downtown Crystal Lake. In the Pingree Road Station Area, the City can leverage opportunities to implement public art at the Station Area's key intersections.
8. Create a design guidelines document that provides clear expectations of what constitutes quality development and desired design features for new development in Downtown Crystal Lake.
9. Proactively work with the Crystal Lake Elementary District 47 Schools to ensure the District's future development on its newly acquired land will minimize land use conflicts with the surrounding neighborhoods.





CHAPTER 4  
**LAND USE &  
DEVELOPMENT**

**DRAFT**  
for Review





## LAND USE PLAN

The Land Use Plan provides a roadmap for Crystal Lake and Pingree Road Station Area's growth and development and outlines the type and location of appropriate uses within both Station Areas. The Plan aims to increase activity within Downtown Crystal Lake by increasing the number of businesses and residents near the Crystal Lake Metra station and promoting a complementary mix of development surrounding the historic core of Downtown Crystal Lake. In the Pingree Road Station Area, the Plan strives to continue to strengthen the Pingree Road Station Area's position as a local and regional employment center and bring in additional residential development within proximity to the Pingree Road Metra station.

## SINGLE-FAMILY DETACHED

Single-family detached consists of single-family homes occupying individual lots. In Crystal Lake Station Area, the single-family detached neighborhoods surrounding Downtown Crystal Lake is likely to remain. Portion of single-family detached houses in Downtown Crystal Lake, primarily along North Walkup Avenue and East Crystal Lake Avenue are considered as single-family conversions, because the buildings have been repurposed to accommodate commercial/office use.

## Recommendations:

- The Land Use plan envisions the single-family conversions at the southeast corner of North Walkup Avenue and Woodstock Street would transition into single-family attached townhomes and multifamily buildings. Other single-family conversions could remain to serve as a buffer between Downtown Crystal Lake and the adjacent residential neighborhoods.
- The existing single-family detached subdivisions north of the railroad tracks in the Pingree Road Station Area would remain.
- Additional residential land uses in the Pingree Road Station Area would primarily consist of single-family attached townhomes and multifamily buildings.

## SINGLE-FAMILY ATTACHED

Single-family attached includes residential structures where dwelling units share a common exterior wall with at least one adjacent unit and each dwelling unit has its own dedicated entrance. Examples of this land use include townhomes, rowhomes, and duplexes. Single-family attached units provide opportunities for the City to expand housing options at increased residential densities while complementing the scale and character of surrounding single-family detached neighborhoods.



**Recommendations:**

- The City should encourage additional single-family attached development in the eastern side of Walkup Avenue between Woodstock Street and Grant Street and in the southern end of East Crystal Lake Avenue and North Main Street in the Crystal Lake Station Area. Single-family attached should also be prioritized as one of the residential land uses within the Main Street Crossing Site in the Pingree Road Station Area.
- The City can consider creating a new 'Transition Urban' (TU) zoning district, which should accommodate transitional land uses, such as single-family conversions, single-family attached, multi-family, and offices, to create a transition between Downtown Crystal Lake and the surrounding neighborhoods. The TU zoning district should be located between the Walkup Avenue/Minnie Street/Woodstock Street redevelopment opportunity site, and along blocks where a transition between Downtown Crystal Lake and adjacent neighborhoods is desired.

**MULTIFAMILY**

Multifamily uses include buildings containing multiple dwelling units that are vertically stacked with shared common areas and entrances. This also includes single-family conversions, wherein single-family homes have been converted to house multiple tenants/families. Multifamily development can be either renter- or owner-occupied and plays a critical role in increasing housing options and addressing demand from young professionals and "empty nesters" looking to downsize and remain in Crystal Lake. Currently, single-family attached and multifamily land uses in Downtown Crystal Lake need special use that requires a Planned Unit Development (PUD) to be approved for development.

**Recommendations:**

- Multifamily should be concentrated within Downtown Crystal Lake, along the eastern edge of Walkup Avenue between Crystal Lake Avenue and Woodstock Street, and the Main Street Crossing site.
- Between Walkup Avenue, Grant Street, and Woodstock Street, the City can consider requiring special use with conditions, such as performance-based zoning, for single-family attached and multifamily uses create some flexibility in the development approval process while retaining City's ability to review the developments' proposed site design and program.

- Single-family attached and multifamily uses in other areas of Downtown Crystal Lake should continue to require special use with PUDs.





## COMMERCIAL

Commercial uses include retail and service businesses, such as shops, restaurants, salons, and auto service stations. This is the predominant non-residential land use in Crystal Lake Station Area, as they mostly front North Williams Street and North Main Street. Commercial uses in the Pingree Road Station Area are primarily located along US-14.

### Recommendations:

- Future commercial uses in the Crystal Lake Station Area should be situated within mixed-use buildings near the intersection of North Main Street, East Crystal Lake Avenue, and Brink Street.
- In the Pingree Road Station Area, existing commercial uses along US-14 would most likely to remain and there are opportunities for commercial uses along North Main Street within the Main Street Crossing site.

## MIXED-USE

Mixed-Use development consists of different land uses coexisting within the same building or complex. Mixed-use developments, with upper-story multifamily housing above ground floor commercial, can help improve housing options and increase local retail and office opportunities. They can also contain unique entertainment venues and hospitality businesses to create destinations.

### Recommendations:

- Mixed-use development is recommended only for the Downtown Crystal Lake Station Area. Higher-density mixed-use projects should be concentrated around the intersection of North Main Street, East Crystal Avenue, and Brink Street in the Crystal Lake Station Area. They should also complement Downtown Crystal Lake's existing architectural character.
- Mixed-use development is not recommended in the Pingree Road Station Area.



## OFFICE

Office uses include professional services, such as banks, medical offices and clinics, and law offices. In the Crystal Lake Station Area, office uses mostly reside in both standalone buildings and single-family detached homes at the southwest portion of the Station Area. In the Pingree Road Station Area, office uses, including medical offices, are situated in standalone buildings and are concentrated along Congress Parkway.

### Recommendations:

- Given uncertainties facing the office market and the strength of the industrial, the Plan only shows the office use designation for existing office locations desired to remain.
- Available sites where future office development could be accommodated could also accommodate high-quality business park uses and are shown as Flex in the Land Use Plan.

## FLEX

The Flex land use is recommended for several sites in the Pingree Road Station Area so as to promote a mix of uses that are compatible with the business park character of the Pingree Road Station Area. Flex is a class of space realtors use to define buildings that feature higher-ceilings that may accommodate office, research and development, light industrial, logistics, or a combination of similar uses. The intent is to preserve land use flexibility, so the City can better respond to future changes in the office and industrial real-estate markets while still supporting the vision for the Pingree Road Station Areas.

### Recommendations:

- Flex uses are primarily designated for vacant sites within Pingree Road Station Area including the central and northern portions of the Main Street Crossing site.

## INDUSTRIAL/LOGISTICS

Industrial uses, including warehousing, distribution, and manufacturing, can be found in the Pingree Road Station Area. They are mostly located between Congress Parkway and Exchange Drive and east of Pingree Road.

### Recommendations:

- Some industrial buildings have non-traditional tenants, such as fitness centers and indoor sports facilities, and the City should continue to provide such flexibility to improve industrial land use's resiliency against changing market trend. It is important these uses should be situated away from areas with heavy volumes of industrial/freight traffic.
- There are opportunities for industrial/logistics uses in the northern portion of the Main Street Crossing site with a potential rail spur connection to the existing freight railroad to the north.





### PUBLIC/SEMI-PUBLIC

Public and semi-public uses include municipal facilities, community service providers, schools, and religious institutions. Examples include City Hall west of the Crystal Lake Station Area, Crystal Lake Park District office at the southeast corner of North Walkup Avenue and East Crystal Lake Avenue, and Beardsley Middle School north of Pingree Road Metra Station. Crystal Lake Elementary District 47 has purchased the vacant parcels southwest of the Beardsley Middle School and this area is shown as an public/semi-public land use within the Land Use Plan.

#### Recommendations:

- The City should proactively work with the school district to minimize potential land use conflicts between the planned public/semi-public development and nearby residential land uses.

### TRANSPORTATION/UTILITIES

Transportation/Utilities uses support local infrastructure, provide right-of-way for the transmission of gas, electricity, water, and sewer, and movement of goods and people via rail. Metra currently has a coach yard by the Crystal Lake and Pingree Road Metra Stations. A coach yard is a type of facility Metra uses to store its trains overnight. It's possible that some of them may be relocated to other communities within the Chicago region in the future.

### PARKING

Parking consists of public parking lots owned by the City and Metra in the Crystal Lake Station Area. They can be one of the following parking lot types: daily commuter pay lot, 4-hour free parking, multi-day, and employee permit parking. Most parking lots by the Crystal Lake Metra Station and the ones located at Ellsworth and Beardsley Street at daily commuter pay lots. Metra also owns a 2.5-acre site near the Pingree Road Metra station which has been identified as excess property and is currently for sale.

### Metra Parking Lots

Several of the Metra parking lots around the Crystal Lake Metra station are currently bound by existing parking agreements between the City and Metra. These agreements are intended to provide sufficient parking capacities for the Crystal Lake Metra station. One of the parking agreements covers the Metra parking lot at Ellsworth and Beardsley Street and set to expire in 2045.

#### Recommendations:

- As the parking agreements approach their expiration year, the City should coordinate with Metra to analyze that Station Area's parking utilization rates and explore opportunities in redeveloping underutilized parking lots.





**REDEVELOPMENT OPPORTUNITIES**

The Plan identified multiple potential redevelopment opportunity sites for within the Crystal Lake and Pingree Road Station Areas. These sites were informed by the findings from field reconnaissance and analysis from market and demographic, existing land use and development, and transportation. They also present opportunities for (re) developments that would help advance both Station Areas' vision and goals. As resources are limited, the City should work with relevant stakeholders to prioritize resources in these sites.

Among these sites, three sites were selected among the redevelopment opportunity sites to target areas in the City that are most susceptible to redevelopment and have the potential to greatly impact the character of the community. These plans address the desired mix of land uses, access and mobility, as well as image and character. The selected sites are the following:

- Walkup Avenue/Minnie Street/Woodstock Street (Crystal Lake Station Area)
- Main Street/Brink Street (Crystal Lake Station Area)
- Main Street Crossing (Pingree Road Station Area)

These sites and their respective visualizations can be found in **Chapter 6: Illustrative Redevelopment Concept**.

LAND USE PLAN

# CRYSTAL LAKE STATION AREA



**Legend**

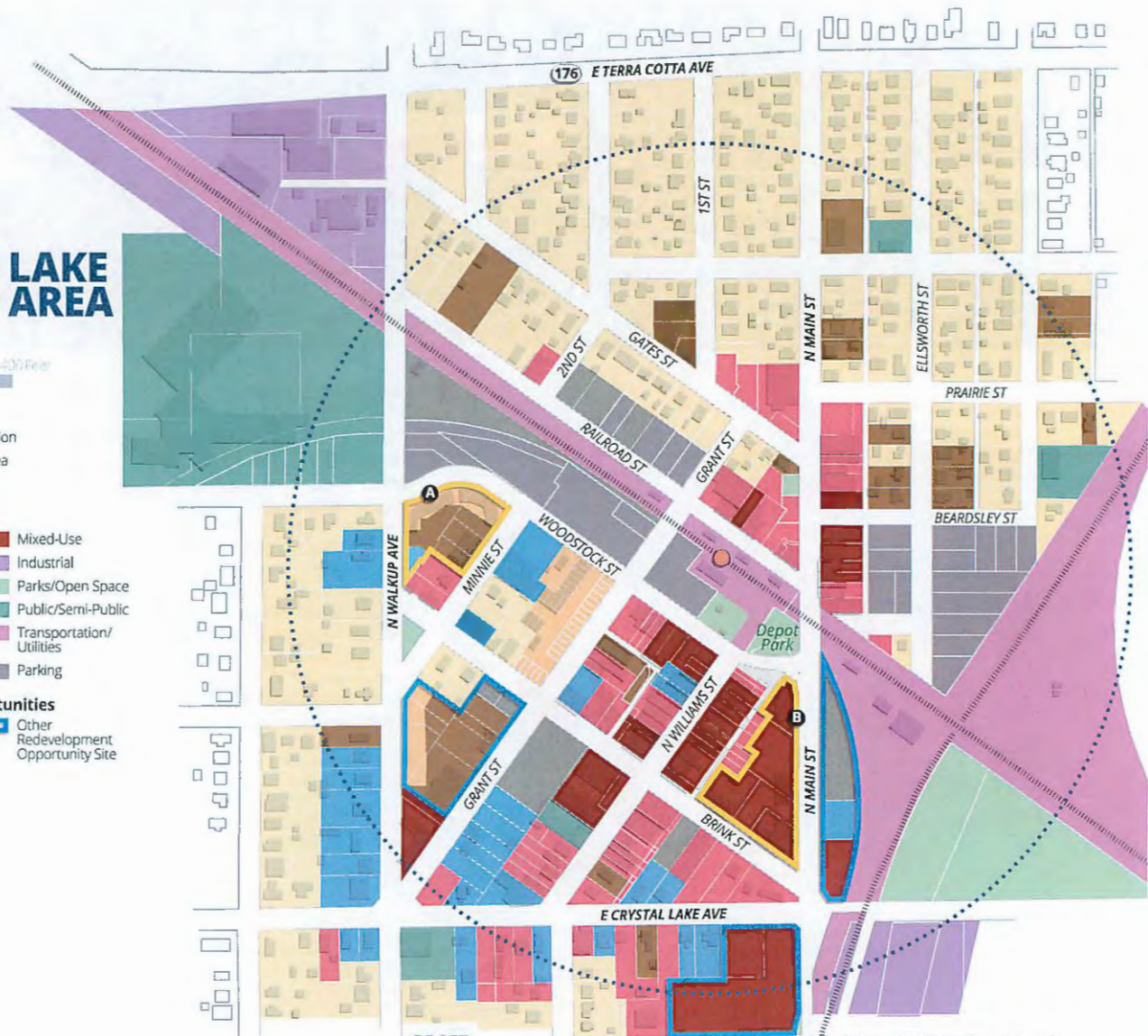
- Crystal Lake Metra Station
- Crystal Lake Station Area
- Railroad

**Future Land Use**

- |                        |                          |
|------------------------|--------------------------|
| Single-family Detached | Mixed-Use                |
| Single-family Attached | Industrial               |
| Multi-family           | Parks/Open Space         |
| Commercial             | Public/Semi-Public       |
| Office                 | Transportation/Utilities |
|                        | Parking                  |

**Redevelopment Opportunities**

- Walkup Avenue/ Minnie Street/ Woodstock Street
- Main Street/Brink Street
- Other Redevelopment Opportunity Site



**DRAFT**  
for Review

Crystal Lake & Pingree Road Stations Strategic Action Plan  
**Land Use and Development**



LAND USE PLAN

# PINGREE ROAD STATION AREA

Scale: 1" = 100'-0" (30.48m)

**Legend**

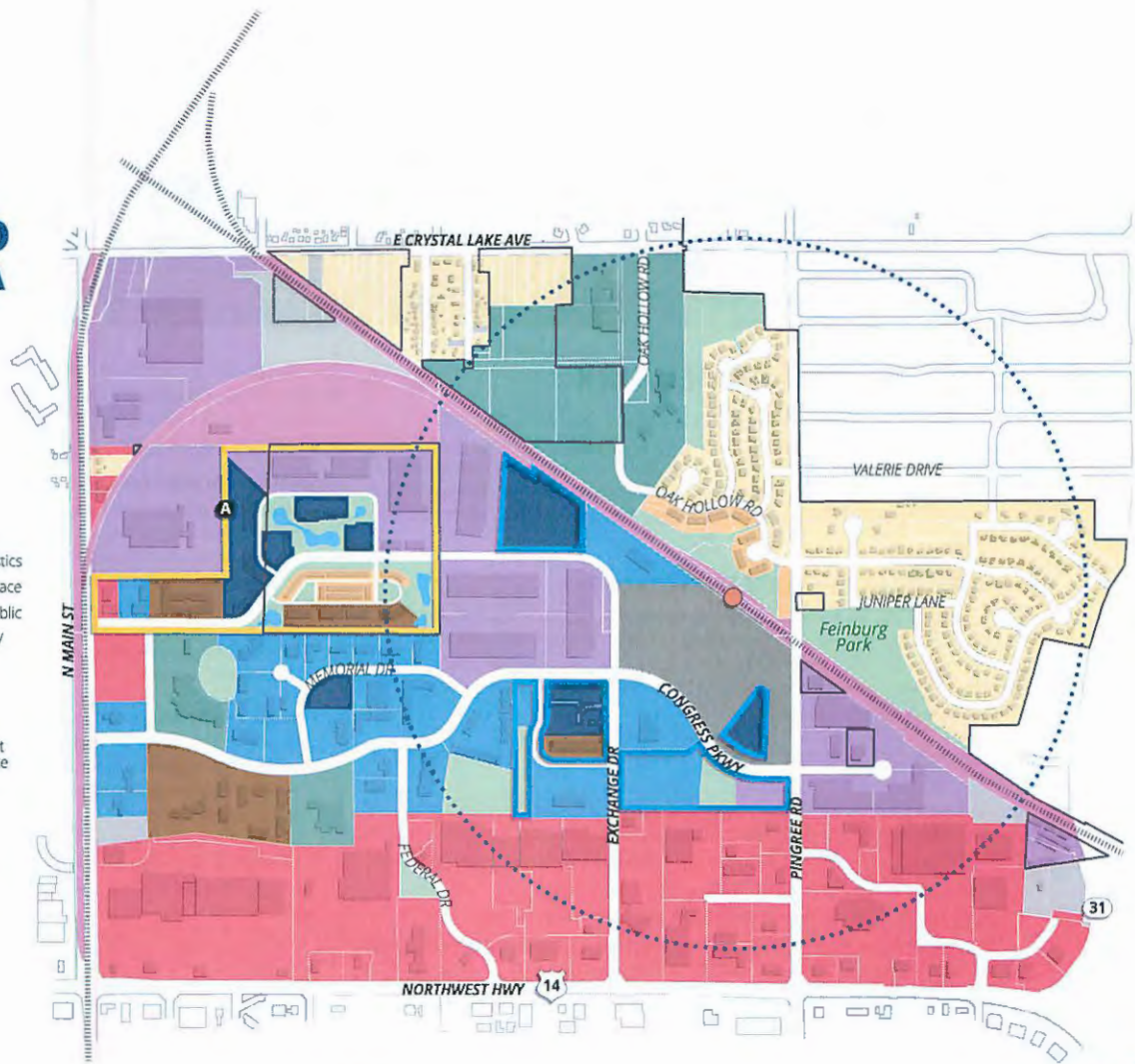
- Pingree Road Metra Station
- Pingree Road Station Area
- Railroad

**Future Land Use**

- |                        |                          |
|------------------------|--------------------------|
| Single-family Detached | Flex                     |
| Single-family Attached | Industrial/Logistics     |
| Multi-family           | Parks/Open Space         |
| Commercial             | Public/Semi-Public       |
| Office                 | Transportation/Utilities |

**Redevelopment Opportunities**

- Main Street Crossing
- Other Redevelopment Opportunity Site





## URBAN DESIGN FRAMEWORK

The Urban Design Framework chapter consists of a toolbox of urban design best practices that can be implemented throughout the Crystal Lake and Pingree Road Station Areas. Some of the tool focus on the public realm while other tools will guide improvements to private property. While recommendations can generally be applied to both Station Areas, where possible the Plan identifies specific applicability within each Station Area.

### PROMOTE VISUALLY ATTRACTIVE FACADES

A building's façade influences the overall visual appeal of the surrounding area and helps define how the building contributes to a sense of activity or streetlife. It is important for future (re)development in both Station Areas to have visually attractive façades with minimal blank walls. Some of the key characteristics to achieving visually appealing façades include, but are not limited to:

- Articulations, such as building wall off-set, on the façade wall at a regular interval and avoid blank walls
- Articulations on the building rooflines, such as adding ornamental features and having varying roofline heights and/or off-sets at regular intervals
- An appropriate amount of transparency, such as windows and doors with glass panels, in the ground- and upper-floors
- Varying façade material between the ground- and upper-floors
- Elements, such as cornices, bulk heads, masonry piers, transom, window hoods, and other similar details, to create horizontal and vertical articulation
- Awnings and building canopies to add visual interest and a degree of weather protection

The City should coordinate and communicate its expectations for visually appealing façades with developers to ensure future (re)developments would foster a sense of activity, such as pedestrians interacting with activities inside the buildings, while complementing with the surrounding areas' character and building scale.

### Crystal Lake Station Area

- **Mixed-Use and Multi-Family Building.** Façade articulation and design is particularly critical for larger-scale projects such as multifamily and mixed-use developments visible from the street. Façade articulation should help make the buildings' primary and secondary entrances visible. Since many properties in the Downtown are visible from multiple sides, quality articulation and design should also be encouraged on secondary façades such as those accessible from a side street or rear alley. In addition to building scale and mass, should complement with Downtown Crystal Lake's historic character.
- **Single-Family Attached Townhome.** Townhomes should achieve the "brownstone" character. This may incorporate bricks, sandstone, or limestone as the primary façade material and consistent use of building design elements including wide front stairways leading to a front porch or stoop, bay windows, and upper story balconies. They should have consistent height and scale within the same development and should be clustered together to form a solid streetwall while offering occasional gaps for pedestrian access to the rear of the site.





### Pingree Road Station Area

- **Prioritize Façade Design at Prominent Corners and Entrances.** The Pingree Road Station Area features several flex and industrial buildings that are of significant size. Blank façades should be minimized when visible from public rights-of-way and façade articulation and design should be prioritized at building entrances and prominent corners. Less prominent façades should, at a minimum, have a consistent rhythm in façade design and articulation.



## STRENGTHEN THE RELATIONSHIP BETWEEN THE BUILDING AND THE STREET

A strong relationship between the building and the street is crucial in generating a positive experience and a cohesive environment in both Station Areas. In general, buildings should orient towards the street with primary entrances located along major streets. Secondary entrances should provide access to adjacent amenities, such as open space and outdoor dining space, and service areas including parking lots and rear alleys. Buildings should also feature a high level of transparency on the ground floor to generate visual interest between the pedestrian and activities within the building.

### Crystal Lake Station Area

- **Minimal or No Front and Side Setback.** Buildings should be located as close to the sidewalk as possible to maximize opportunities for pedestrian and cyclists to interact with adjacent buildings and businesses. Where appropriate, buildings should abut each other to form a solid streetwall and foster a sense of enclosure that fosters a safe and comfortable pedestrian experience. New development should allow for wider sidewalks and enhanced streetscaping.
- **Locate Parking at the Rear.** Although parking lots are a necessary development component, they can be unsightly and creates voids in pedestrian activity that adversely impacts the overall experience of the Crystal Lake Station Area. Off-street parking lots should be located at the rear of the buildings. Those that are visible to the public right-of-way should be sufficiently screened with a combination of low-lying landscaping and short decorative fencing or a masonry knee wall.

- **Access management.** Although driveways are needed to provide vehicular and freight truck access into non-residential buildings, they may contribute to a disjointed pedestrian and bicycle environment. As future (re)developments occur within Crystal Lake, driveways should be consolidated to minimize the number of traffic conflict points. As part of the development review and approval process, the City should be proactive in identifying opportunities for future cross access alignments between adjacent developments.
- **Building Scale.** In addition to façade design, building scale is an important contributor to a positive Downtown experience. While it is desired to achieve greater density of housing options within proximity to the Crystal Lake Metra station, the scale of future single-family attached and multifamily (re)development should complement existing desirable development in Downtown Crystal Lake. For example, developments in Crystal Lake Station Area should have consistent none or minimal and consistent front setback requirements. Future mixed-use developments in the Main/Brink site, which has a significant grade change, may possess additional number of stories while avoiding a significant building height difference than that of existing buildings in Downtown Crystal Lake. Developments in Pingree Road Station Area should have consistent development bulk standards, so they collectively contribute to a cohesive sense of place.





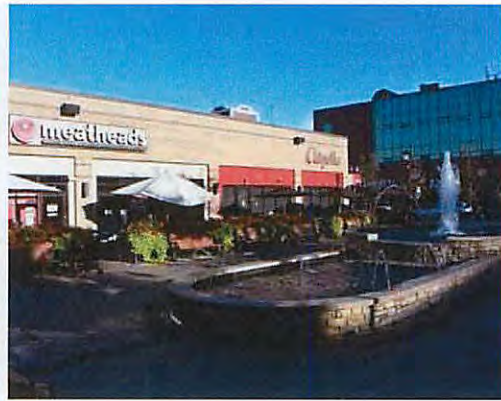
### Pingree Road Station Area

- Building Setback.** Residential land uses should have appropriate front setback that provides adequate front yard for landscaping. Given the suburban character of the Pingree Road Station Area, non-residential land uses such as flex, commercial, and industrial, may be located further away from public streets. The setbacks should be consistent with that of adjacent and existing non-residential land uses. If a proposed development is located between two building with different front setbacks, its front setback should be average of the adjacent buildings' front setback. Developments should have sufficient landscaping to improve the building and streets' visual appeal.
- Pedestrian Access.** Where possible, parking should be located at the rear or side of a building. Off-street parking should include direct and safe pedestrian pathways between the public right-of-way and the building's primary entrance. Parking lots that are visible to the public right-of-way should be sufficiently screened with a combination of low-lying landscaping and short decorative fencing or a masonry knee wall.



Examples of providing safe and direct pedestrian access through surface parking lots to developments' primary entrance





### CREATE "THIRD PLACES"

Third places are places that provide opportunities for social interaction outside of the home (first place) and workplace (second place) and anchor civic life in a downtown. Examples include restaurants and coffee shops, farmer's markets, and public parks and plazas. Third Places are important in building a strong sense of place and a vibrant pedestrian environment, because they provide destinations for people to meet and socialize with one another. When located within a downtown or commercial district, third place amenities such as outdoor patios, seating areas, and plazas can also benefit the business community. Future development should provide opportunities for enhanced placemaking, public art, community identity, and development of third places.

### Crystal Lake Station Area

- **Public Open Space.** Downtown Crystal Lake currently has limited open space. Future development in the Crystal Lake Station Area should incorporate open space components that are visible and accessible from the public right-of-way. They should be well-shaded to improve comfort for users during warmer weathers. Depending on the size and intended use, these spaces can be designed to accommodate outdoor events, such as farmer's markets and neighborhood block parties.
- **Outdoor Dining Space.** Outdoor dining helps activate the surrounding space and public sidewalk and attracts more visitors and potential customers to Downtown Crystal Lake. Future developments should incorporate outdoor dining spaces into their site design. Ideally, they should be located where they would be visible and accessible from the public right-of-way. Existing businesses can also apply for an outdoor seating permit from the City, and the City should educate and inform businesses that such option is available.
- **Mixed-Use Development.** Mix of land uses in the Crystal Lake Station Area will be an important ingredient in bringing more people into Downtown Crystal Lake. Throughout the outreach process, there has been a strong desire for additional residential units being situated within and/or near Downtown Crystal Lake. These units, in terms of unit layout and building design, should orient towards young professionals, empty-nesters, and other tenants who are not buying a home.
- **Festival/Shared Streets.** Festival Street involves temporarily closing a section of a street to host community events. Shared Streets takes a step further by removing the grade difference between the driveways and sidewalks and install streetscaping elements to prioritize the streets for pedestrians and bicyclists over motorists. Both help activate the place and provide social gathering opportunities. The 'Install a Festival/Shared Street' section in the Urban Design and Development Considerations explores this concept further.





### Pingree Road Station Area

- **Outdoor Dining Space.** Like Crystal Lake Station Area, future developments should leverage opportunities to provide outdoor dining spaces for users to congregate and socialize.
- **Trail Network.** Pingree Road Station Area presents opportunities to form a cohesive off-street shared-use path network. Given this area is significantly larger than Crystal Lake Station Area is, the trail network would function as a third place, where people can socialize while taking walks.
- **Orientation to a Central Feature.** The Main Street Crossing site features a central stormwater detention system. In addition to mitigating stormwater runoff, this also functions as a public open space where adjacent buildings should orient towards to. This combination of this feature and building orientation fosters a cohesive employment/business park environment.



Example images of stormwater detention facilities serve a dual-purpose of mitigating stormwater runoff and as a central open space where buildings and activities orient toward to.

## IMPLEMENTING THIRD PLACES IN THE STATION AREAS

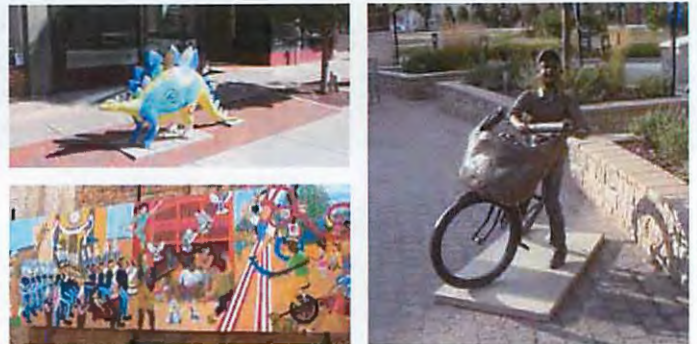
The previous section described high-level opportunities the City and relevant partners can leverage to create additional third places in both Station Areas. This section provides more detailed discussion on how third places can be expanded through three strategies:

- Tactical Urbanism
- Streetscaping Projects
- Future Development

### Tactical Urbanism

Tactical Urbanism typically involves temporary, low-cost installations in the public realm to test new urban design and transportation concepts and see if the community has the appetite for a more permanent solution. The following are examples of potential tactical urbanism interventions that could be used to generate more destinations and increase opportunities for people to stay longer and socialize in the Station Areas. It should be noted that these strategies are more applicable to the Crystal Lake Station Area since it is smaller and more pedestrian-oriented than the larger Pingree Road Station Area.

**Public Art:** As previously described, public art installations, such as sculptures and murals, are instrumental in creating a sense of place and can attract visitors to a specific area in Downtown Crystal Lake. Public art is well-suited to provide visual interest in areas with few activities, such as a block fronted by a surface parking lot or long blank wall, and fill gaps between activated spaces. Wall murals can be situated on buildings with blank facades to improve to buildings' visual interest. Alleyways, such as the existing one at the south side of North Williams Street, are great opportunity areas for mural installations that would give the alleyways unique identities and make them as destinations and 'hidden gems' of Downtown Crystal Lake rather than as purely service throughways.



Public art can take place in various forms, such as sculptures (top-left and right) and murals on blank facades (bottom-left).

**Parklets:** A parklet is a temporary or permanent conversion of one or more on-street parking spaces into small-sized public park or enclosed seating area. These areas typically feature a variety of streetscaping elements, such as decorative fences and planters, a seating area, enclosures for shading, bicycle parking, and public art installations. Parklets should be designed to be accessible to the public and not associated with a particular business. Parklets have been effective in generating additional pedestrian activities and sales for nearby businesses by breaking the traffic flow and giving people a reason to stop and socialize. Parklets work best when they are located in a street that has an active level of pedestrian activity and features a mix of businesses.





Parklets can be useful in creating additional public gathering spaces (top-left: Flickr, Arlington Department of Environmental Services; bottom-left: Flickr, Mission Bicycle Company). Festival street involves temporary closure of a section of a street to host community events (right).

**Weekly/Annual Street Closure:** As previously described related to festival streets, the City may consider closing off a section of a street to host community events as a means of bringing people together and creating opportunities for businesses to exhibit their goods and services. As highlighted in the adjacent case study, communities can be creative in creating events that are popular while using proceeds generated to fund public projects.

### Case Study: Paint the Port, Freeport, IL

Every year, the City of Freeport, in collaboration with the Freeport Art Museum, hosts the Paint to Port in Downtown Freeport. The City closes several sections of Downtown Freeport's streets and spray-paints over 400 4-foot square canvases on the roadways for the community to paint on. Participants pay a fee to register for this event and receive a bucket of art materials to paint on their designated street canvas. Local businesses and organizations can also donate a portion of their event-day proceeds to be one of the event sponsors. Attendees visit nearby businesses after painting their canvas, contributing to greater activity base in the Downtown. The event's net proceeds are used to fund Downtown Freeport's Façade Improvement Program and other City's priority capital projects, such as the completion of the Arts Plaza in Downtown Freeport. This event has quickly gained popularity in Freeport as a fun and family-friendly event.



### Streetscaping Projects

Streetscaping projects are another opportunity the City can leverage to build additional third places in the Crystal Lake and Pingree Road Station Areas. In the Crystal Lake Station Area, the City has a well-established streetscape palette that should be expanded to the identified redevelopment opportunity sites. To maximize opportunities to create additional third places, streetscaping projects should achieve the following principles:

**Wide sidewalks/pedestrian zones.** This creates sufficient room for outdoor dining spaces while maintaining enough room for pedestrian traffic.

**Curb Extensions & Mid-Block Crossings.** Refer to the transportation plan for where recommendation location for curb extensions and mid-block crossings are located. Both improvements involve the extension of the sidewalks into the drive lanes and/or parking lanes, and this provides opportunities to install various streetscaping elements, such as street trees, benches, planters, wayfinding signage, and public art installations. A well-designed curb extension and mid-block crossing create additional opportunities for people to stop, relax, and socialize.

**Public Art Installations.** The design and implementation process of streetscaping projects should examine where gaps of activities exist in both Station Areas. Once identified, the streetscaping project should provide sufficient space at these locations for public art installations, which would create interest and give a reason for people to visit the area, hence expanding the extent that pedestrians are willing to walk in both Station Areas.

**Building Upon Tactical Urbanism Strategies.** Streetscaping projects, which are considered permanent interventions, can build upon the tactical urbanism interventions that have proven to be well accepted by the community. For example, if a City's pilot project of designating a section of a street as a festival street is popular among businesses and residents, the City may consider a streetscaping project to further enhance amenities such as seating areas, pedestrian lighting, and landscaping, and evaluate potential reconstruction of that section as a shared street.



Example images of streetscaping that incorporate wider sidewalk, public art, and curb extensions.



### Future Development

In the private realm, the City, through its development and Planned Unit Development (PUD) review process, can work with developers to seek and leverage opportunities to incorporate third place elements into the site design and programming of future proposed developments. Desirable elements might include designated outdoor seating and plaza spaces, public open space, wide sidewalks to provide for flexible uses such as seating or public art. It is important that the City ensures that these elements are visible and accessible from the public right-of-way. Creating and adopting a design guideline document can be an effective tool in setting expectations of what constitutes a well-functioning and attractive third place element upfront to developers, hence reducing uncertainties and possible delays in the development review process.







### UTILIZE STREETSCAPING TO ENHANCE/CREATE IDENTITY

Streetscaping includes improvements to the design of public rights-of-way to enhance the user experience. Streetscaping is an effective public-realm strategy in creating and reinforcing the unique identity for a particular district or corridor. Investments in the design, construction, maintenance, and programming of public space in the Station Areas is needed to foster social interaction and a productive business environment. In both Station Areas, streetscaping should also play a role in promoting a safer and more accessible physical environment for all modes of transportation.

### Crystal Lake Station Area

- **Continuing Downtown Crystal Lake's Streetscape Palette.** Downtown Crystal Lake's existing streetscape palette has been instrumental in promoting a 'historic' and pedestrian-friendly environment. Downtown's streetscape includes a variety of complementary elements, such as decorative pavers, curb bump outs, ornamental pedestrian-scale street lights, limestone planters, street furniture, and wayfinding signage. Current streetscaping focuses on Williams Street with some elements present on secondary streets like Brink Street or Woodstock Street. It is critical that the Downtown Crystal Lake's streetscaping improvements be expanded to include key redevelopment opportunity sites and the western edge of Downtown Crystal Lake.



*Downtown Crystal Lake's existing streetscaping elements.*

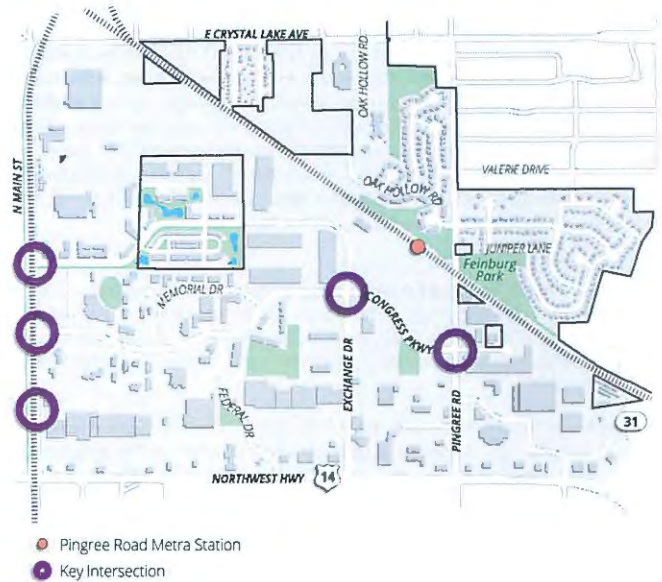


### Pingree Road Station Area

- Promote Dual-Scale Lighting Fixtures.** The City should replace its auto-oriented street lighting fixtures along Congress Parkway and Memorial Drive with dual-scale fixtures to provide sufficient lighting on both roadways and adjacent sidewalks and trails. The proposed extension of Exchange Drive and the existing segment south to Station Drive should also be improved as development occurs at the Main Street Crossing site.
- Prioritize Streetscaping at Key Intersections.** Pingree Road Station Area should have its own streetscaping palette to promote a multi-modal environment. It may borrow certain fixtures and models from the Downtown Crystal Lake's streetscape design to improve cost-effectiveness. Pingree Road's streetscaping improvements should be prioritized at key intersections, such as the intersection of Congress Parkway and Exchange Drive. These elements may include marked crosswalks, planters, gateway signage, and decorative pavers. Strategically implementing these elements within the Pingree Road Station Area will help improve safety in key intersections and transform them into welcoming gateways into the Pingree Road Station Area.



Examples of dual-scale lighting fixtures. Alternating pedestrian- and auto-scale fixtures can be an appropriate alternative.











### Pingree Road Station Area

- **Gateway Signage at Main Street Crossing.** As future development occurs at the Main Street Crossing, the City should work with developers to implement gateway signage at roadway connections between Pingree Road Station Area and Main Street Crossing to announce entry into this area. Pingree Road Station Area already has existing gateway signage at the intersection of Congress Pkwy at North Main Street, and Congress Parkway at North Pingree Road. The City should continue to coordinate with Metra and relevant parties to ensure they are well-maintained.
- **Visually Appealing Developments at Gateway Locations.** Pingree Road Station Area currently has vacant sites that are open for development at gateway locations entering the Pingree Road Station Area. They consist of Metra's Excess Property and the vacant land to the south at the intersection of Congress Parkway and North Pingree Road, in addition to Main Street Crossing fronting North Main Street. It is important that future developments in these locations prioritize façade design and articulation at the developments' primary entrance and facades that are visible to the major roadways.
- **Public Art.** The City should prioritize public art installations, such as sculptures or specially-designed gateway signages, in Pingree Road Station Area's key intersections. They would complement the recommended streetscaping improvements in these locations and generate positive impressions among people who enter into the Station Area.





### INSTALL A FESTIVAL STREET OR SHARED STREET

A festival street is a specific portion of a street that is intended to host frequent community events and can be closed to vehicular traffic on a regular basis. These areas are improved with placemaking elements such as overhead lighting, unique planters and signage, and seating areas and can be used by adjacent businesses. The intent is to activate the space and provide a venue where people can gather and socialize. Moveable and temporary planters or bollards and lighting can be used in combination with special streetscaping and pavement treatments to delineate the festival space. As a strategy to promote pedestrian activity in areas outside of the Williams Street corridor, the City should evaluate potential locations for a festival street within Downtown Crystal Lake.

A shared streets takes the festival streets concept further with permanent improvements. A shared street is a curbless roadway or plaza with limited on-street parking that prioritizes pedestrians and bicyclists while allowing cars to travel through at low speeds. A shared street could form the focal point of new development along a roadway and serve to attract businesses and new residents to the area. The shared street concept has been successfully implemented on N River Street in Downtown Batavia as well as Argyle Street in the City of Chicago's Uptown neighborhood. It is recommended that a shared street concept be considered after successfully piloting a less resource intensive festival street concept.

The following highlight potential Festival/Shared Street locations, as well their associated pros and cons, that should be evaluated further for a potential pilot study.

#### Railroad Street between N Main Street and 2nd Street

- **Pros.** Level of activity is relatively lower in Downtown Crystal Lake north side of the railroad tracks than it is south side of the railroad tracks. Implementing a Festival Street provides an opportunity to expand the Downtown's activity to the north.
- **Cons.** It is located close to Crystal Lake Metra Station's train platform, so safety measures, such as fences and bollards, would be required to ensure safety for users.





**N Williams Street between Brink Street and Woodstock Street**

- **Pros.** This section of N Williams Street is already considered as the 'core' of Downtown Crystal Lake. Implementing the festival/shared street concept can further strengthen this existing core.
- **Cons.** Potentially lose opportunities to activate spaces and generate activities in other parts of Downtown Crystal Lake that have relatively lower levels of activities. Upgrading this section of N Williams Street into a shared street would require significant logistical planning and buy-in from existing businesses. During construction phase, the existing on-street parking on this section of N Williams Street may become unavailable.

**Brink Street between Grant Street and N Main Street**

- **Pros.** The festival/shared street would help bridge the gap of activities and spur redevelopment along this section of Brink Street, which currently seems to be function purely as an east-west connector through Downtown Crystal Lake.
- **Cons.** Designating this section as festival/shared street may result in traffic spillover to adjacent roadways, including Woodstock Street, Grant Avenue, N Main Street, and Crystal Lake Avenue.







### SCREEN UTILITIES & PARKING LOTS

Parking lots, loading bays, dumpsters, and utilities are essential development components; however, if they are not properly screened they may adversely impact the surrounding area's visual appeal. The City should ensure off-street parking lots and other necessary utility components are located at the rear of the building. Where these elements abut to the street, decorative landscaping and fencing are effective strategies in maintaining an aesthetically pleasing public right-of-way and mitigating stormwater runoff. The City may consider masonry walls as a screening device where landscaping alone cannot provide a sufficient screen. Berms, where feasible, may be conducive in providing a more substantial screening and physical buffer, especially mitigating land use conflicts between residential and non-residential uses. The City should ensure its landscaping and buffering standards are updated and ensure that site improvements comply with adopted regulations.

### Crystal Lake Station Area

- **Using Buildings to Screen Parking Lots and Utilities.** Parking lots and other necessary development utilities, such as dumpsters, should be located at the rear of the building. Any portion of such facilities that is visible to the public-right-of-way should be effectively screened with a combination of landscaping, fencing, and masonry wall.

### Pingree Road Station Area

- **Separation of Non-Residential and Residential Areas.** Vacant sites within the Pingree Road Station Area present the potential for new employment-focused development to be located in proximity to existing single-family attached and multifamily development. The land use mix recommended for the Main Street Crossing site also includes residential development located across the Exchange Drive extension from flexspace and industrial uses. Where possible, landscaped berms should be accommodated within future site designs to achieve physical separation between conflicting land uses and minimize impacts to residential viewsheds. Berming will be instrumental in making the residential area recommended for the Main Street Crossing site feel sufficiently separated from nearby industrial uses and function as its own neighborhood.
- **Utilize Building Massing to Screen Loading Bays.** Future industrial/logistics developments should be encouraged to have a L-shaped front that minimizes sight lines along access drives and screens loading bays from the public-rights-of-way. If this is not feasible, then the City should ensure such developments have sufficient screening through landscaping.



## PROMOTE LOW-IMPACT DESIGN

All development within the Station Areas will utilize the City's infrastructure and contribute to stormwater runoff. It is important to incorporate best practices for future (re) developments to minimize runoff and the related burden placed on the City's infrastructure. Incorporating green infrastructure strategies can be effective in mitigating stormwater runoff resulted from a development. Example strategies that allow for natural filtration of stormwater include the use of permeable pavers, stormwater planters or rain gardens, tree boxes, and bioswales.

### Crystal Lake Station Area

- Minimize Stormwater Runoff in Parking Lots.** Space is limited in the Crystal Lake Station Area, so mitigating stormwater runoff will mostly be achieved through the use of planters and permeable pavers. Off-street parking lots should incorporate the use of stormwater planters, which involves a cut on the curb to allow run off into the parking lot islands and perimeter landscaping zones for gradual absorption. Parking lot islands and the landscaping zones should have enough trees to further mitigate against runoff. If an off-street parking lot does not have heavy-loaded truck uses, the use of permeable pavers should also be encouraged.

### Pingree Road Station Area

- Low-Impact Development.** Vacant infill sites and the Main Street Crossing site present a significant development opportunity; however, it is critical to conserve natural areas whenever possible. Developments should be clustered together to dedicate contiguous areas to open space and whenever possible, future development should preserve the existing tree canopy. Additionally, areas situated between residential and non-residential land uses should be dedicated to berming or bioswales to function both as a landscaping and stormwater-management elements.
- Naturalized Detention Areas.** There is potential for a regional detention basin to be integrated within the Main Street Crossing site as development occurs. This facility should be naturalized and planted with native vegetation to improve stormwater quality, provide greater habitat benefits, and serve as an amenity to future residents and employees.





*Mugge Building / Built in 1904 / 115-119 N. Main Street*



*Town Hall Building / Built in 1906 / 121 N. Main Street*



*Prickett & Faine Building / Built in 1912 / 71 N. William*



*Warner Building / Built in 1909 / 83 N. Williams*

## HISTORIC PRESERVATION

Historically significant homes and commercial buildings within and surrounding Downtown Crystal Lake are an asset to the community and help define the desirable character of the study area. Historic assets add to the neighborhood's aesthetic quality and contribute to local identity.

### Historic Preservation Commission

The City currently has an Historic Preservation Commission which is an advisory body that determines eligibility of structures be designated as possible landmarks and works with the City in administering the Landmark Program. A landmark designation preserves the structure's exterior facades and the Commission's oversight ensures landmarked properties are properly maintained and not destroyed by demolition, inappropriate alterations, and neglect. The Commission is responsible for reviewing certificates of appropriateness for renovations to the City's historic structures. Every year, the Commission hosts an Annual Heritage Tour that showcases the City's history. Funds raised from this program are used to provide matching grants, which are the Commission's financial assistance tool for landmarked properties.

### Historic Assets in Downtown Crystal Lake

As redevelopment occurs per the Plan's Land Use Plan, the City should continue to work with the Historic Preservation Commission to preserve historically significant buildings. The Historic Preservation Commission has previously inventoried a series of buildings within Downtown Crystal Lake for consideration of a historic district. That inventory has been mapped and can help provide focus in preservation efforts. Although a historic district has not been officially adopted, the City should explore strategies for promoting preservation and leveraging historic assets for economic development.





United States Bank Building / Built in 1911  
(Facade: 1927) / 72 N. Williams



Home State Bank Building / Built in 1916 /  
78 N. Williams



Ballou/Marshall Building / Built in 1882 & 1892 /  
86-90 N. Williams



Bauer Building / Built in 1914 /  
77 Woodstock Street

The Historic Preservation Commission's inventory of historic assets in the Downtown consisted of the two following classifications:

- **Contributing.** Any structure which adds to the historical integrity or architectural qualities that make the potential historic district significant.
- **Not In Present State.** Buildings that do not contribute to a district's historical and/or architectural significance due to poor maintenance or alterations. Such buildings can be restored/rehabilitated to become a contributing building.



**Contributing Buildings**

- |                             |                               |
|-----------------------------|-------------------------------|
| 1 Witte Building            | 12 Jackman Building           |
| 2 Town Hall Building        | 13 Osmun Building             |
| 3 Mugg Building             | 14 Warner Building            |
| 4 E.N. Brown Building       | 15 Osmun Building/Gem Theatre |
| 5 Ballou Building           | 16 Prickett & Paine Building  |
| 6 Kruger Building           | 17 Beatty Building            |
| 7 Train Depot               | 18 Teckler Building           |
| 8 Schroeder Building        | 19 United State Bank Building |
| 9 Bauer Building            | 20 Home State Bank Building   |
| 10 McCollum/Lowell Building |                               |
| 11 Ballou/Marshall Building |                               |

**Contributing Buildings**

- |  |               |
|--|---------------|
| 21 El Tovar Theater/Raue Center for the Arts | 22 Hale House |
|--|---------------|

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CHAPTER 5  
**TRANSPORTATION**

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The Transportation chapter builds upon previous efforts to expand and enhance the existing multi-modal transportation infrastructure, focused on balancing the needs of all users. Recommendations are provided in support of an interconnected system of transportation infrastructure consisting of roads, sidewalks, trails, paths, and public transit to facilitate the safe and efficient movement of vehicles and pedestrians within the City of Crystal Lake and access to both the Crystal Lake and Pingree Road Station Areas. As documented in the Existing Conditions Report (ECR), it is clear from public input, stakeholder input, and the emphasis from previous work that there is a need and desire to improve walkability and pedestrian comfort in both the Crystal Lake and Pingree Road Station Areas. Mobility challenges concerning traffic, congestion, and safety were also raised.

A Complete Streets approach provided the framework for developing mobility recommendations. Streets, sidewalks, and crosswalks should be continuous, well-connected, comfortable, and designed to minimize conflicts with vehicular traffic. A safe and comfortable pedestrian and bicycle network is important to encourage more people to walk and bike around both Station Areas, meaning the ability to travel comfortably, having direct and accessible connections, being visible, having a protected space, and free from conflicts. These characteristics inform the development of Station Area alternatives.

## TRANSPORTATION FRAMEWORK

The Transportation Framework is a toolbox of transportation strategies that can be implemented throughout both Station Areas. Some strategies primarily focus on the public right-of-way, such as shared-use path, while other strategies focus on the private realm, such as access management. The following generally describes what these strategies are. The Transportation chapter's overall and specific strategies for each Station Area describes how these strategies apply to the Crystal Lake and Pingree Road Station Area.

### Transportation Strategies Maps

The small maps accompanying each transportation strategy in the following pages illustrate locations where a given strategy could be implemented. Some transportation strategies apply to both Station Areas while some apply to only one of the two Station Areas. Composite maps summarizing the applicability of all transportation strategies for each station area are located on pages 67 and 71.



## RAILROAD CROSSING IMPROVEMENT

Railroad crossings can present safety issues for pedestrians, particularly those using wheeled devices such as wheelchairs and scooters. All pedestrian railroad crossings should be designed to minimize the time required for pedestrians to cross, with emphasis on avoiding entrapment of pedestrians on or between sets of tracks. Pedestrian safety can be improved by better delineating the pedestrian space through the use of fencing, channelization, swing gates; pedestrian barriers, pavement markings, widened crossings, and refuge areas. Optimally the crossing should be designed so that the pedestrian paths of travel intersect the railroad track at a 90 degree angle.



Source: (top) railtec.illinois.edu; (above) www.rosehillrail.com



## ROUNDBABOUT

A roundabout is a one-way circular intersection in which traffic flows around a central island. Roundabouts can provide operational and safety improvements and can reduce speeds. Studies have shown that roundabouts are safer than conventional intersections controlled by signals or stop signs. Roundabouts require drivers to reduce their speed before entering. Since all traffic is moving in the same direction, it eliminates the need for right angles and left turns. Benefits to pedestrians can include reduced vehicle speeds, reduced number of conflict points, shorter crossing distances, and splitter islands provide a refuge so that the pedestrian crosses one direction of traffic at a time, with the crosswalk is placed one car length back.

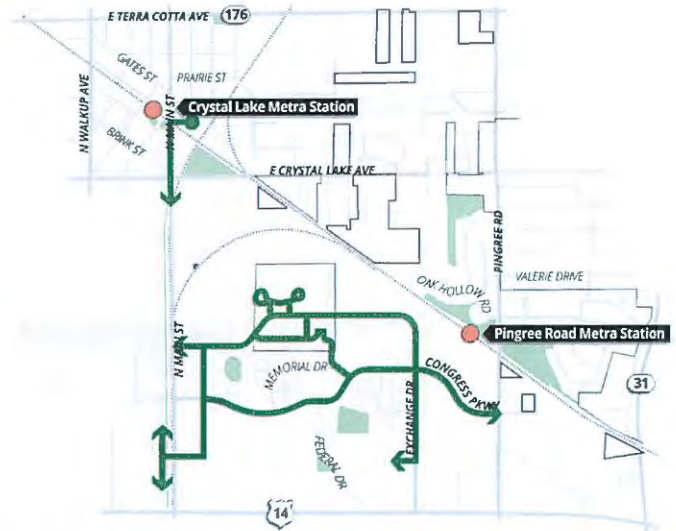


## ACCESS MANAGEMENT (CONSOLIDATE CURB CUTS/ACCESS)

Motorist access across a sidewalk not only degrades the quality of the pedestrian environment, but it also increases the potential for motorist-motorist and motorist-pedestrian conflicts. When providing access to adjacent property, the number of motorist access points across the pedestrian path should be minimized. Commercial driveways generally have higher motorist volumes than other driveway types and have the greatest potential for conflicts between motorists and pedestrians. Limiting and consolidating multiple driveways reduces the number of conflict points and can also redirect motorists to intersections with appropriate control devices.







### ON-STREET PROTECTED BIKE LANE

Protected bike lanes (PBLs) are dedicated bike lanes that physically separate bike riders from vehicular traffic, increasing safety and comfort for those traveling on bikes while having limited negative impact on car and truck traffic. PBLs define and allocate space exclusively for people on bikes – not shared with pedestrians or vehicular traffic. This is accomplished through a variety of physical separation designs between moving motor vehicle traffic and the bike lane such as plastic posts, bollards, curbs, planters, raised bumps or parked cars. In some cases, adding a PBL requires re-allocating street space from cars, such as converting parking or travel lanes.



### SHARED-USE PATH/SIDEPATH

Shared-use paths are like multi use trails, sometimes called sidepaths, alongside the road are completely separated from vehicular lanes and include paved space for both bicyclists and pedestrians. In locations where sidewalks already exist the design would involve widening the sidewalk by a few to several feet. Intersection and crossing improvements for pedestrians and bicyclists should be made wherever a shared used path crosses the roadway. Shared-use paths are lower stress facilities than on-street bike lanes and recommended on roadways where right-of-way is available and the roadway may have higher traffic volumes, vehicle speeds, or wider pavement.







**BIKE BOX**

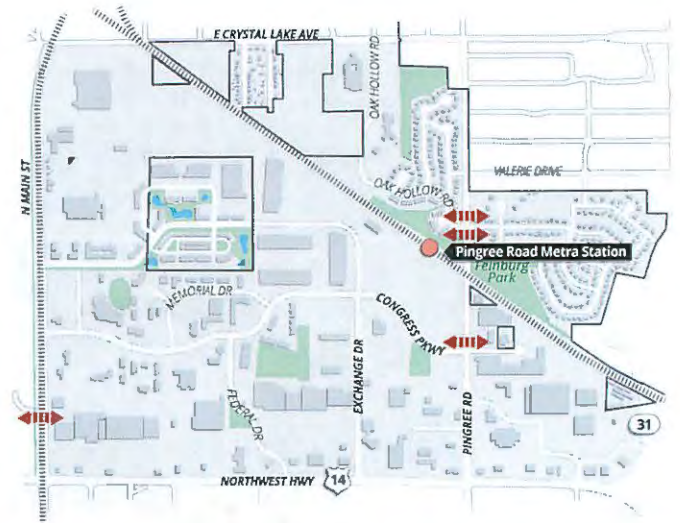
Bike boxes provide priority for bicyclists at signalized intersections. They can reduce signal delay for bicyclist and help prevent 'right-hook' conflicts with turning vehicles at the start of the green signal. Bike boxes can be appropriate at signalized intersections with high volumes of bicycles and/or motor vehicles, especially those where there may be right or left-turning conflicts between bicyclists and motorists, or where there is a desire to better accommodate left turning bicycle traffic.



**MID-BLOCK CROSSING**

Mid-block crossings provide a more direct path for pedestrians and with appropriate design increase safety and visibility of pedestrians. Where there are places pedestrians want to go but are not serviced by a crosswalk. When installed at a mid-block crossing, the island should be supplemented with a marked high-visibility crosswalk along with consideration of advance lighting to provide illumination of the pedestrian.





### CURB EXTENSION

Curb extensions, or "bump outs" extends the curb line into the roadway, increasing the visibility of pedestrians and shortening the crossing distance. Curb extensions minimize pedestrian exposure during crossing by shortening crossing distance and giving pedestrians a better chance to see and be seen before committing to crossing.



### MEDIAN REFUGE ISLANDS

Median refuge islands help improve pedestrian safety by allowing pedestrians to cross one direction of traffic at a time. Refuge islands minimize pedestrian exposure by shortening crossing distance and increasing the number of available gaps for crossing. Active Warning Beacons, such as a Pedestrian Hybrid Beacon (PHB), are user-actuated illuminated devices used to warn and control traffic at an unsignalized location to assist pedestrians in crossing a street or highway at a marked crosswalk. PHBs can be used if gaps in traffic are not adequate to permit pedestrians to cross, or if the speed for vehicles approaching on the major street is too high to permit pedestrians to cross.



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Crystal Lake & Pingree Road Stations Strategic Action Plan  
Transportation





### RAISED CROSSWALKS

Serve as a marked pedestrian crossing and provide stronger visual clues for drivers. These are particularly useful at mid-block crossings. Benefit can include visibility improvements to areas with limited sight distance, enhance pedestrian safety, reduces vehicular speed, and traffic calming. Raised crosswalks should include advance signage and high-visibility markings. Street lighting is important for improved nighttime lighting. Curb extensions would also be appropriate.



### HIGH VISIBILITY CROSSWALKS

High Visibility Crosswalks signal to motorists that they must stop for pedestrians and encourage pedestrians to cross at designated locations.

### PEDESTRIAN SIGNAL TIMING

Use leading pedestrian intervals (LPIs), give people crossing the street a head start before cars are given a green light, typically providing the WALK signal 3-7 seconds before the motorists are allowed to proceed through the intersection. LPIs are particularly helpful for crossing with high volumes of pedestrians or those accommodating school-aged children or persons needing more time to cross the street.





## CRYSTAL LAKE STATION AREA

Crystal Lake Station Area's transportation recommendations are presented below, followed by strategies for specific roadways.

### TRANSPORTATION RECOMMENDATIONS

#### Sidewalk Network

The City's sidewalk network is broad, with many streets having sidewalks on one or both sides of the street, although there are locations that are not connected and in disrepair. Several sidewalk gaps exist that should be addressed to enhance accessibility to Downtown Crystal Lake. Locations are addressed related to each specific roadway.

#### Transportation Plan Map

The Crystal Lake Station Area's Transportation Plan map is located on page 67. This map illustrates the locations where the various transportation recommendations within the Crystal Lake Station Area could be implemented.

#### Access Management

Consolidate and/or eliminate driveways. There are many blocks with multiple driveways, creating conflict between motorists, pedestrians, and bicyclists, particularly the block bounded by Walkup Ave, Grant St, Crystal Lake Ave, and Brink Street. Most of these driveways are located at off-street parking lots.

#### Gap in Prairie Path Trail

Bicycle travel can be an effective travel mode in Downtown Crystal Lake, serving both recreational trips as well as work/school trips, but requires comfortable travel routes along with addressing crossing locations, network gaps and maintenance. A gap in the Prairie Trail Path exists through Downtown, from Main St to Veterans Acre Park, causing a disconnected north-south bike network through Downtown. Although the City and McHenry County Conservation District have discussed routing options for the Prairie Trail through the downtown, there are potential opportunities for expanding bike infrastructure in the downtown.

#### Bicycle Parking

The downtown area would benefit from expanded bike parking stations and covered bike parking, along with shared bikes. Having bicycle parking facilities in Downtown Crystal Lake would also encourage bicyclists to stop and visit Downtown Crystal Lake's businesses and amenities.

#### Transit Service

Mobility recommendations are focused on physical infrastructure, including boarding locations and access to transit service. Pace Route 550 is the primary route operating through the Downtown, connecting to the Crystal Lake Metra Station. Transit stops should be highly visible locations that pedestrians of all abilities can reach easily by means of accessible travel routes, fully accessible to pedestrians in wheelchairs, have paved connections to sidewalks where landscape buffers exist, should not block pedestrian travel on the sidewalk, and have adequate space to operate wheelchair lifts. Other treatments to increase the comfort of transit stops include sufficient lighting, sheltered seating and lean bars, trash receptacles,

bike racks, and transit service information, and wayfinding information.

The Pace and MCRide boarding/alighting area next to the downtown Metra station should improve on overall amenities and ADA accessibility, such as lighting, curb-less stop area, audible signals, buffer for expanded boarding area, detectable warning strips, and signage.

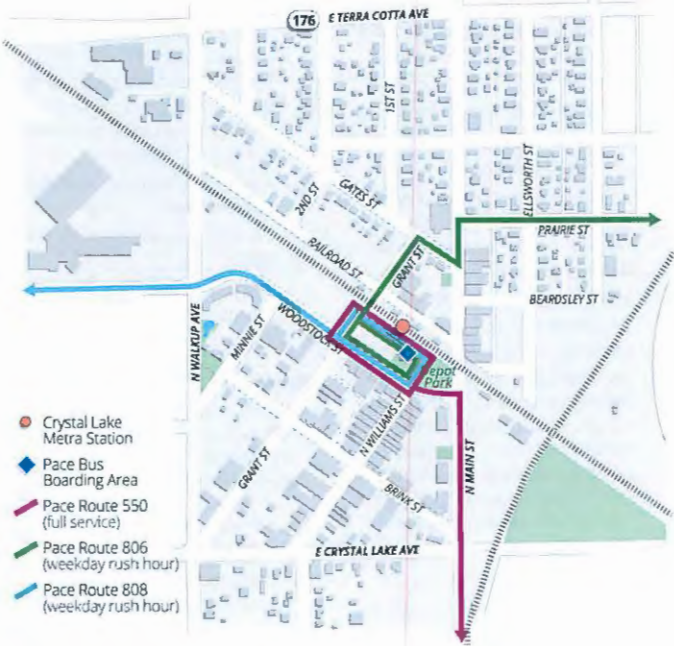
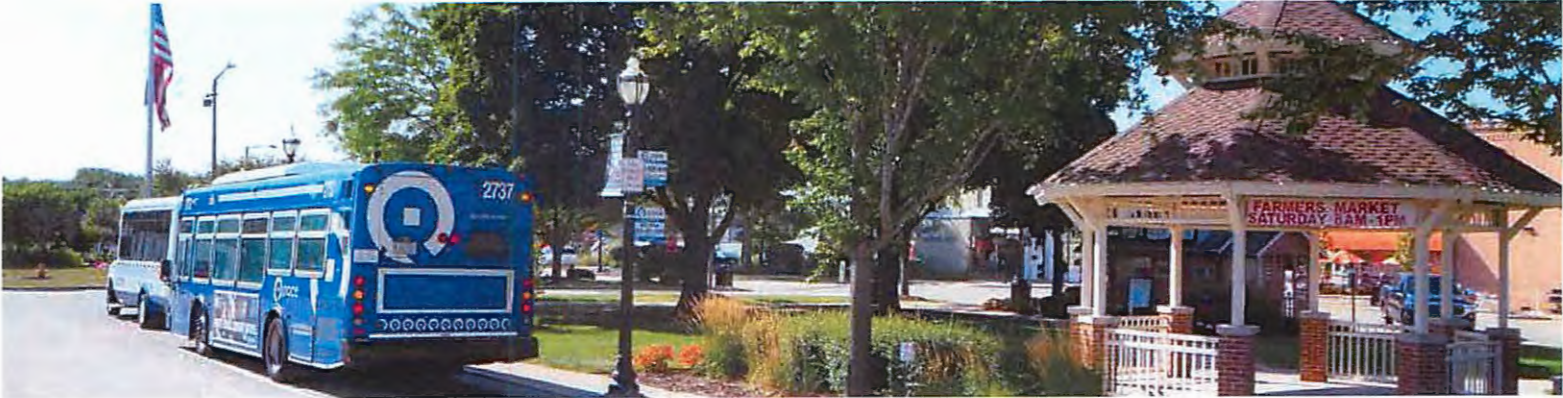
The City should continue to coordinate with Pace and McHenry County implementing a preferred future transit concept plan, as currently being addressed through Pace's strategic plan update, Driving Innovation. Driving Innovation is based on how to provide the most appropriate level of service, or the "right size" of transit using a family of mobility services menu.

New developments should reference Pace's Transit Supportive Design Guidelines.

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Crystal Lake & Pingree Road Stations Strategic Action Plan  
**Transportation**





### Metra Parking Lot

In addressing downtown parking strategically, the City should continue working with Metra to rethink the demand for commuter parking, particularly changes that have occurred during the COVID-19 pandemic. The most dramatic impact of COVID-19 has been on transit use, with the greatest impact on Metra ridership as businesses have yet to return to downtown offices. However, prior to COVID-19, commuter parking was only 62% utilized.

### SPECIFIC STRATEGIES

#### Walkup Avenue

Walkup Avenue is an important north-south roadway at the western edge of the Crystal Lake Station Area. The focus is to improve the roadway's safety through improved access management and enhancing pedestrian and bicycle infrastructure.

- Continue consideration of a roundabout at Walkup Avenue/Grant Street/Crystal Lake Avenue.
- Add on-street protected bike facility between Crystal Lake Avenue and Railroad Street through eliminating the center turn lane and reallocating to on-street bike lanes. A shared-use path should be considered if on-street protected bike lanes are not possible.

- Improve railroad crossing to clearly delineate pedestrian space. Design elements could include fencing, channelization, pavement markings and texturing, refuge areas, and fixed message signs.
- Consolidate bank driveways for Home State Bank by eliminating the in/out drive just south of the Bank drive-through access drive.
- The McHenry County Division of Transportation (MCDOT) has jurisdiction over Walkup Avenue north of IL-176. The City will need to coordinate with MCDOT for any future changes/improvements along the roadway's right-of-way.

### Railroad Street

Railroad Street is a gateway to the downtown, providing multimodal access to the downtown and Metra Station Area. Changing the overall character of Railroad Street between 2nd Avenue and Main Street to a "shared mobility street" would provide not only benefits of identifying the importance of this roadway to all users, but improve accessibility, safety and comfort for pedestrians, bicyclists, and transit riders.

- Add on-street bike lanes.
- Widen the existing sidewalk on north side between Grant Street and Main Street.

- Install a bike station adjacent to the Crystal Lake Metra Station's building at Grant Street – covered bike parking with bike repair, bikeshare, e-scooters. This can potentially be a trailhead for the Prairie Trail.
- Remove the on-street commuter parking between 2nd Avenue and Grant Street.
- Convert angled parking between Grant Street and Main Street into parallel parking, while designating space for shared-ride drop off/pick up.
- Add mid-block raised crossing at the Crystal Lake Metra Station's entrance. Raised crosswalks improves pedestrian safety by forcing drivers to slow down when traversing the crosswalk location.
- It appears that adequate space may be available on the east side of Main Street for a 10-foot shared-use path. The City should complete an engineering assessment of possible design opportunities. A shared-use path only on one side should include signage indicating both pedestrians and bicycles are allowed. Should it be determined that adequate space is not available, signs should be added on Main Street, including:
  - Wayfinding signs to Prairie Trail
  - "Bikes May Use Full Lane"
  - "Share the Road"
- Add a pedestrian/bicycle connector at the northeast corner of the railroad crossing to connect to Metra parking lots and Ellsworth Street.
- Improve delineation of the pedestrian zone at the railroad crossing.
- Add bike boxes at Crystal Lake Ave intersection (intersection now under construction). Consider leading pedestrian interval (LPI) phase, if possible. LPIs give the pedestrian the WALK signal 3-7 seconds before the motorists are allowed to proceed through the intersection.

### Main Street

Transportation issues concerning traffic, congestion, and safety were raised by community stakeholders, noting that traffic on Main Street creates barriers and a lack of comfort for bikes and pedestrians. The intersection of Main Street and Crystal Lake Avenue is a key gateway not just for motor vehicles, but for bikes and peds as well, with the Prairie Trail path transitioning to a signed on-street route east through Crystal Lake Avenue. Strategies are focused on enhancing bike and pedestrian travel and reducing modal conflicts.

### Woodstock Street

Woodstock is another key gateway to the downtown, provides access to the Metra station and commuter parking, and accommodates Pace bus service. Strategies are focused on providing more direct, efficient, and visible pedestrian access routes.

- Add curb extensions (corner bump-outs) at Minnie Street. Curb extensions shorten the crossing distance and allows the pedestrian waiting to cross to be more visible to the approaching driver.
- Add gateway and pedestrian features at Grant Street
- Complete sidewalk gap on south side.
- Add bike boxes on Walkup at Woodstock.

### Brink Street

As an important roadway in the downtown, strategies are focused on reducing modal conflicts and facilitating pedestrian travel.

- Eliminate multiple driveways to parking lots.
- Add a mid-block raised crosswalk on Brink Street between Grant and Williams and between Williams and Main Street



TRANSPORTATION PLAN

# CRYSTAL LAKE STATION AREA



**Legend**

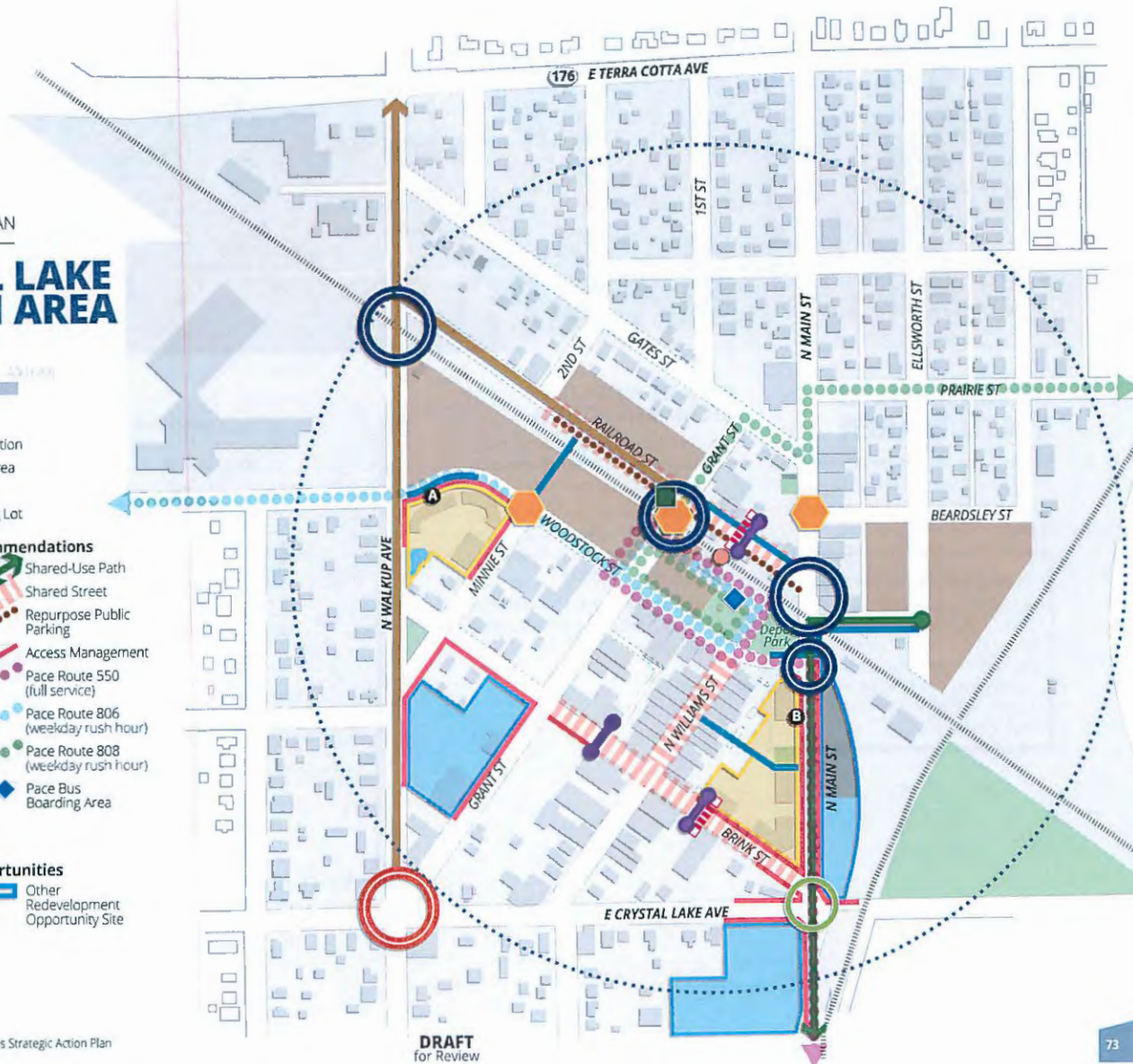
- Crystal Lake Metra Station
- Crystal Lake Station Area
- Railroad
- Existing Metra Parking Lot

**Transportation Recommendations**

- Bike Box
- Roundabout
- Intersection / Railroad Crossing Improvement
- Curb Extension
- Bike Station
- Mid-block Crossing
- Raised Crosswalk
- Pedestrian Connection / Sidewalk Connection
- On-Street Protected Bike Lane
- Shared-Use Path
- Shared Street
- Repurpose Public Parking
- Access Management
- Pace Route 550 (full service)
- Pace Route 806 (weekday rush hour)
- Pace Route 808 (weekday rush hour)
- Pace Bus Boarding Area

**Redevelopment Opportunities**

- Walkup Avenue / Minnie Street / Woodstock Street
- Main Street / Brink Street
- Other Redevelopment Opportunity Site



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## PINGREE ROAD STATION AREA

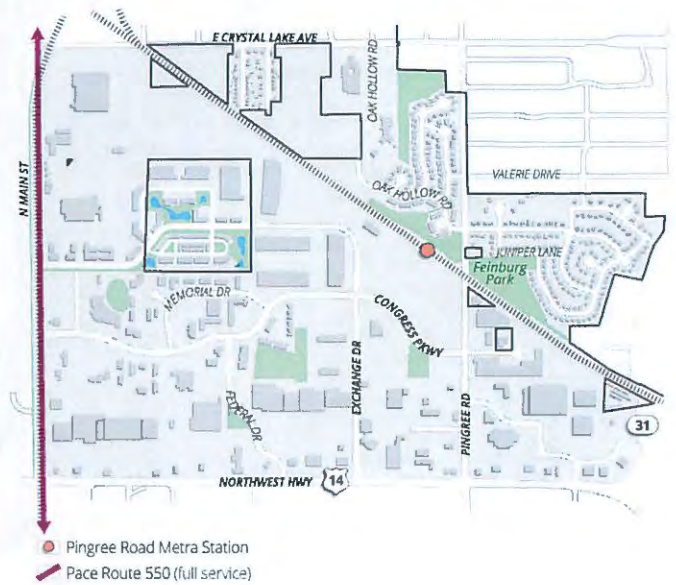
Mobility recommendations addressing the Pingree Road Station Area are presented below, followed by strategies for specific roadways.

### Transportation Plan Map

The Pingree Road Station Area's Transportation Plan map is located on page 71. This map illustrates the locations where the various transportation recommendations within the Pingree Road Station Area could be implemented.

## OVERALL MOBILITY RECOMMENDATIONS

- Complete sidewalks gaps on both sides of the streets throughout the Pingree Road Station Area.
- Add wayfinding/directional signs throughout Station Area identifying directions Prairie Trail, Pingree Road path, and downtown.
- New developments should reference Pace's Transit Supportive Design Guidelines.







## SPECIFIC STRATEGIES

### Congress Parkway

Congress Parkway provides a key access route to Pingree Road Station area for motorists, bicyclists, and pedestrians. This area not only includes the Metra Station, but commercial, office, and industrial destinations as well. While sidewalks exist along Congress Parkway, there are not dedicated bicycle facilities and limited protected crossings. The Strava heat map indicated a high level of bike/ped activity on the roadway, as it provides a key connection from the Prairie Trail. It was also observed that many area employees walk during the day on Congress Parkway, crossing at Exchange Drive.

Strategies include:

- Complete roundabout at Congress Parkway and Exchange Drive and at Congress Parkway and Federal Drive.
- Add shared-use path on both sides of the roadway, connecting to Pingree Road path. Shared-use paths are proposed to enhance the feeling of safety through separation from motor vehicles. Design elements would include signage and green paint at driveway crossings. Additionally, space appears to be available for an off-road path.
- Complete sidewalks gaps on the south side of the roadway.
- Add median refuge island on the west leg of Congress Parkway at Pingree Road.

### Pingree Road Metra Station Pedestrian Tunnel

The tunnel provides a critical connection between residential areas to the north to both the Metra station and destinations along Congress Parkway.

- The need for regular maintenance of the Pingree Road Metra Station's pedestrian tunnel, particularly during the winter, was raised by Stakeholders. The City should continue to work with Metra to maintain the Pingree Road Metra Station's pedestrian tunnel.

### Memorial Drive

Memorial Drive provides access to a number of medical, professional, and commercial uses. Since it is within about a ½ mile distance from the Pingree Road station, opportunity exists to attract reverse commuters.

- Complete sidewalk network along both sides of the street
- Install marked pedestrian routes through surface parking lots to adjacent land uses and buildings.
- Narrow Memorial Drive at Congress Parkway by adding curb extensions



### Exchange Drive

Exchange Drive directly connects the Pingree Road Metra station and US14, linking Metra service to Northwestern Medical Center and numerous commercial uses including the Crystal Point Shopping Center. The residential area north of the Metra station is connected to Exchange Drive via the pedestrian tunnel at the Metra station. Enhancing and expanding bike and pedestrian mobility between the residential area, the Metra station, and commercial uses would include improving the intersection of Congress Parkway and Exchange Drive, along with dedicated bike/ped infrastructure:

- Complete roundabout at Congress Parkway and Exchange Drive.
- Add shared use path on both sides of the roadway, connecting north side (and Main Street Crossing site) to the retail center north of US-14. Shared use paths are proposed to enhance the feeling of safety through separation from motor vehicles. Design elements would include signage and green paint at driveway crossings.

### Pingree Road

Feinburg Park is located on the east side of Pingree Road, north of the railroad tracks and adjacent to residential areas. There is a marked crosswalk across Pingree Road with a Rectangular Flashing Beacon at Grandview Drive. A trail also exists along the southern edge of the park that connects to Pingree Road.

- Add a pedestrian refuge island (median island) on both legs of Pingree Road at Grandview Drive/Oak Hollow Road. Median islands are beneficial on wide multi-lane roadways by allowing pedestrians to cross a two-way street in two stages by finding a gap in one direction, and then stopping on a median island of sufficient width to wait while searching for a gap in the other direction of traffic.

- Complete traffic warrant study to consider upgrading flashing beacon to a Pedestrian Hybrid Beacon (PHB), which is used to warn and control traffic at an unsignalized location to assist pedestrians in crossing a street or highway at a marked crosswalk. PHBs can be used if gaps in traffic are not adequate to permit pedestrians to cross, or if the speed for vehicles approaching on the major street is too high to permit pedestrians to cross.

### Commonwealth Drive

This road provides a direct connection to the Prairie Trail, access to Northwestern Medicine, and access to potential developments at the north end. As an important connector, adding bike and pedestrian infrastructure, intersection improvements, and wayfinding would provide important benefits:

- Add shared-use path on both sides of the roadway, connecting from Main Street/Prairie Trail to the Northwestern Medical Center.
- Add sidewalks on west side of roadway.
- Implement intersection improvements at the intersection of Commonwealth Drive and Main Street.
- Add bike route signage at intersection of Commonwealth Drive and Main Street.
- Stripe Prairie Trail crossing on the intersection of Commonwealth Drive and Main Street with green paint.
- Add median refuge island on the intersection of Commonwealth Drive and Main Street.

TRANSPORTATION PLAN

# PINGREE ROAD STATION AREA



**Legend**

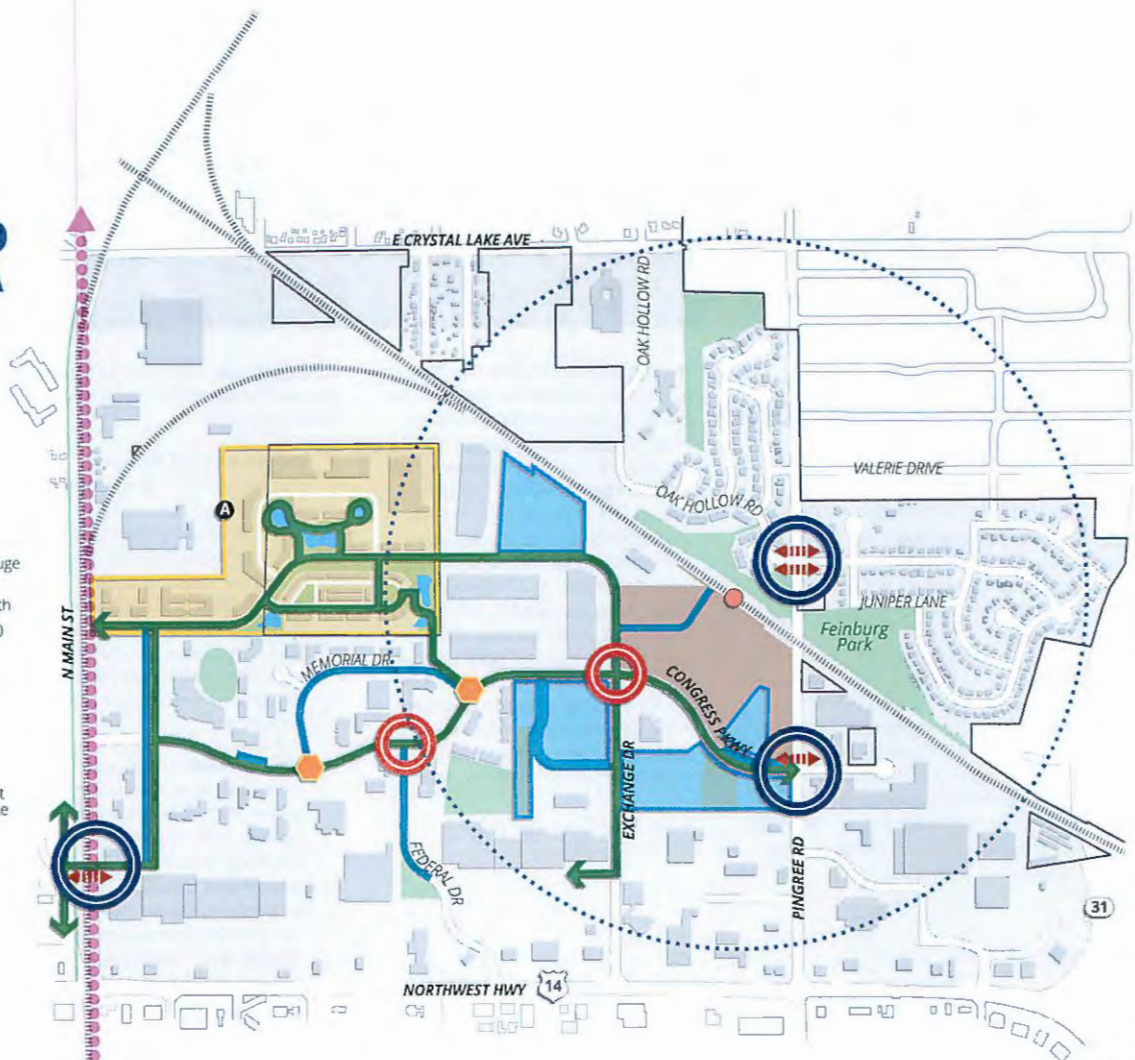
- Pingree Road Metra Station
- Pingree Road Station Area
- Railroad
- Existing Metra Parking Lot

**Transportation Recommendations**

- Roundabout
- Intersection / Railroad Crossing Improvement
- Curb Extension
- Pedestrian Connection/ Sidewalk Connection
- Pedestrian Refuge Island
- Shared-Use Path
- Pace Route 550 (full service)

**Redevelopment Opportunities**

- Main Street Crossing
- Other Redevelopment Opportunity Site





## CITY-WIDE TRANSPORTATION IMPROVEMENT

The Crystal Lake Stations Strategic Action Plan acknowledges the Crystal Lake Transportation Plan (completed by CMAP and the City of Crystal Lake) and builds upon recommendations included in that plan related to the Station Areas (Page 14\*). The Stations Strategic Action Plan also supports the Transportation Plan's vision to increase community safety, reduce congestion, and decrease environmental impact by providing safe and convenient travel along and across streets through a comprehensive, integrated transportation network for pedestrians, bicyclists, public transportation riders and drivers, and people of all ages and abilities. (Page 31\*)

The Crystal Lake Transportation Plan provides city-wide recommendations for improving the transportation network based upon community context, travel behavior, infrastructure conditions, and current design standards. The Stations Strategic Action Plan builds upon this previous effort with recommendations and strategies that are not only specific to the Crystal Lake and Pingree Road Station Areas, but also as a part of and relating to a greater transportation network. Several of the Crystal Lake Transportation Plan's recommendations are also included as recommendations in the Stations Strategic Action Plan with additional details or refinements that relate specifically to the Station Areas.

*\*Page references to the Crystal Lake Transportation Plan*

The Crystal Lake Transportation Plan generally includes the following types of recommendations:

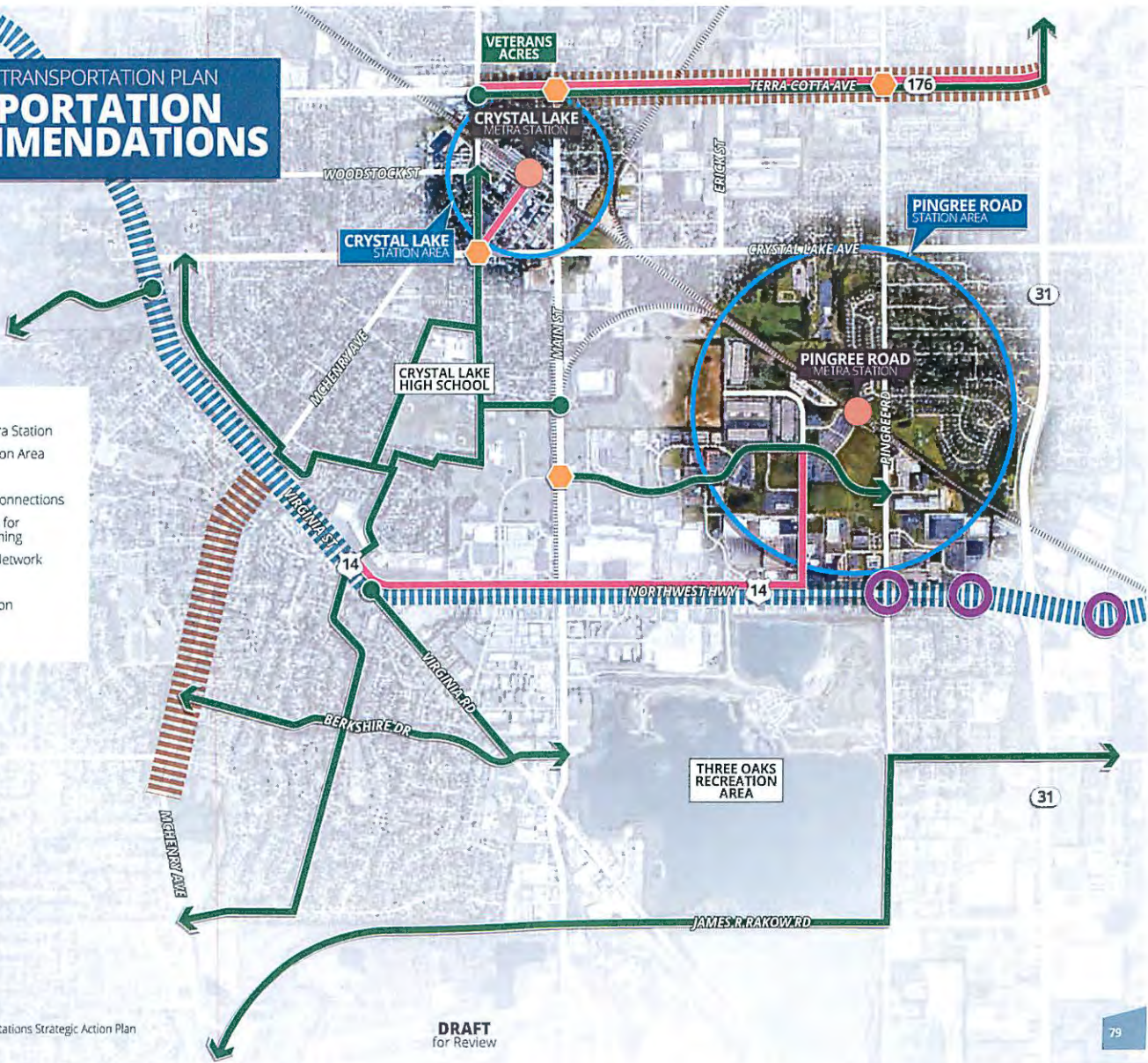
- Future Bikeway Network.** This focuses on creating a safe and interconnected system of bicycle facilities that link to the regional greenway system. The Stations Strategic Action Plan's recommendations support and relate to this city-wide bicycle network addressing connectivity, network gaps, intersection improvements, reducing modal conflicts, and wayfinding.
- New Sidewalk.** This focuses on creating a safe and convenient system which connects residents with key community destinations, such as schools, shopping, employment, public transit, parks, and community services. The Stations Strategic Action Plan's recommendations support this with specific strategies aimed at reducing modal conflicts, addressing safety concerns, improving comfort and visibility, and providing options for direct pedestrian connections.
- Vehicle Network Improvements including new/improved connections, traffic signal optimization, and future roadway studies.** These focus on creating an efficient and integrated system that improves traffic flow along major thoroughfares, balances competing transportation modes, and decreases environmental impact. The Stations Strategic Action Plan's recommendations, while more focused in scope addressing travel by bus, bike, and walking, acknowledges the need for enhancing/coordinating traffic signals along major corridors, providing better access management, and improving intersections create a safer multi-modal network that connects the various areas and destinations in the City.



# CRYSTAL LAKE TRANSPORTATION PLAN TRANSPORTATION RECOMMENDATIONS

## Legend

-  Crystal Lake Metra Station
-  Crystal Lake Station Area
-  Railroad
-  New/Improved Connections
-  Key Intersections for Future Programming
-  Future Bikeway Network
-  New Sidewalks
-  Signal Optimization
-  Roadway Study



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CHAPTER 6

# **ILLUSTRATIVE REDEVELOPMENT CONCEPTS**

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## ILLUSTRATIVE REDEVELOPMENT CONCEPTS

Building on the Plan's vision, goals, objectives, and key recommendations, more detailed visualizations were developed for the three illustrative redevelopment concepts in the Crystal Lake and Pingree Road Station Areas. These sites were selected because they have the greatest (re)development potential and their transformation could significantly contribute to both Station Areas in achieving their vision. The visualizations demonstrate development potential and highlight key concepts and strategies for land use and development, access and mobility, and other improvements for these important sites. The City can also utilize these visualizations as a marketing tool to attract development interest at these sites. The three illustrative redevelopment concepts are:

- A** Walkup Avenue/Minnie Street/Woodstock Street
- B** Main Street/Brink Street
- C** Main Street Crossing

The following section describes each site's desired character and intensity of development, provides example imagery of desirable features, and illustrates a potential use program supported by the Plan.





## WALKUP AVENUE/ MINNIE STREET/ WOODSTOCK STREET

This site will be instrumental in contributing a positive first impression to residents and visitors entering Downtown Crystal Lake from the northwest. The goal of this development concept is to provide greater housing choice at transit-supportive densities while complementing while serving as a transition between the core of Downtown Crystal Lake and the residential neighborhood to the west. Rear-loaded townhomes are shown fronting North Walkup Avenue and Woodstock Street to form a solid streetwall. These townhomes would be similar to "brownstones" in terms of scale and façade design and articulation. A multifamily building is situated at the rear of the site with an internal parking lot that is mostly screened from the three surrounding roadways. The rear of the site is reserved for a stormwater detention facility to mitigate runoff. Main access into the site will be from Minnie Street, while North Walkup Avenue would have right-in/right-out access. Landscaping is established at the rear to screen the site from adjacent buildings.



Character image: (Top) Multi-family building; (Bottom-left) Facade and roofline articulation in townhomes; (Bottom-right) 'Brownstone'-theme design on townhomes.

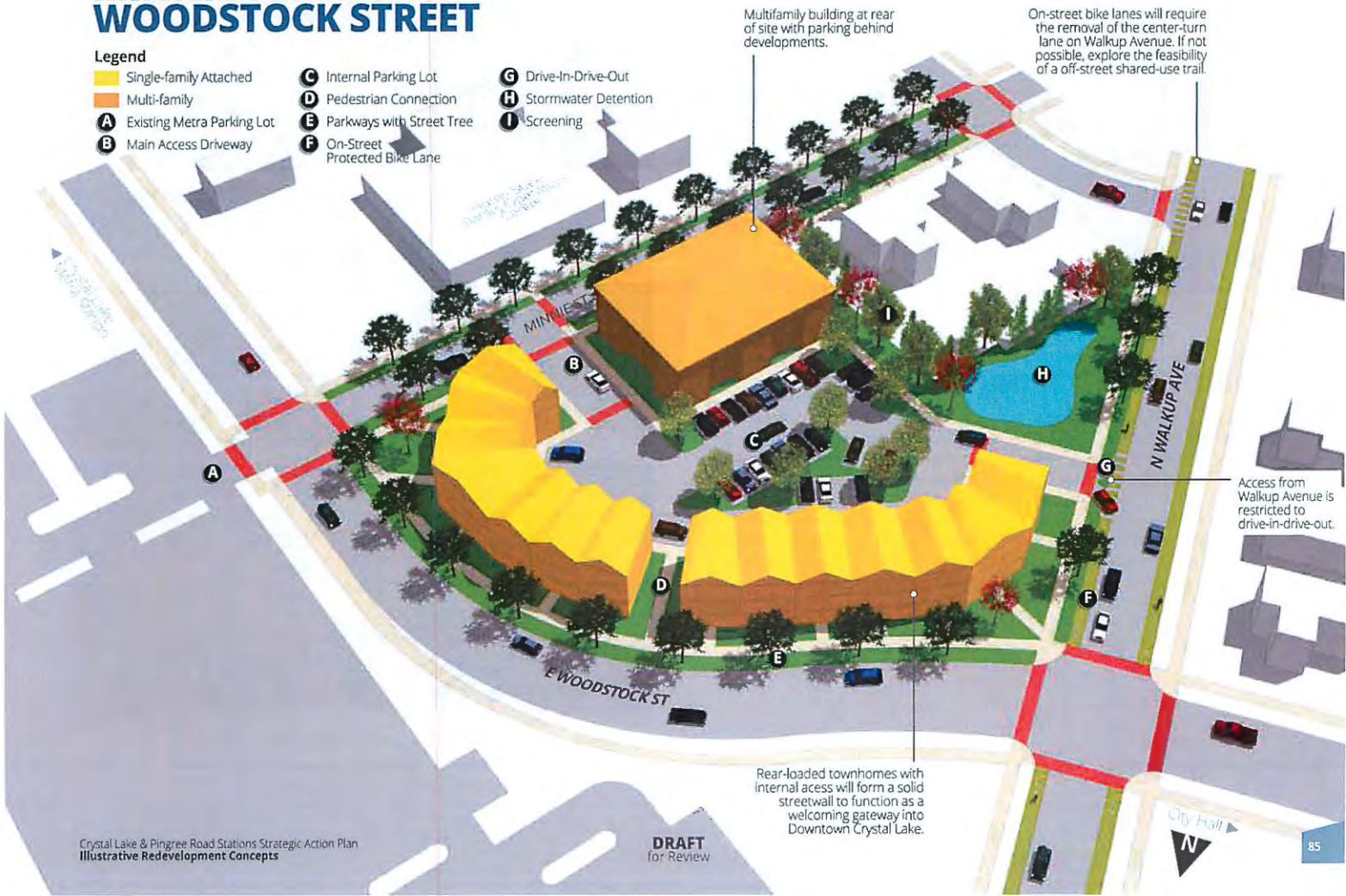


ILLUSTRATIVE REDEVELOPMENT CONCEPT

# WALKUP AVENUE/ MINNIE STREET/ WOODSTOCK STREET

**Legend**

- Single-family Attached
- Multi-family
- A** Existing Metra Parking Lot
- B** Main Access Driveway
- C** Internal Parking Lot
- D** Pedestrian Connection
- E** Parkways with Street Tree
- F** On-Street Protected Bike Lane
- G** Drive-In-Drive-Out
- H** Stormwater Detention
- I** Screening







## MAIN STREET/BRINK STREET

This site is intended to transform an underutilized site in the heart of Downtown Crystal Lake with several mid-rise mixed-use buildings that expand the Downtown's inventory of commercial space while increasing the household base. The site features an approximately 16-foot grade change between Woodstock Street and Brink Street. This provides an opportunity for a building at Brink Street and N Main Street, which is at the lower elevation point, to have additional stories while still conforming to the scale and character of buildings on nearby Williams Street. Retaining walls may be necessary to reconcile the site's downslope and flat topography of N William Street's rear alley. A central open space is proposed on North Main Street with a direct pedestrian connection to the existing alleyway on North Williams Street. Full access into the site will be provided at Brink Street, while it is anticipated and access from North Main Street would require right-in/right-out access drives due to proximity to existing intersections.



Character Image: (Top & Bottom) Mixed-use building; (Center-left) Outdoor dining space; (Center-right) Public Open Space.



ILLUSTRATIVE REDEVELOPMENT CONCEPT

# MAIN STREET/BRINK STREET



Acquiring the existing buildings behind the businesses on North Williams Street can provide additional space for parking spaces.

Create a pedestrian connection between the existing alleyway on North Williams Street and the central open space.

Given the site has a grade change, this building, at the lowest elevation point, can have additional stories while complementing with the surrounding area's building scale and height.

Leverage opportunities to create outdoor dining spaces to generate additional 'Third Places' in Downtown Crystal Lake.

A central open space will function as a focal point in this site with pedestrian access from the existing alleyway on North Williams Street.

**Legend**

- Multi-family
- Commercial
- A** Gateway Signage
- B** Outdoor Dining/Patio Area
- C** Drive-In-Drive-Out Access
- D** Streetscaping Elements
- E** Shared-Use Trail
- F** Central Open Space
- G** Pedestrian Connection to N Williams St
- H** Existing Alley
- I** Retaining Wall to Accommodate Grade Change
- J** Internal Parking Lot
- K** Mid-Block Crossing





## MAIN STREET CROSSING

The 62-acre Main Street Crossing site represents a significant development opportunity for the Pingree Road Station Area. Given that approximate half of this site is unincorporated, annexation and rezoning will be needed. The Plan envisions extending Exchange Drive west to connect N Main Street and Commonwealth Drive. The area fronting North Main Street will primarily consist of commercial outlots with flex and senior living facilities to the east. North of the Exchange Drive extension will be mostly flex uses and industrial/logistics at the rear with a potential rail spur to serve larger industrial users. The buildings are sited to preserve the existing tree canopy and accommodate an interconnected series of naturalized stormwater detention ponds that will also function as a central open space amenity. South of the Exchange Drive extension is a new residential neighborhood with rear-loaded townhomes and multifamily buildings. The buildings are clustered together to dedicate greater space for natural areas and stormwater detention ponds. A large central berm is included to screen the new housing from non-residential uses to the north. If desired, the berm is wide enough to accommodate another row of townhomes, however, this will require additional landscaping along the Exchange Drive extension to minimize land use conflicts. Trails and central open spaces are provided throughout the site to create additional third places and improve access between this area and the surrounding area including the Pingree Road Metra Station.



*Character Image: (Top) Flex-type building; (Center-left) L-shaped building to screen loading bays; (Center-right) Stormwater detention system as an attractive and central feature of an existing business park; (Bottom) Outdoor gathering space adjacent to developments.*

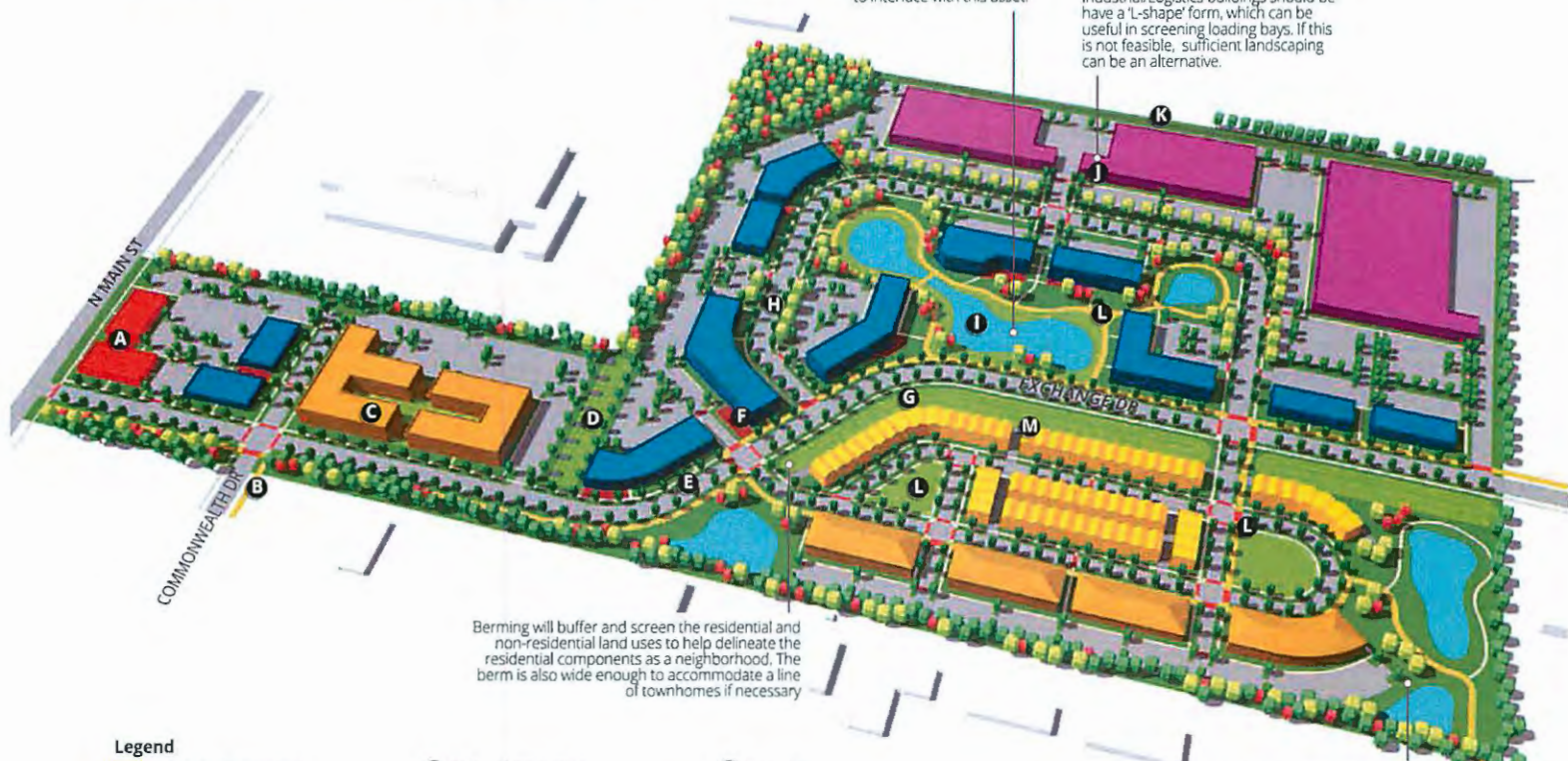


ILLUSTRATIVE REDEVELOPMENT CONCEPT

# MAIN STREET CROSSING

The central stormwater detention mitigates runoff while acting as a central open space where the surrounding flex uses should orient to. An accessible network of trails is needed to provide opportunities for people to interface with this asset.

Industrial/Logistics buildings should be have a 'L-shape' form, which can be useful in screening loading bays. If this is not feasible, sufficient landscaping can be an alternative.



Berms will buffer and screen the residential and non-residential land uses to help delineate the residential components as a neighborhood. The berm is also wide enough to accommodate a line of townhomes if necessary

Developments are clustered together to minimize impervious surface and conserve open space as much as possible.

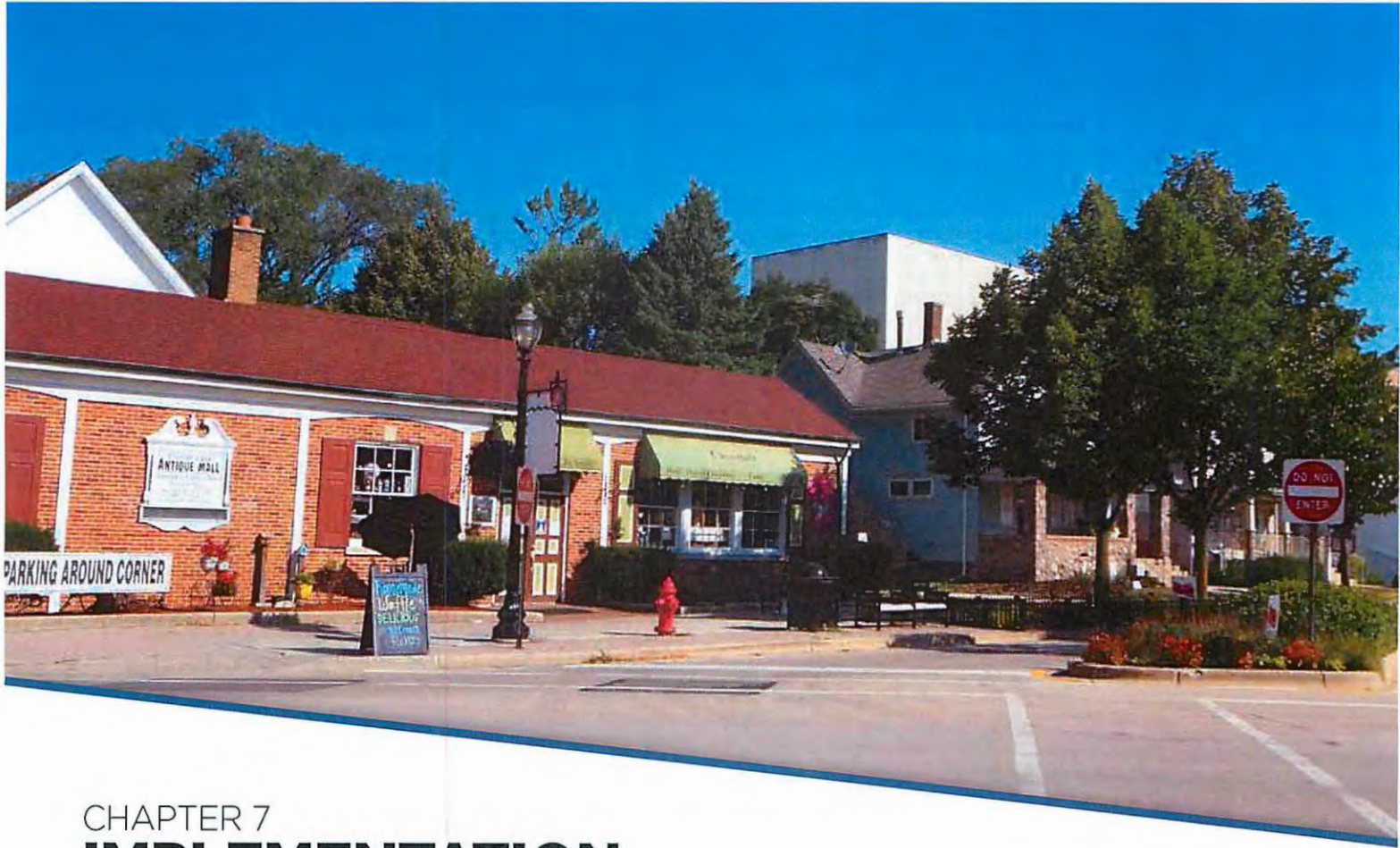
**Legend**

- Single-Family Attached
- Multi-family
- Commercial
- Flex
- Industrial/Logistics
- Commercial Outlot
- Shared-Use Trail

- C** Senior Living Facility
- D** Bioswale/Green Infrastructure Element
- E** Parkway Buffer with Street Tree
- F** Outdoor Dining/Patio Area
- G** Berm
- H** Flex/Industrial/Logistics Ring Road
- I** Stormwater Detention
- J** Screening
- K** Rail Spur Option
- L** Central Open Space
- M** Rear Alley

**DRAFT**  
for Review





## CHAPTER 7 **IMPLEMENTATION**

**DRAFT**  
for Review

The Plan provides policies and recommendations the City should undertake over the next 15 to 20 years to support the vision and goals for the Crystal Lake and Pingree Road Station Areas. Close coordination and participation of local departments, RTA, Metra, Pace, McHenry Department of Transportation (MCDOT), the business community, property owners, developers, and residents will be crucial in the success of the Plan's implementation. This chapter outlines best practices on how to utilize the Plan as well as potential funding sources to consider. The chapter concludes with an Implementation Action Matrix, which identifies Station Area applicability, timeframe, and potential partnerships and funding sources for all Plan recommendations.

## REGULAR USE OF THE STATIONS STRATEGIC ACTION PLAN

The Crystal Lake and Pingree Road Stations Strategic Action Plan should be used as the official policy guide for planning and development decisions made by the City Staff, City Council, the Planning and Zoning Commission, the Economic Development Committee, and other boards and commissions. The Plan should act as a primary reference in evaluating projects and planning initiatives, reviewing development proposals, and prioritizing public expenditures in and around both the Crystal Lake and Pingree Road Station Areas. Referencing the Plan will ensure future planning efforts align with both Station Areas' long-term vision and goals. Transit agencies and developers should also be encouraged to utilize the Station Strategic Action Plan when considering new projects, transit service updates, and programming within their parameters. To promote regular use of the Plan, the City should:

- Publish the Plan document on the City's website and make it available as hard copies in the City Hall and City's Public Library for public access.
- Utilize the Plan in the development review process to ensure future (re) developments in both Station Areas align with the Station Areas' vision and recommendations.
- Use this Plan as a reference document to coordinate with Metra and potential developers about future and possible (re)development opportunities surrounding the two Metra Stations.
- Meet with the City's key department heads, officials, and commissions to explain the Plan's purpose, importance and benefits.

## REVIEWING AND UPDATING THE PLAN

The Stations Strategic Action Plan is not a static document and must be able to evolve and change over time. For example, if new issues arise that are beyond the Plan's current scope or transit agencies update facilities and services, the Plan should be revised and updated accordingly.

The City should undertake a complete a review and update of the Plan every five years, and intermittent evaluations every two to three years to identify smaller changes. The City should maintain a public list of potential amendments, issues, or needs. Ideally, these reviews should coincide with the preparation and adoption of the City's budget and Capital Improvement Program. In this manner, recommendations or changes relating to capital improvements or other programs can be considered as part of the commitments for the upcoming fiscal year. The City should also coordinate and assist with the City Council in the Plan amendment process, if needed. Routine examination of the Plan will help ensure that the planning program remains relevant to community needs and aspirations.

## REVIEW OF ZONING AND DEVELOPMENT CONTROLS

The City's Unified Development Ordinance is the most direct tool in implementing the Stations Strategic Action Plan's recommendations. They guide the nature and design of development throughout both Station Areas. All development controls should be consistent with and complement the Stations Strategic Action Plan's vision, goals, and recommendations. The City may have existing ordinances that contradict new or revised policies or prohibit desired improvements. The City should review all development regulations and amend them as necessary to ensure they align with the recommendations of the Stations Strategic Action Plan.



The following is a list of potential amendments to the City's zoning and development controls (these are also listed in the Implementation Action Matrix):

- Consider the creation of a Transition Urban zoning district to accommodate transitional land uses to help create a buffer between Downtown Crystal Lake and the surrounding neighborhoods.
- After incorporating the Main Street Crossing site into the City, rezone the site to support the Plan's proposed land uses in that area.
- Consider requiring a Special Use process for single-family attached and multi-family uses between Walkup Avenue, Grant Street, and Woodstock Street in Downtown Crystal Lake to create some flexibility in the development approval process while retaining the City's ability to review the developments' proposed site design and program
- Require PUDs for the Main Street/Brink Street and Main Street Crossing redevelopment opportunity sites to provide greater flexibility in buildings' bulk, height, and setback requirements.
- Consider rezoning the identified redevelopment opportunity sites, except Main Street Crossing, into M-Manufacturing to accommodate both office and high-quality business park (flex) uses.
- Continue to allow non-traditional uses in industrial buildings, such as wellness clinics, indoor sports center, and fitness centers, in the O-Office, M-L Manufacturing Limited, and M-Manufacturing zoning districts, to improve industrial land use's resiliency against changing market trend.

## DEVELOPER-SOLICITATION PROCESS

The Stations Strategic Action Plan has identified a series of redevelopment opportunity sites in the Crystal Lake and Pingree Road Station Area. These sites, when redeveloped, have the potential to positively influence its surrounding character and help the Station Area better achieve its vision. The City will need a strategy to attract and solicit developers to initiate redevelopment efforts into these sites. The following outlines the recommended steps associated with an RFP/Q process to solicit development interest related to key sites within the two Station Areas. It should be noted that this process can be followed whether or not the City has complete or partial site control, as long as the property owner is supportive and willing to partner in a developer-solicitation process.

### 1. DEVELOPER IDENTIFICATION AND PRE-QUALIFICATION

Based on the confirmed development program, economic and market parameters, proposed zoning amendment(s), and finance strategy, we recommend that the City formulate a developer recruitment strategy and initiate a pre-qualification rubric based on the City's required parameters. We anticipate that the pre-qualification process would include:

- Identifying potential developers, developer teams, or joint venture partners;
- Evaluating options for developers specializing in specific products (e.g., affordable housing, retail, medical office, etc.);
- Preparing a request-for-qualifications (RFQ) document;
- Soliciting and reviewing pre-qualification information
- Recommending to the City a short-list of pre-qualified developers developer teams, or joint-venture partners; and
- Seeking the property owner's support to initiate this process.



## 2. DEVELOPER PROSPECTUS

Using information developed during the pre-qualification process, we recommend preparing a detailed developer prospectus, including, as appropriate:

- The plan document and other relevant planning studies;
- Site surveys;
- Environmental and soils information;
- Property ownership information;
- Development concepts and design guidelines;
- Market analysis information;
- Financial projections;
- Opportunities for developer incentives and public-private financing;
- Development rights per proposed zoning amendment(s);
- Responsibilities of the developer(s);
- Required financial parameters (e.g., land price and other financial considerations)
- Proposed process and timeline; and
- Coordination with the property owner.

The developer prospectus will then be issued to the short-listed developers and developer teams to solicit formal development proposals.

## 3. PROPOSAL REVIEW AND DEVELOPER SELECTION

Finally, we recommend that the City conduct a comprehensive review of developer proposal submittals and recommend a developer or developer team with whom to proceed to the negotiations phase of the project. Proposals should be reviewed with a specific focus on:

- Proposed project plan;
- Financial feasibility;
- Land price and other financial considerations;
- Adherence to Stations Strategic Action Plan's vision, goals, objectives, and recommendations;
- Aesthetic quality;
- Developer experience
- Developer financial strength; and
- Coordination with the property owner.

After the proposals have been reviewed, the City can hold interviews with a short list of developers or developer teams. After interviews are completed, City staff and other officials can make a recommendation to the City Council for a preferred developer or developer team.

## POTENTIAL FUNDING SOURCES

The following is a review of potential funding sources that the City and its partners can pursue to support the implementation of the Plan. It is important to note that funding sources are subject to change over time. As such, the City should continue to research and monitor grants, funding agencies, and programs to identify deadlines, funding specifications, and new opportunities as they become available. These programs are organized by funding categories, which include:

- General Economic Development
- Transportation and Infrastructure
- Parks, trails, and Open Spaces

### GENERAL ECONOMIC DEVELOPMENT

#### Capital Improvement Program (CIP)

The Capital Improvement Program (CIP) prioritizes and schedules public improvement projects within the City of Crystal Lake. CIPs provide detail for all planned public projects within a community, offering information for funding, priority, time frame, phases, and other details. Typically, CIPs are completed for five years and updated annually to reflect new priorities and completed projects.

As planned projects are completed and new funding sources form, the CIP should be updated to include new projects, cost estimates, and priorities, guided by the Stations Strategic Action Plan. As financial resources in the City will always be limited and public dollars must be spent wisely, aligning the CIP with the Stations Strategic Action Plan will ensure completion of the most desirable public improvements and help keep public improvement projects within the City's budget.

#### Tax Increment Financing (TIF)

The City has established three TIF Districts. Among the three, only the Crystal Lake Avenue and Main Street TIF District is located within the Crystal Lake Station Area. This TIF District generally consists of 18 parcels at the southwest (Hines Lumber site), southeast, and northeast of the intersection of Crystal Lake Avenue and Main Street. The Vulcan Lakes TIF District is located south of the Pingree Road Station Area and generally covers the 22 parcels that constitute the Three Oaks Recreational Area. The third TIF District is the Virginia Street Corridor and is situated away from both Station Areas.

One of the primary purposes of TIF funding is to incentivize and attract desired development within key commercial areas. TIF dollars can be used for such things as infrastructure, streetscaping, public improvements, land assemblage, and offsetting the cost of development.

TIF utilizes future increases in property tax revenues generated within a designated area or district, to pay for improvements and further incentivize continued reinvestment. As the Equalized Assessed Value (EAV) of properties within a TIF District increases, the incremental growth in property tax over the base year that the TIF was established, is reinvested in that area. Local officials may then issue bonds or undertake other financial obligations based on the growth in new revenue. The maximum life of a TIF district in the State of Illinois is 23 years, although a district can be extended beyond that horizon to 35 years through authorization from the State Legislature.

The City needs to be judicious in how funds are allocated to ensure that catalyst projects receive priority. Each funding request should be carefully evaluated to ensure that it is in keeping with the vision for the City.

### **Illinois Angel Investment Credit Program (IAICP)**

The Illinois Angel Investment Credit Program encourages investment into early-stage, innovative businesses within the State of Illinois by offering tax credits to qualifying investors. The credits equal to 25 percent of the claimant's investment made directly in a new business. The tax credit may not exceed the taxpayer's Illinois income tax liability for the taxable year, but the credit may be carried forward for up to five years following the excess credit year. Tax credits are allocated on a quarterly basis throughout the year and are awarded on a first-come, first-served basis. The investments prompted by this program provide new businesses with critical funds to support their growth and success.

### **Revolving Fund Program (RFP)**

A revolving loan fund is administered to provide financial support and assistance to new or expanding businesses, funded through the Illinois Department of Commerce and Economic Opportunity. The borrower may use the low-interest loan for construction, property rehabilitation/improvements, and land acquisition, among other things.

### **Business Districts (BDD)**

As authorized by Division 74.3 of the Municipal Code of the State of Illinois, a municipality may designate, after public hearings, an area as a Business Development District (BDD). A BDD would allow the City to levy up to an additional one percent retailer's occupation tax, one percent hotel tax, and one percent sales tax within a designated district. Similar to a TIF district, a BDD has a maximum life of 23 years. BDD legislation also permits municipalities to utilize tax revenue growth that has been generated by BDD properties to fund improvements in the district.

- Business district designation empowers a municipality to carry out a business district development or redevelopment plan through the following actions:
- Acquire all development and redevelopment proposals
- Acquire, manage, convey, or otherwise dispose of real and personal property acquired according to the provisions of a development or redevelopment plan
- Apply for and accept capital grants and loans from the federal government and the State of Illinois for business district development and redevelopment
- Borrow funds as it may be deemed necessary for business district development and redevelopment, and in this connection, issue such obligation or revenue bonds as it shall be deemed necessary, subject to applicable statutory limitations.

- Enter into contracts with any public or private agency or person
- Sell, lease, trade, or improve such real property as may be acquired in connection with business district development and redevelopment plans
- Expend such public funds as may be necessary for the planning, execution, and implementation of the business district plans
- Create a Business District Development and Redevelopment Commission to act as an agent for the municipality for business district development and redevelopment

BDD funds can be used for infrastructure improvements, public improvements, site acquisition, and land assemblage and could be applicable in the identified subareas, or emerging business and industrial parks in the community. Given the limited amount of funds that a BDD is capable of generating, compared to a TIF district, BDD is best suited for funding small scale improvements and property maintenance programs.

### **Property Tax Abatement (TA)**

A property tax abatement is a versatile tool that can be applied to address a wide range of community issues. Property tax abatements are typically used as an incentive to attract business and revitalize the local economy. In the State of Illinois, municipalities and other taxing districts can abate any portion of the tax that they individually levy on a property. The period of tax abatement on a given property can be no longer than 10 years and the total combined sum of abated taxes for all taxing districts cannot exceed \$4 million over that period. A taxing district can administer the abatement by one of two methods: 1) lowering the tax rate; or 2) initiating a property tax freeze where the property is taxed based on a pre-development assessed value.

In some circumstances, municipalities can also petition the County to lower a property's assessment. For example, a commercial property could be assessed at a percentage equivalent to that of a residential property. This is an effective means of lowering a property tax bill; however, it should be noted that this method impacts all taxing districts and not just the district making the request.



### Payment in Lieu of Taxes (PILOT)

Payment in Lieu of Taxes (PILOT) is a tool similar to tax abatement. The City can use PILOT to reduce the property tax burden of a business for a predetermined period. In this instance, the City and property owners will agree to the annual payment of a set fee in place of property taxes. Payments are generally made in the form of a fixed sum, but they may also be paid as a percentage of the income generated by a property.

PILOT can also be a means of reducing the fiscal impact on the City of a nonprofit, institutional use, or other non-taxpaying entity location on a key site. While such uses can be desirable as activity generators, they can also negatively impact municipal services because they do not pay taxes. Provisions can be made to offset that negative impact by allowing the City to be compensated for at least a portion of the revenue that would otherwise be collected in the form of property tax.

### On-Site and Façade Improvement Programs (FIP)

An **On-Site Improvement Program** assists property owners in upgrading their existing parking lots and installing onsite landscaping. The program would apply to such things as improvements to surface parking areas, privately owned open space, and other areas not directly related to façade features. This could function separately or in conjunction with a Façade Improvement Program.

A **Façade Improvement Program** encourages projects that contribute to the economic revitalization and character of an area by providing financial and technical assistance for façade improvements. Building façades, both individually and collectively, create a strong first impression of an area. By implementing a Façade Improvement Program, current property owners are provided an opportunity to improve their outdated or failing structures without having to relocate. The City currently has a **Retailer Façade and Commercial Tenant Improvement Grant Program** that resembles this funding concept.

### Enhanced Sales Tax Incentive Program (ESTIP)

This program is intended stimulate recruitment, retention, and expansion of sales-tax-generating businesses in the City. Applicants (new retailers, existing and new automobile dealerships) can apply to the City for this program. The City would provide economic incentives to the applicants' improvement projects based on the program's stipulated minimum project costs and annual taxable sales. These improvements may include, but not limited to public improvements, such as sidewalks, landscaping, occupancy of vacant space, and public art. It's envisioned that these improvements would help improve the businesses' taxable sales, which would subsequently increase the sales tax collected by the City. This would enable to the City to better maintain and expand its municipal services to its residents and businesses.

## TRANSPORTATION AND INFRASTRUCTURE

### Congestion Mitigation and Air Quality (CMAQ)

The CMAQ program is designed to fund several types of surface transportation projects which improve air quality and mitigate congestion. Northeastern Illinois receives CMAQ funding because the region does not meet federal air quality standards for ozone. For the CMAQ program, eligible sponsors include any state agency or unit of government having the authority to levy taxes and those agencies authorized to receive Federal Transit Administration (FTA) Section 5307 funding. Sponsors include but are not limited to counties, municipalities, townships, park districts, forest preserve districts, and transit agencies. Private for-profit and non-profit organizations may submit proposals but are required to partner with a public sponsor that meets the previously stated conditions.

Projects are eligible for CMAQ funding if they provide an air quality or congestion reduction benefit without increasing single occupancy vehicle capacity and do not simply maintain an existing facility. Eligible Projects reduce congestion or encourage people to shift trips to less-polluting modes, such as transit improvements, traffic flow improvements and bicycle/pedestrian projects. The federal participation is 80 percent with a 20 percent local match. Phase I (preliminary) engineering is the responsibility of the project sponsor to complete without CMAQ funding. With limited exceptions, all other phases -- including phase II engineering, right-of-way acquisition, construction (including construction engineering), and implementation -- are eligible for CMAQ funding.

### **Illinois Transportation Enhancement Program (ITEP)**

The Illinois Transportation Enhancement Program, or ITEP, is a competitive grant program administered by the Illinois Department of Transportation (IDOT) for projects that enhance pedestrian and bicycle facilities. ITEP funds can also be used for streetscape beautification and other projects that enhance the historic, aesthetic, or environmental impact of transportation infrastructure. Local governments and non-profits are eligible for ITEP grants. Funding reimbursement is available for up to 50% of the cost for right-of-way and 80% for preliminary and final engineering and construction costs. The Illinois 2019 capital plan, for the first time ever, included dedicated funding for walking and biking projects. This state funding is now being distributed through ITEP, which was previously solely federally funded.

To advance transportation equity, the bill designates that at least 25 percent of total ITEP funds are directed towards projects in high-need areas, with a sliding-scale local match based on need. Projects eligible for ITEP funding include new sidewalks, the replacement of existing sidewalks, pedestrian crossings, bike lanes, side paths, and trails that provide a transportation option for people walking or biking. A 20 percent local match will be required for most communities

### **Illinois Department of Natural Resources Bike Paths and Recreational Trails Program (RTP)**

The Recreational Trails Program (RTP) is a federal grant program managed by the Illinois Department of Natural Resources (IDNR). These funds may be used for motorized or non-motorized trails and require a 20% non-federal match. In the past, IDNR has used this grant program to fund trail construction and rehabilitation, restoration of natural areas adjacent to existing trails, and land acquisition for future trails.

### **Illinois Bicycle Path Program (IBP)**

The Illinois Bicycle Path Program is a grant program that provides local units of government with funding to develop, expand, and enhance non-motorized paths and trails. Local units of governments are eligible to apply for a grant. Funds awarded through the program may be used for land acquisition, trail construction, signage, fencing, draining, or construction of support facilities, such as water fountains and restrooms. Illinois Bicycle Path Program grants will cover up to 50% of total project costs, with a maximum of \$200,000 per year for development projects and no maximum for land acquisition.

### **RTA Access to Transit (RTA AtT)**

The RTA's Access to Transit Improvement program is designed to increase access to the transit system through small scale capital improvements. These improvements will help to increase ridership and make the system more accessible for users, particularly persons with disabilities, older adults, and occasional riders. The Access to Transit Improvement Program is intended to leverage RTA funds with federal CMAQ funding to help implement recommendations contained in RTA-funded Community Planning studies. Funding for the program is dependent on RTA-supported projects being accepted into the CMAQ program.

Eligible projects include small-scale capital improvements that stem from recommendations contained in a Community Planning study. Eligible projects must be able to demonstrate the ability to increase ridership and improve access to existing transit stations, stops and services. If accepted into the Program, the RTA will provide the required 20% local match for federal CMAQ funding up to a maximum of \$1,000,000 per project. Applicants who wish to submit projects that exceed \$1,000,000 may do so but will be required to provide any excess local match over \$200,000.

### **IDOT Safe Routes to School (SRTS)**

Safe Routes to School (SRTS) is a program that promotes walking and bicycling to school through infrastructure improvements, enforcement, planning tools, safety education, and incentives to encourage walking and bicycling to school. This program provides funds to government and non-profit organizations for projects that make it is easier and safer for children to walk or bike to school, such as sidewalk and bike trail improvements, pedestrian crossing signals, and signage, or non-infrastructure projects, such as crossing guards, bicycle safety trainings, and other educational sessions. This program provides \$6 million annually, with the majority going to infrastructure projects.

### **STP Shared Fund (STP-SF)**

Managed by CMAP, the STP Shared Fund was originally established in 2018 to fund transportation projects with available federal funds that are of "regional significance." Although walking and biking projects were specifically excluded, CMAP made stand-alone pedestrian and bicycle projects eligible for STP funding and increased points available for projects with walking and biking elements. These changes took effect for the 2021 funding cycle. The STP Shared Fund has a broad eligibility in comparison to other funding sources (CMAQ, TAP, HSIP). Eligible project types include pedestrian/bicycle barrier elimination, bridge or road reconstruction, bus speed improvements and transit station improvements, among others. Stand-alone pedestrian and bicycle projects that are now eligible for funding must eliminate a barrier such as a waterway, highway or railroad that prevent travel by foot or bike. Examples include a bicycle/pedestrian overpass or underpass or a new multi-use trail. Transit station improvements can also address gaps in pedestrian and bicycle access. Examples include completing the sidewalk or bicycle network that connects to the station or installing bike parking or bike share at a station.

When projects are scored, additional points are awarded to communities that have adopted a Complete Streets Policy. Points are also awarded to projects that have Complete Streets elements including sidewalks, bike lanes, trails, refuge islands, bump-outs, and crosswalks. STP typically funds phase 2 engineering, land acquisition and construction for projects that have a total cost of \$5 million or more. High-need communities facing hardship may be eligible for funding phase 1 engineering and in some cases may be exempt from the program's 20 percent local match requirement.

### **Transportation Alternatives Program (TAP-L)**

The locally programed Transportation Alternatives Program (TAP-L) is a federally-funded program of surface transportation improvements designed to support non-motorized transportation. CMAP uses a competitive process to select bicycle facility projects to fund under this program that help complete the Regional Greenways and Trail Plan. For the TAP-L program, eligible sponsors include local governments, regional transportation authorities, transit agencies, natural resource or public land agencies, school districts, and any other local or regional governmental entity with responsibility for oversight of transportation or recreational trails. IDOT is not an eligible sponsor of TAP-L-funded projects but may partner with an eligible project sponsor to carry out a project. The same is true for non-profit agencies.

For TAP-L funding, only bicycle facility projects are eligible. The bicycle facility should serve a transportation purpose and not be solely a recreational facility. Cost items that are not required for the operation of a bicycle facility are not allowed. Phase I (preliminary) engineering will be the responsibility of the project sponsor to complete without TAP-L funding. With limited exceptions, all other phases -- including phase II engineering, right-of-way acquisition, construction (including construction engineering), and implementation -- are eligible for TAP-L funding.

### **Motor Fuel Tax (MFT)**

The Illinois Motor Fuel Tax (MFT) is a statewide charge on motor fuel used to maintain the public highway system. IDOT allocates revenue from the tax to counties, townships, and municipalities each month as outlined by the MFT fund distribution statute. MFT funds may be used for accessibility projects, street improvement, pedestrian crossing signals, bicycle facilities, and wayfinding signs, among other uses. MFT revenues may be used as a local match for federal grants.



### **IDOT Highway Safety Improvement Program (HSIP)**

Illinois' HSIP is a federal program administered by the Federal Highways Administration (FHWA), intended to produce a measurable and significant reduction in fatalities and serious injuries resulting from traffic related crashes on all public roads. Emphasis is placed on performance. States are required to set performance measures and targets for reductions in the number of fatalities and serious injuries and the reduction in the rate of fatalities and serious injuries per million vehicle miles traveled. In addition, ensuring there are reductions in fatalities and serious injuries on rural major and minor collector roads and rural local roads as well as with older (>65 years old) drivers and pedestrians is a priority.

HSIP funds may be used for a total reconstruction or also to address safety issues without completely reconstructing entire roadway segments or intersections to the latest policies and standards. The federal funding level per project is a maximum 90 percent of the total eligible improvement cost for the project with the local public agency responsible for the ten (10) percent matching funds and any non-participating items. All phases of a safety improvement project are eligible for this program, including preliminary engineering, design, construction, and construction engineering. Right-of-way costs are typically not eligible to be covered by this funding.

### **Illinois Commerce Commissions Grade Crossing Protection Fund (GCPF)**

The Illinois Commerce Commission (ICC) has the statutory responsibility to improve safety at public highway-rail crossings in the State of Illinois. The GCPF, appropriated to IDOT but administered by the ICC, was created to assist local jurisdictions (counties, townships and municipalities) in paying for safety improvements at highway-railroad crossings on local roads and streets only. Assistance from the GCPF cannot be used for safety improvements at highway-rail crossings located on the state road or highway system. The GCPF is typically used to help pay for projects such as warning device upgrades, grade separations, pedestrian grade separations, improvements to the portion of the public roadway directly adjacent to the crossing surface, and low cost improvements at unsignalized crossings.

### **Private Sources**

#### **PeopleForBikes Community Grant Program (PFB)**

The PeopleForBikes Community Grant Program supports bicycle infrastructure projects and targeted advocacy initiatives that make it easier and safer for people of all ages and abilities to ride. PeopleForBikes accepts grant applications from non-profit organizations with a focus on bicycling, active transportation, or community development; from city or county agencies or departments and from state or federal agencies working locally. PeopleForBikes focuses most grant funds on bicycle infrastructure projects, such as bike paths, lanes, trails, and bridges; mountain bike facilities; Bike parks and pump tracks; BMX facilities; and end-of-trip facilities such as bike racks, bike parking, bike repair stations and bike storage.

PeopleForBikes will fund engineering and design work, construction costs including materials, labor and equipment rental and reasonable volunteer support costs. For advocacy projects, staffing that is directly related to accomplishing the goals of the initiative may be funded. PeopleForBikes accepts requests for funding up to \$10,000. There is not a specific percentage match, look at leverage and funding partnerships very carefully. Grant requests for funding of more than 50% of the project budget are not considered.

#### **Openlands ComEd Green Region Program (GRP)**

This program provides grants of up to \$10,000 to support open space projects, including trails. This annual grant typically opens every January.

#### **AARP Livable Communities (AARP)**

The AARP Community Challenge grant program is part of the nationwide AARP Livable Communities initiative that helps communities become great places to live for residents of all ages. The program is intended to help communities make immediate improvements and jump-start long-term progress in support of residents of all ages. The AARP Community Challenge grant program is part of the nationwide AARP Livable Communities initiative, which helps communities become great places to live for residents of all. Priority projects include projects that create vibrant public places that improve open spaces, parks and access to other amenities and projects that deliver a range of transportation and mobility options that increase connectivity, walkability, bikeability, wayfinding, access to transportation options and roadway improvements.

## ENERGY EFFICIENCY AND RESILIENCY

### Illinois Environmental Protection Agency (IEPA) Grants

The Illinois EPA provides financial assistance for certain land, air, and water-related projects. These grants offer funding to keep drinking water safe and improve the water quality of Illinois' lakes, streams, and wetlands.

### Green Infrastructure Grant Opportunities (GIGO) Program

GIGO funds green infrastructure projects and best management practices (BMPs) that prevent, eliminate, or reduce water quality issues by decreasing stormwater runoff. This is especially relevant in the Main Street Crossing Site in the Pingree Road Station Area. The state-financed program offers \$75,000 to \$2.5 million grants. The program matches 75 percent of costs, with 25 percent to be provided by the applicant.

### Section 319(h) Nonpoint Source Pollution Control Financial Assistance Program (NSPCFAP)

The Section 319(h) program aims to protect the water quality in Illinois through the control of NPS pollution. The program includes providing funding to groups to implement projects that utilize cost-effective best management practices (BMPs) on a watershed scale. Projects may include detention basins and filter strips or erosion control ordinances and setback zones to protect community water supply wells. Technical assistance and information/education programs are also eligible. The IEPA funds approximately 15 projects per year, with the average annual funding being \$3.5 million. Typically funds are approximately 60 percent of total project costs and the applicant provides the remainder as a match.

## ACTION MATRIX

The Action Matrix presents a complete list of all actionable recommendations made within the Crystal Lake and Pingree Road Stations Strategic Action Plan. The matrix includes a key action, time estimate, potential partnerships, and potential funding sources. The Action Matrix should be used to evaluate the City's progress based on completed actions.

### KEY ACTION

Key actions include capital projects, policy or regulatory amendments, and strategies that should be prioritized to lay the foundation for long-term Plan implementation. For example, a key action may include regulatory amendments that remove barriers to desired development or coordination among stakeholders to procure funding for recommended improvements.

### STATION AREA

Certain key action items may be applicable to only one of the two Station Areas, and some may be applicable to both. Descriptions of Station Area applicability are listed below:

- **CL:** Applies to Crystal Lake Station Area only
- **PR:** Applies to Pingree Road Station Area only
- **Both:** Applies to both Crystal Lake and Pingree Road Station Areas

### TIME ESTIMATE

Time estimates indicate, in years, how long it would take to complete a specific action. Descriptions of each timeframe are detailed below:

- **Short:** less than two years.
- **Medium:** two to five years.
- **Long:** greater than five years.
- **Ongoing:** requires immediate action and continued attention throughout the life of the Stations Strategic Action Plan

## POTENTIAL PARTNERS

Implementation requires a coordinated effort between the City, transit agencies, transportation departments, and developers. The City should assume the leadership role in taking action on the Crystal Lake and Pingree Road Stations Strategic Action Plan, but it will need to maintain partnerships and facilitate regular communication and cooperation with the various local groups. The City already has a variety of partnerships that should be maintained to work collaboratively toward mutual interests and create an organized approach to implementation. New partnerships should include nearby municipalities, regional and state agencies, local community organizations, school districts and other taxing districts, local business communities, and other groups with a vested interest in Crystal Lake. The City should continue to partner with or explore new opportunities with the following :

- Developers/Property Owners
- Realtors
- Downtown Crystal Lake
- Historic Preservation Commission
- City's Public Works Department
- Metra
- Pace
- McHenry County Division of Transportation (MCDOT)
- McHenry County Conservation District (MCCD)
- Illinois Department of Transportation (IDOT)
- Regional Transportation Authority (RTA)
- Union Pacific (UP) Railroad
- BNSF Railway
- Crystal Lake Elementary District 47 Schools
- Local/Regional Artists

## POTENTIAL FUNDING SOURCES

These are potential funding sources that the City can pursue to support the implementation of the Plan. It is important to note that funding sources are subject to change over time. As such, the City should continue to research and monitor grants, funding agencies, and programs to identify deadlines, funding specifications, and new opportunities as they become available. Some action items, such as zoning amendments, do not require funding sources for implementation.



#	Action Item	Station Area	Time Estimate	Potential Partners	Potential Funding Source
<b>4</b>	<b>Land Use &amp; Development</b>				
<b>4A</b>	<b>Land Use Plan</b>				
4A-1	Evaluate the creation of a Transition Use (TU) zoning district.	CL	Short	Developers/Property Owners, Realtors	
4A-2	Encourage or require the use of a PUD for the Walkup Avenue/Minnie Street/Woodstock Street redevelopment opportunity site to provide developers the flexibility to build high-quality developments that aligns with the vision of the Plan's Illustrative Redevelopment Concept for this site.	CL	Short	Developers/Property Owners, Realtors	
4A-3	Work with McHenry County and property owners to annex the Main Street Crossing site into the City of Crystal Lake including rezoning to into R3-B: Multifamily Residential to accommodate single-family attached and multi-family land uses and into M: Manufacturing and O: Office to accommodate industrial/logistics and flex land uses.	PR	Long	McHenry County, Developers/Property Owners, Realtors	
4A-4	Encourage use of a PUD for the Main Street Crossing site to encourage high-quality and comprehensive developments that aligns with the vision of the Main Street Crossing site's Illustrative Redevelopment Concept.	PR	Long	Developers/Property Owners, Realtors	
4A-5	Continue to require PUDs for future single-family attached and multifamily uses in Downtown Crystal Lake between North Main Street, East Crystal Lake Avenue, Woodstock Street, and Grant Street.	CL	Ongoing	Developers/Property Owners, Realtors	
4A-6	Encourage the use of PUDs on the Land Use Plan's redevelopment opportunity sites near East Crystal Lake Avenue and North Main Street, including the Main Street/Brink Street redevelopment opportunity site, to provide greater flexibility in building bulk, height, and setback requirements.	CL	Short	Developers/Property Owners, Realtors	
4A-7	Assist developers/property owners and business owners in identifying relevant and eligible economic incentives.	Both	Ongoing	Downtown Crystal Lake Main Street, Business Owners	FIP, STR, TA, RFP, IAICP, TIF
4A-8	Maintain the existing B-2 General Commercial zoning district along US-14 to preserve the existing land use pattern.	PR	Ongoing	Developers/Property Owners, Realtors	
4A-9	Rezone the identified redevelopment opportunity sites and future land uses dedicated to flex uses in the Pingree Road Station Area, except the Main Street Crossing Site, into M – Manufacturing to accommodate both office and high-quality business park uses.	PR	Medium	Developers/Property Owners	
4A-10	Coordinate and work with Metra to solicit (re)development interest on Metra's excess property at the northwest corner of Pingree Road and Congress Parkway.	PR	Long	Developers/Property Owners, Realtors	
4A-11	Continue to allow non-traditional uses in industrial buildings, such as wellness clinics, indoor sports center, and fitness centers, in the O-Office, M-L Manufacturing Limited, and M-Manufacturing zoning districts, to improve industrial land use's resiliency against changing market trends while requiring site design to mitigate conflict between passenger and freight traffic.	PR	Short	Developers/Property Owners, Realtors	

4A-12	Work with the Crystal Lake Elementary District 47 to ensure the school districts' future facility development by the Beardsley Middle School conforms with the City's existing landscaping and buffer requirements.	PR	Medium	Crystal Lake Elementary District 47 Schools	
4A-13	As station area parking agreements approach their expiration year, coordinate with Metra to analyze that Station Area's parking utilization rates to determine whether the Metra parking lots should remain or be redeveloped.	Both	Long	Metra	
<b>4B</b>	<b>Urban Design Framework</b>				
4B-1	Compose and adopt a design guidelines document that provides clear expectations of what constitutes quality development and desired design features for new development in both Station Areas including guidance for residential, mixed-use, commercial, and flex and industrial uses.	Both	Short		
4B-2	Work with developers to incorporate 'Third Place' elements, such as outdoor dining space and open space, in their site programming and building design to foster the additional creation of Third Places' in both Station Areas.	Both	Long	Developers/Property Owners	Developer contribution
4B-3	Work with Downtown Crystal Lake Main Street to develop business support promotional materials and coordinate with Downtown Crystal Lake Main Street to educate Downtown Crystal Lake's business owners on incentive opportunities and tools (e.g. outdoor seating permits).	CL	Short	Business Owners; Downtown Crystal Lake Main Street	
4B-4	Coordinate with Downtown Crystal Lake Main Street and relevant stakeholders to regularly host community events in Downtown Crystal Lake.	CL	Ongoing	Downtown Crystal Lake Main Street	
4B-5	Work with developers and the City's Public Works Department to establish a central stormwater detention system that would also serve as a public open space amenity in the Main Street Crossing site.	PR	Long	Developers/Property Owners, Realtors, City's Public Works Department	GIGO, NSPCFAP, Developer contribution
4B-6	Work with developers to establish a connected network of shared-use path in the Main Street Crossing site.	PR	Long	Developers/Property Owners, Realtors, City's Public Works Department	ITEP, RTP, IBP, STP-SF, TAP-L, GRP, Developer contribution
4B-7	In the development review process, work with developers to enhance the right-of-way by their site by incorporating Downtown Crystal Lake's streetscaping elements.	CL	Medium	Downtown Crystal Lake Main Street	Developer contribution
4B-8	Create a streetscaping palette, based on the Plan's recommendations, for the Pingree Road Station Area.	PR	Short	City's Public Works Department	
4B-9	Incorporate the replacement of existing auto-oriented lighting fixtures with dual-scale fixtures, in addition to widening sidewalks into shared-use paths, as part of future roadway improvement projects for Congress Parkway and Memorial Drive in the City's. The City can schedule and budget them through its Capital Improvement Project.	PR	Medium	City's Public Works Department	CIP
4B-10	Maintain the City's existing gateway signage in Downtown Crystal Lake while evaluating whether its existing gateway signs design reflect the community's character and aspirations.	CL	Ongoing	Downtown Crystal Lake Main Street	Developer contribution, Private Fundraising/Sponsorship
4B-11	In coordination with Downtown Crystal Lake Main Street Program, establish a program that the City can help fund and work with local artists to implement temporary public art installations.	CL	Short	Downtown Crystal Lake Main Street, Local/Regional Artists	
4B-12	Prioritize new public art installations at locations that would expand the area that pedestrians would walk around in Downtown Crystal Lake.	CL	Short	Downtown Crystal Lake Main Street, Local/Regional Artists, City's Public Works Department	
4B-13	Prioritize new public art installations at Pingree Road Station Area's key intersections, as identified in the Plan's Key Intersection diagram.	PR	Short	Local/Regional Artists, City's Public Works Department	



4B-14	Work with developers to install a visually attractive gateway signage at the intersection of North Main Street and the proposed east-west connector in the Main Street Crossing site.	PR	Long	Developers/Property Owners, Realtors, City's Public Works Department	Developer contribution, Private Fundraising/Sponsorship
4B-15	Coordinate with Metra and relevant parties to ensure the existing gateway signage by the intersection of Congress Parkway and North Pingree Road is well-maintained.	PR	Short	Metra, City's Public Works Department	
4B-16	Initiate a pilot project by designating a section of a street in Downtown Crystal Lake as a festival street including complementary streetscaping improvements.	CL	Short	Downtown Crystal Lake Main Street, Property/Business Owners, City's Public Works Department	
4B-17	Pending results of a festival street pilot, undertake a study to define the scope, scale, and budget to implement a shared street concept.	CL	Medium	Downtown Crystal Lake Main Street, Property/Business Owners, City's Public Works Department	CIP
4B-18	Continue to enforce the City's landscaping requirements to minimize land use conflicts between residential and non-residential land uses. Utilize the PUD process to ensure proposed landscaping elements from the proposal's landscaping plan align with Stations Strategic Action Plan's vision and recommendations.	Both	Long	Developers/Property Owners, Realtors, City's Public Works Department	
4B-19	Work with developers in the PUD process to implement a berm to spatially and visually separate between the residential and non-residential land uses in the Main Street Crossing site.	PR	Long	Developers/Property Owners, Realtors	Developer contribution, Private Fundraising/Sponsorship, GIGO, NSPCFAP
4B-20	Encourage the use of stormwater planters and other best practices in stormwater management to minimize stormwater runoff in future (re)developments.	CL	Ongoing	Developers/Property Owners, Realtors, City's Public Works Department	Developer contribution, Private Fundraising/Sponsorship, GIGO, NSPCFAP
4B-21	Encourage clustered developments in the Main Street Crossing site and other infill developments to dedicate contiguous areas to open space and preserve existing tree canopy (if any).	PR	Ongoing	Developers/Property Owners, Realtors, City's Public Works Department	
4B-22	Continue to work with the Historic Preservation Commission to preserve historically significant buildings as the Crystal Lake Station Area experience land use and transportation changes over time.	CL	Ongoing	Historic Preservation Commission	
4B-23	Work with the Historic Preservation Commission to have an updated inventory of historically significant buildings in the Crystal Lake Station Area	CL	Ongoing	Historic Preservation Commission	

#	Action Item	Station Area	Time Estimate	Potential Partners	Potential Funding Source
<b>5</b>	<b>Transportation</b>				
<b>5A</b>	<b>Crystal Lake Station Area</b>				
5A-1	Encourage developers to refer to the Pace's Transit Supportive Design Guidelines to foster transit-supportive developments.	CL	Ongoing	Developers/Property Owners, Realtors	
5A-2	Improve design of Pace and McRide boarding/alighting area next to Metra station, addressing overall amenities and ADA accessibility. Potential improvements could include: lighting, curb-less stop area, audible signals, buffer for expanded boarding area, detectable warning strips, and signage.	CL	Short	City, Pace, Metra, MCDOT	STP-SF RTA AtT
5A-3	Continue to work with Pace and Metra on transit service changes in post-COVID era	CL	On-going	City staff, Pace, Metra, McDOT	



Walkup Avenue					
5A-4	Consider initiating an engineering study to study the feasibility of a roundabout at Walkup Avenue/Grant Street/Crystal Lake Avenue.	CL	Long	City's Public Works Department	
5A-5	Add bike facility between Crystal Lake Avenue and Railroad Street, either through eliminating the center turn lane and reallocate to on-street bike lanes, or by widening the east side sidewalk expanding as a shared use path. Complete engineering study to determine opportunity for protected on-street bike lane or shared use path.	CL	Medium	City's Public Works Department	ITEP MFT TAP-L IBP
5A-6	Improve railroad crossing to clearly delineate pedestrian space. Design elements could include fencing, channelization, pavement markings and texturing, refuge areas, and fixed message signs.	CL	Medium	City's Public Works Department, Metra, UP Railway	MFT
5A-7	Consolidate bank driveways for Home State Bank by eliminating the in/out drive just south of the Bank drive-through access drive.	CL	Medium	City's Public Works Department, Developers/Property Owners,	
Railroad Street					
5A-8	Add on-street bike lanes by reallocating on-street parking and using available right-of-way. East of Grant Street would be designed as a shared street.	CL	Short	City's Public Works Department	IBP STP-SF MFT
5A-9	Widen the existing sidewalk on north side between Grant Street and Main Street.	CL	Medium	City's Public Works Department	STP-SF RTA Act CITY
5A-10	Install a bike station adjacent to the Crystal Lake Metra Station's building at Grant Street - covered bike parking with bike repair, bike share, e-scooters.	CL	Short	Metra, McHenry County Bicycle Advocates	IBP STP-SF CITY Art
5A-11	Remove the on-street commuter parking between 2nd Avenue and Grant Street.	CL	Short	City's Public Works Department, Metra	CITY
5A-12	Convert angled parking between Grant Street and Main Street into parallel parking, while designating space for shared-ride drop off/pick up.	CL	Short	City's Public Works Department, Metra	CITY RTA Act
5A-13	Add mid-block raised crossing at the Crystal Lake Metra Station's entrance	CL	Short	City's Public Works Department, Metra, Pace	RTA Act STP-SF MFT
Main Street					
5A-14	Complete an engineering assessment of possible design opportunities of widening the existing sidewalk at the east side of N Main Street into a 10-foot shared-use path.	CL	Short	City's Public Works Department, MCCD, Developers/Property Owners	TAP-L IBP
5A-15	Should widening the existing sidewalk at the east side of N Main Street is not possible, install bicycle signages along N Main Street, such as wayfinding signs to the Prairie Path Trail, "Bikes May Use Full Lane," and "Share the Road" signages.	CL	Short	City's Public Works Department	
5A-16	Add a pedestrian/bicycle connector at the northeast corner of the railroad crossing to connect to Metra parking lots and Ellsworth Street.	CL	Medium	City's Public Works Department, Developers/Property Owners	RTA Act IBP
5A-17	Improve delineation of the pedestrian zone at the railroad crossing.	CL	Medium	City's Public Works Department, Metra, UP Railway	METRA MFT GCPF
5A-18	Add bike boxes at Crystal Lake Avenue intersection. Consider leading pedestrian interval (LPI) phase, if possible.	CL	Short	City's Public Works Department	HSIP



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Crystal Lake & Pingree Road Stations Strategic Action Plan  
Illustrative Redevelopment Concepts

Woodstock Street					
5A-19	Add curb extensions (corner bump-outs) at Woodstock Street and Minnie Street.	CL	Short	City's Public Works Department	MFT STP-SF
5A-20	Complete the sidewalk gap on south side of Woodstock Street	CL	Medium	Developers/Property Owners, City's Public Works Department	MFT STP-SF
Brink Street					
5A-21	Eliminate multiple driveways to parking lots.	CL	Long	Developers/Property Owners	CITY
5A-22	Add a mid-block raised crosswalk on Brink Street between Grant Street and Williams Street and between Williams Street and Main Street	CL	Short	City's Public Works Department	CITY MFT
5B Pingree Road Station Area					
Overall Station Area					
5B-1	Complete sidewalks gaps on both sides of the streets throughout the Pingree Road Station Area.	PR	Long	City's Public Works Department	RTA AtT
5B-2	Add wayfinding/directional signs throughout the Pingree Road Station Area identifying directions Prairie Trail, Pingree Road path, and Downtown Crystal Lake.	PR	Short	City's Public Works Department	CITY RTA AtT MFT
5B-3	New developments should reference Pace's Transit Supportive Design Guidelines.	PR	Ongoing	Developers/Property Owners, Realtors	
Congress Parkway					
5B-4	Complete roundabout at Congress Parkway and Exchange Drive and at Congress Parkway and Federal Drive.	PR	Medium	City's Public Works Department, MCDOT, IDOT	HSIP CMAQ
5B-5	Add shared-use path on both sides of the roadway, connecting to the shared-use path on the west side of Pingree Road. Design elements would include signage and green paint at driveway crossings.	PR	Medium	City's Public Works Department, Developers/Property Owners, Realtors	ITEP CMAQ RTP
5B-6	Complete sidewalk gaps on the south side of the roadway.	PR	Medium	City's Public Works Department, Developers/Property Owners, Realtors	MFT STP-SF
5B-7	Add median refuge island on the west leg of Congress Parkway at Pingree Road.	PR	Medium	City's Public Works Department	HSIP STP-SF
5B-8	The City should contract with an engineering firm to complete a Phase I Engineering Study/Preliminary Design to determine alignment and impacts for implementing a continuous shared-use path/sidewalk along Congress Parkway.	PR	Short	City's Public Works Department	
Pingree Road Metra Station Pedestrian Tunnel					
5B-9	The City should continue to work with Metra to maintain the Pingree Road Metra Station's pedestrian tunnel.	PR	Short	Metra, City Manager's Office	
Memorial Drive					
5B-10	Complete sidewalk gaps along both sides of the roadway.	PR	Long	City's Public Works Department, Developers/Property Owners	MFT STP-SF
5B-11	Install marked pedestrian routes through surface parking lots to adjacent land uses and buildings.	PR	Long	Developers/Property Owners	CITY PRIVATE
5B-12	Narrow Memorial Drive at Congress Parkway by adding curb extensions	PR	Short	City's Public Works Department	STP-SF

Exchange Drive					
5B-13	Complete roundabout at Congress Parkway and Exchange Drive.	PR	Medium	City's Public Works Department, IDOT	
5B-14	Add shared use path on both sides of the roadway, connecting to the retail center north of US-14. Design elements would include signage and green paint at driveway crossings.	PR	Medium	City's Public Works Department, Developers/Property Owners	ITEP CMAQ MFT
Pingree Road					
5B-15	Add a pedestrian refuge island on both legs of Pingree Road at Grandview Drive/Oak Hollow Road.	PR	Short	City's Public Works Department	MFT STP-SF
5B-16	Complete traffic warrant study to consider upgrading flashing beacon to a Pedestrian Hybrid Beacon (PHB), which is used to warn and control traffic at an unsignalized location to assist pedestrians in crossing a street or highway at a marked crosswalk.	PR	Short	City's Public Works Department, Consultant (if needed)	HSIP
Commonwealth Drive					
5B-17	Add shared-use path on both sides of the roadway, connecting from Main Street/Prairie Trail to the Northwestern Medical Center.	PR	Short	City's Public Works Department	ITEP CMAQ IBP
5B-18	Add sidewalks on the west side of Commonwealth Drive.	PR	Short	City's Public Works Department, Developers/Property Owners	MFT STP-SF
5B-19	Implement intersection improvements at the intersection of Commonwealth Drive and Main Street.	PR	Medium	City's Public Works Department, MCDOT, IDOT, Union Pacific Railroad	HSIP
5B-20	Add bike route signage at intersection of Commonwealth Drive and Main Street.	PR	Medium	City's Public Works Department	ITEP IBP
5B-21	Stripe Prairie Trail crossing on the intersection of Commonwealth Drive and Main Street with green paint.	PR	Medium	City's Public Works Department	IBP ITEP
5B-22	Add median refuge island on the intersection of Commonwealth Drive and Main Street.	PR	Medium	City's Public Works Department	HSIP

#	Action Item	Station Area	Time Estimate	Potential Partners	Potential Funding Source
<b>6</b>	<b>Illustrative Redevelopment Concepts</b>				
6-1	Utilize the Plan's Illustrative Redevelopment Concepts as a marketing tool to attract development interest at the Walkup Avenue/Minnie Street/Woodstock Street, Main Street/Brink Street, and the Main Street Crossing sites.	Both	Long	Developers/Property Owners, Realtors	
<b>6A</b>	<b>Walkup Avenue/Minnie Street/Woodstock Street</b>				
6A-1	Utilize the visualizations to encourage higher-density residential developments while complementing with the surrounding area's physical character.	CL	Long	Developers/Property Owners, Realtors	
6A-2	Work with developers to situate parking components at the rear of future developments.	CL	Long	Developers/Property Owners, Realtors	
6A-3	Encourage the incorporation of 'brownstone'-like architectural style in the facade design of future single-family attached housing.	CL	Long	Developers/Property Owners, Realtors	
6A-4	Utilize the City's landscaping ordinance to ensure future developments are sufficiently screened against adjacent developments.	CL	Long	Developers/Property Owners, Realtors	
6A-5	Work with developers to ensure primary access into this site is from Minnie Street. A secondary right-in/right-out drive can be established along North Walkup Avenue.	CL	Long	Developers/Property Owners, Realtors	

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Crystal Lake & Pingree Road Stations Strategic Action Plan  
**Illustrative Redevelopment Concepts**



<b>6B Main Street/Brink Street</b>					
6B-1	Encourage developers to incorporate well-designed and publicly accessible 'third place' components including outdoor dining space and a central open space area or park, into the site's developments' program.	CL	Long	Developers/Property Owners, Realtors	
6B-2	Work with property owners and developers to install a central pedestrian walkway that connects the existing walkway between North Williams Street and the alley, to the Main Street/Brink Street redevelopment opportunity site.	CL	Long	Developers/Property Owners, Realtors	
6B-3	Leverage Main Street/Brink Street redevelopment opportunity site's grade change to promote taller structures that complement the height of buildings fronting Williams Street.	CL	Long	Developers/Property Owners, Realtors	
6B-4	Utilize the Plan's Urban Design Framework to promote visually attractive developments with a strong relationship with the public right-of-way.	CL	Long	Developers/Property Owners, Realtors	
6B-5	Explore the feasibility of acquiring the rear structures of businesses along North Williams Street to facilitate the comprehensive redevelopment of the Main Street/Brink Street opportunity site.	CL	Long	Developers/Property Owners, Realtors	
6B-6	Ensure the main access point into the Main Street/Brink Street redevelopment opportunity site is located along Brink Street. Secondary access can be implemented along North Main Street, but they need to be right-in/right-out drive to avoid conflicts with the existing intersections at the south and north of the site.	CL	Long	Developers/Property Owners, Realtors	
6B-7	Encourage a mix of residential and non-residential land uses within the Main Street/Brink Street redevelopment opportunity site to expand business opportunities and base of residents living within Downtown Crystal Lake.	CL	Long	Developers/Property Owners, Realtors	
<b>6C Main Street Crossing</b>					
6C-1	Extend Exchange Drive west and connect to North Main Street and Commonwealth Drive.	PR	Long	Developers/Property Owners, Realtors	
6C-2	Use the visualizations to encourage traditional commercial outlots and flex uses along North Main Street.	PR	Long	Developers/Property Owners, Realtors	
6C-3	Work with developers to implement green infrastructure, such as bioswales and naturalized detention areas to naturally manage and filter stormwater runoff.	PR	Long	Developers/Property Owners, Realtors	
6C-4	Encourage the use of berming or other forms of substantial screening to separate residential from non-residential land uses.	PR	Long	Developers/Property Owners, Realtors	
6C-5	Utilize the visualization and the Plan's Urban Design Framework to illustrate the importance of having future flex and industrial/logistics land uses orient towards a central natural feature while connected with a network of off-street shared-use path to promote an attractive business-park-like environment.	PR	Long	Developers/Property Owners, Realtors, City's Public Works Department	
6C-6	Work with developers to incorporate outdoor dining and seating areas, open space, and trails into their development plans to increase opportunities for 'third places'.	PR	Long	Developers/Property Owners, Realtors	
6C-7	Utilize the Main Street Crossing's Illustrative Redevelopment Concept to situate future industrial/logistics land uses at the northern side of the Main Street Crossing site, which would potentially have access to a rail spur connection.	PR	Long	Developers/Property Owners, Realtors, UP Railway, City's Public Works Department	
6C-8	Work with developers to situate parking lots at rear of the building and to implement safe and direct pedestrian accessways between the public right-of-way and future developments' primary entrance(s).	PR	Long	Developers/Property Owners, Realtors	
6C-9	Utilize the visualization to encourage future developments to be clustered together to minimize impervious surface and conserve open space as much as possible.	PR	Long	Developers/Property Owners, Realtors	



## Agenda Item No: 13

### City Council Agenda Supplement

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**Meeting Date:**

September 7, 2021

**Item:**

Bid Award - Sewer Lining Services

**Staff Recommendation:**

Motion to award the contract for sewer lining services to the lowest responsive and responsible bidder, Visu-Sewer, Inc., and to adopt a Resolution authorizing the City Manager to execute a contract with Visu-Sewer, Inc. in the submitted bid amounts with a 10% contingency for unforeseen expenses and to approve warranted completion date extensions.

**Staff Contact:**

Michael Magnuson, Director of Public Works and Engineering

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**Background:**

Sewer lining is an annual program in the Public Works Department. This program addresses structural deficiencies (broken or cracked pipe) and infiltration that has been detected by Public Works crews as part of our sewer assessment operations throughout the year. Infiltration is the flow of groundwater into the sanitary sewer system through cracks or other defects that are caused by age, tree roots, etc. Infiltration can overload the sanitary sewer system (cause backups) and result in operational issues at wastewater treatment facilities.

Lining is significantly less expensive and more efficient than traditional open-cut excavation and pipe replacement methods. The lining is normally installed with minimal surface disruption.

Public Works has identified critical need areas of sanitary sewer mains that require lining for defects and infiltration. Some of these areas include sanitary sewers within the limits of the upcoming Main Street and Route 176 intersection project. Lining of the sewer will greatly minimize the potential for future repairs after the roadway is resurfaced.

On August 6, 2021, the City of Crystal Lake publicly opened and read aloud the bids received for the lining of sewers at various locations throughout the City. The initial as-read bid amount was determined to be over budget. Staff analyzed which lining areas were critical and prioritized these areas to be within our approved budget. The areas deferred for this bid will be requested during the upcoming budget process.

The following is a breakdown of the base bids received:

	√ Visu-Sewer, Inc. Bridgeview, IL	Hoerr Construction Peoria, IL	Insituform Technologies Orland Park, IL
As-read Bid Amount	\$815,586.25	\$860,490.00	\$919,233.50
Awarded Amount	\$680,071.25	\$724,685.00	\$717,370.50

√ Indicates lowest responsive and responsible bidder

**Recommendation**

This contract is being presented pursuant to a competitive bidding process. Under such process, the contract is to be awarded to the “lowest responsive and responsible bidder.” The lowest responsive and responsible bidder is the contractor: (i) whose bid substantially conforms to the material provisions of the bid specifications, (ii) who demonstrates the financial capacity and ability to undertake and complete the project in question in accordance with bid specifications, and (iii) whose bid price is lowest among the responsive and responsible bidders. Selecting a contractor on bases not set forth in the bid specifications can lead to challenges to the City’s award.

The Public Works Department has reviewed bids received for completeness and accuracy in accordance with the invitation to bid document. City staff has worked with Visu-Sewer, Inc. previously with satisfactory results. Therefore, the Public Works Department recommends that the contract for Sewer Lining Services be awarded to the lowest responsive and responsible bidder, Visu-Sewer, Inc., in the amount of \$680,071.25. Sufficient funds are available for this project in the FY2021/2022 Budget.

**Votes Required to Pass:**

Simple majority



**DRAFT**



## **RESOLUTION**

**WHEREAS** the CITY OF CRYSTAL LAKE has identified the need for sewer lining to address structural deficiencies and infiltration; and

**WHEREAS** the CITY received and publicly opened bids for lining of various sewers in the City on August 6, 2021; and

**WHEREAS** the lowest responsive and responsible bidder is Visu-Sewer, Inc., Bridgeview, Illinois.

**NOW THEREFORE BE IT RESOLVED BY THE MAYOR AND CITY COUNCIL OF THE CITY OF CRYSTAL LAKE** that the foregoing recitals are repeated and incorporated as though fully set forth herein; and

**BE IT FURTHER RESOLVED** the City Manager is authorized to execute a contract between the CITY OF CRYSTAL LAKE and Visu-Sewer, Inc. for the lining of sewers in various locations in the amount of \$680,071.25; and

**BE IT FURTHER RESOLVED** that the City Manager is authorized to execute change orders for up to 10% of the contract amount and to approve warranted completion date change orders relating to the contract.

**DRAFT**

DATED this 7<sup>th</sup> day of September, 2021.

CITY OF CRYSTAL LAKE, an  
Illinois municipal corporation,

By: \_\_\_\_\_  
Haig Haleblian, MAYOR

SEAL

ATTEST

\_\_\_\_\_  
Nick Kachiroubas, CITY CLERK

PASSED: September 7, 2021  
APPROVED: September 7, 2021



**Agenda Item No: 14**

**City Council  
Agenda Supplement**

**Meeting Date:** September 7, 2021

**Item:** Bid Award - 2021 Sidewalk Cutting Program

**Staff Recommendation:** Motion to award the 2021 Sidewalk Cutting Program Bid to the lowest responsive and responsible bidder, Hard Rock Concrete Cutters, and adopt a Resolution authorizing the City Manager to execute a contract with Hard Rock Concrete Cutters for \$63,437.50 with a 10% contingency for unforeseen expenses and to authorize the City Manager to review and approve necessary completion date change orders relating to the contract.

**Staff Contact:** Michael Magnuson, P.E., Director of Public Works and Engineering

**Background:**

The City uses sidewalk saw cutting as a means of eliminating sidewalk trip hazards that are created by minor displacement of sidewalks over time (heaving or settling). The saw cutting process horizontally saws off trip hazards by creating a tapered edge and thereby brings the location in compliance with American Disabilities Act (ADA) standards. During the previous five years, staff has been satisfied with the process and final outcome and has incorporated this program into the City's annual maintenance efforts. The cost of cutting the sidewalk is less than the cost to replace the sidewalk. Sidewalk cutting is accomplished relatively quickly after which the sidewalk is open to pedestrian traffic. Full replacement requires closing of the sidewalk while the concrete cures and also requires some type of landscape restoration. Large displacements are addressed in the City's annual concrete program with replacement of the entire section(s) sidewalk.

On August 31, 2021, the City opened and publicly read the bids received for the 2021 Sidewalk Cutting Program. The City received one bid:

<i>Bidder</i>	<i>Price Per SF</i>	<i>Quantity</i>	<i>Bid Total</i>
<sup>1</sup> Hard Rock Concrete Cutters	\$43.75	1,450	\$63,437.50

<sup>1</sup> Indicates Recommended Lowest Responsive and Responsible Bidder.



In 2020, the bid price per square feet was \$44.00. This year's bid price is a reduction from the 2020 unit price.

The equipment used to grind/cut the sidewalk is specialized and there are two vendors that the City is aware of that perform this type of work in this area. Historically, the City received only one bid in 2017 and 2018 (Hard Rock Concrete Cutters). In 2019 and 2020, Hard Rock Concrete Cutters and ASTI provided bids for this program. While this year the City only received one bid (as in 2017 and 2018), the bid price is less than last year and therefore staff is comfortable with awarding based on a single bid. ASTI did not submit a bid due to contractual obligations with other agencies.

**Recommendation:**

This contract is being presented pursuant to a competitive bidding process. Under such process, the contract is to be awarded to the "lowest responsive and responsible bidder." The lowest responsive and responsible bidder is the contractor: (i) whose bid substantially conforms to the material provisions of the bid specifications, (ii) who demonstrates the financial capacity and ability to undertake and complete the project in question in accordance with bid specifications, and (iii) whose bid price is lowest among the responsive and responsible bidders. Selecting a contractor on bases not set forth in the bid specifications can lead to challenges to the City's award.

Hard Rock Concrete Cutters meets all of the City's requirements and has previously performed this work for the City with satisfactory results. Therefore, staff recommends Hard Rock Concrete Cutters for the 2021 Sidewalk Cutting Program. The project is included within the FY2021/2022 Budget.

**Votes Required to Pass:**

Simple majority vote.

**DRAFT**



## **RESOLUTION**

**WHEREAS** the CITY OF CRYSTAL LAKE has identified the need to eliminate sidewalk trip hazards that are created by displacement of sidewalks over time; and

**WHEREAS** the CITY received and publicly opened one bid for the Sidewalk Cutting Program on August 31, 2021; and

**WHEREAS** the lowest responsive and responsible bidder is Hard Rock Concrete Cutters, Wheeling, IL.

**NOW THEREFORE BE IT RESOLVED BY THE MAYOR AND CITY COUNCIL OF THE CITY OF CRYSTAL LAKE** that the foregoing recitals are repeated and incorporated as though fully set forth herein; and

**BE IT FURTHER RESOLVED** that the City Manager is authorized to execute a contract between the CITY OF CRYSTAL LAKE and Hard Rock Concrete Cutters for the 2021 Sidewalk Cutting Program in the amount of \$63,437.50; and

**BE IT FURTHER RESOLVED** that the City Manager is authorized to execute change orders for up to 10% of the contract amount and to approve warranted completion date change orders relating to the contract.

**DRAFT**

DATED this 7<sup>th</sup> day of September, 2021.

CITY OF CRYSTAL LAKE, an  
Illinois municipal corporation,

By: \_\_\_\_\_  
Haig Haleblian, MAYOR

SEAL

ATTEST

\_\_\_\_\_  
Nick Kachiroubas, CITY CLERK

PASSED: September 7, 2021  
APPROVED: September 7, 2021





**Agenda Item No: 15**

**City Council  
Agenda Supplement**

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**Meeting Date:**

September 7, 2021

**Item:**

Bid Rejection and Award - Water & Wastewater  
Laboratory Services

**Staff recommendation:**

1) Motion to adopt a Resolution rejecting all proposals from the June 2, 2021 RFP opening for Water & Wastewater Laboratory Services, and

2) Motion to award the proposal for water and wastewater laboratory services to the most responsive and responsible proposer, PDC Labs, in the submitted amounts, and adopt a Resolution authorizing the City Manager to execute a one-year contract, with PDC Labs with the option for two additional one-year extensions.

**Staff Contact:**

Michael Magnuson, P.E., Director of Public Works and  
Engineering

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**Background:**

The Illinois Environmental Protection Agency (IEPA) requires all community water suppliers and wastewater treatment plant operators to complete a variety of testing for contaminants and biological properties to demonstrate compliance with IEPA regulations. This information is collected, analyzed and reported on a monthly, quarterly and annual basis to the IEPA for compliance. The water system results are also summarized annually in the Consumer Confidence Report.

In addition to regulatory compliance testing, the Water and Sewer Division requires emergency (after hours and weekend) testing and analysis when water main breaks require a shut down in part of the distribution system. If a water main break cannot be repaired under pressure, the water system in that area is shut down and repairs are made. A shutdown of the water main in an area results in a loss of water pressure and requires issuance of a boil order (IEPA regulations.) The boil order remains in effect until the repairs are completed and a sample from the shutdown area is taken, analyzed and the results pass IEPA testing requirements. Boil orders are disruptive to residents and businesses. Therefore, it is in the public interest to collect and analyze the samples as quickly as possible to lift the boil order.

The Wastewater Division has an in-house laboratory. Many samples are collected and analyzed in house to reduce costs. However, there are certain tests that require outside laboratory analysis. The type and frequency of the tests varies based on IEPA requirements at the time and the workload at the Wastewater Division laboratory.

**Rejecting June 2, 2021 Bids**

On June 2, 2021, the City of Crystal Lake opened proposals for drinking water and wastewater analytical testing required by the Illinois Environmental Protection Agency (IEPA). The RFP sought pricing for both Water and Wastewater services, however, only one vendor submitted pricing. Since the City strongly prefers competitive proposals, staff publically sent out another RFP requesting the same services. The City Council must formally reject the proposals from the June 2, 2021 submittal.

**June 23, 2021 Re-Bid**

On June 23, 2021, the City opened proposals for laboratory services. Prices were solicited for routine anticipated testing services (weekly, monthly, annual) to determine the most advantageous proposer as well as prices for other non-routine testing (process analyses, IEPA imposed requirement), should they be required in the future. Three vendors submitted proposals. The tables below summarize the results for the Water and Sewer Division and Wastewater Division results are as follows:

Water & Sewer Division Analyte Group Name*	Est. Total Cost per Year		
	√ PDC Labs McHenry, IL	*ALS Environmental Holland, MI	*Teklab Downers Grove, IL
Lead and Copper	\$540.00	No bid	No bid
Stage 2 DBPR	\$1,600.00	No bid	No bid
THM HAA	\$1,600.00	No bid	No bid
Corrosion Control Group	\$375.00	No bid	No bid
Barium	\$30.00	No bid	No bid
Nitrate (NO3- Nitrate)	\$60.00	No bid	No bid
Nitrite (NO2 –Nitrite)	\$60.00	No bid	No bid
SOC (Synthetic Organic Compounds)	\$6,000.00	No bid	No bid
VOC (Volatile Organic Compounds)	\$1,500.00	No bid	No bid
IOC (Inorganic Chemicals)	\$1,350.00	No bid	No bid
Fluorides	\$720.00	No bid	No bid
RAD WO U	\$1,075.00	No bid	No bid
Combined Radium 226 & 228	\$185.00	No bid	No bid
Gross Alpha Particle Activity	\$160.00	No bid	No bid
Coliform Coilert Method	\$7,735.00	No bid	No bid
PFAS method 537.1	\$5,400.00	No bid	No bid
<b>TOTAL ESTIMATED ANNUAL COST =</b>	<b>\$28,390.00</b>	<b>No bid</b>	<b>No bid</b>

\*ALS and Teklab did not submit pricing because they could not meet the City's specifications

As noted above, time is of the essence in analyzing samples especially for water main breaks. Both ALS and Teklab, while qualified IEPA laboratories, do not offer the 7 days a week laboratory analysis that PDC Labs offers. PDC's location in McHenry makes it easy to coordinate after-hours pickup and deliveries. PDC will typically analyze water samples up to

8:00 pm each day. This flexibility and turnaround time results in decreased duration of boil orders which benefits the residents and business of the City.

Wastewater Division Sample Name*	Est. Total Cost per Year		
	√ PDC Labs McHenry, IL	ALS Environmental Bensenville, MI	Teklab Downers Grove, IL
Nitrate/Nitrite	\$240	\$108	Cost included
Total Kjeldahl Nitrogen (TKN)	\$270	\$264	Cost included
Dichlorobromomethane	\$720	\$468	\$744
Chromium	\$100	\$72	\$104
Cyanide	\$80	\$68	\$127.60
Cyanide (Total )	\$80	\$68	\$127.60
Fluoride	\$48	\$36	\$79.20
Mercury (Other approved methods may be used for influent composite)	\$60	\$84	\$65.00
Mercury (effluent grab using USEPA method 1631 or equivlant)	\$260	\$204	\$200
Oil	\$120	\$160	\$138
Phenols 1	\$80	\$68	\$104
Cyanide (Total) (Grab)	\$80	\$68	\$127.60
Phenols 2	\$80	\$68	\$104
Radium - 226	\$740	\$880	\$1,264
Radium - 228	\$740	\$880	Included with 226
TKN	\$180	\$176	\$225
pH	\$80	\$72	\$97.20
Pesticides/ PCB's	\$150	\$75	\$140
VOC'S 625	\$175	\$75	\$150
VOC'S 624	\$105	\$41	\$90
Estimated Annual Shipping Charge	\$0	\$810	\$960
<b>Estimated Annual Total Cost</b>	<b>\$4,388.00</b>	<b>\$4,745.00</b>	<b>\$4,847.00</b>

\*Based on typical estimated annual frequency and cost of testing  
 √ Indicates recommended most responsive and responsible vendor

**Recommendation:**

This contract is being presented pursuant to a request for proposal (RFP) process. Under such process, the contract is to be awarded to the vendor whose proposal “will be the most advantageous to the City.” The City staff has reviewed the various proposals received, and determined that the recommended contractor is the most well-suited for the project.

The Public Works Department has reviewed all proposals received for completeness and accuracy in accordance with the request for proposal. Staff has worked with PDC Labs previously and received satisfactory service. Therefore, it is staff’s recommendation to award the contract to the most responsive and responsible proposer, PDC Labs, for drinking water and wastewater laboratory services. Funds have been budgeted for these services.



**Votes Required to Pass:**

Simple majority

**DRAFT**



**RESOLUTION**

**WHEREAS** it is necessary to provide the City’s residents and businesses with a safe and adequate supply of drinking water as well as ensure wastewater is treated effectively; and

**WHEREAS**, City staff sought proposals for Water and Wastewater Laboratory Services, however did not receive a competitive number of proposals on June 2, 2021; and

**WHEREAS** City staff sought another request for proposals for Water and Wastewater Laboratory Services with an opening on June 23, 2021, and

**WHEREAS** the most responsive and responsible vendor for Water and Wastewater Laboratory Services from this opening is PDC Labs, McHenry, IL.

**NOW THEREFORE BE IT RESOLVED BY THE MAYOR AND CITY COUNCIL OF THE CITY OF CRYSTAL LAKE** that the foregoing recitals are repeated and incorporated as though fully set forth herein; and

**BE IT FURTHER RESOLVED** that the proposals received on June 2, 2021 are hereby rejected; and

**BE IT FURTHER RESOLVED** that the City Manager is authorized to execute a one-year agreement with the option for two additional one-year extensions with PDC Labs for Water and Wastewater Laboratory Services in the amounts proposed; and

**BE IT FURTHER RESOLVED** that the City Manager is authorized to execute change orders for up to 10 percent of the contract amount and to approve warranted completion date change orders relating to the contract.

**DRAFT**

DATED this 7<sup>th</sup> day of September, 2021.

CITY OF CRYSTAL LAKE, an  
Illinois municipal corporation,

By: \_\_\_\_\_  
Haig Haleblian, MAYOR

SEAL

ATTEST

\_\_\_\_\_  
Nick Kachiroubas, CITY CLERK

PASSED: September 7, 2021

APPROVED: September 7, 2021





**Agenda Item No: 16**

**City Council  
Agenda Supplement**

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**Meeting Date:**

September 7, 2021

**Item:**

Bid Rejection and Award - HVAC Duct Cleaning

**Staff Recommendation:**

1. Motion to reject the HVAC Duct Cleaning bids from the bid opening on March 17, 2020 and March 23, 2021; and
2. Motion to adopt a Resolution awarding the contract for the HVAC Duct Cleaning to the lowest responsive and responsible bidder, Indoor Air Technologies, in the amounts bid, execute change orders for up to 10% of the contract amount, and approve warranted completion date change orders relating to the contract

**Staff Contact:**

Michael Magnuson, P.E., Director of Public Works and Engineering

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**Background:**

In 2019, the City initiated a program to clean sections of the HVAC ducts in the Municipal Complex. This work consists of the inspection and cleaning of all HVAC ductwork and accessories. The 2019 program cleaned the oldest sections of the Municipal Complex (staff offices). The area presented here for award includes cleaning of the Fire Department offices and City Council Chambers. The remaining Municipal Complex areas (Police Department, Fire Apparatus Bay and Public Works) will be requested in future budget years.

The City previously bid similar work in March 2020 and received three bids, but the onset of the pandemic and mandatory stay at home orders prevented this work from proceeding. Funds were re-budgeted and the project was re-bid. In March 2021, the City developed the bid documents on an "a la carte" basis, allowing the City to decide which areas to have cleaned based on the bid prices and budgetary constraints. Bids were over budget and staff asked vendors if they would complete the project with a reduced scope. The lowest bidder and second lowest bidder withdrew their bids citing a lack of desire to work on an "a la carte" basis even though the specifications were clear this was an option for the City to exercise.

On August 17, 2021, the City of Crystal Lake publicly opened and read aloud the bids received for HVAC Duct Cleaning. The following is a breakdown of the bids received:

	√ Indoor Air Technologies Victor, NY	Mr. Duct Inc. Aurora, IL	Hughes Environmental Louisville, KY	Air Care Services Lisle, IL
Fire Dept. Offices	\$17,400	\$23,500	\$36,060	\$45,800
Council Chambers	\$12,400	\$7,423	\$12,400	\$4,760
<b>Grand Total</b>	<b>\$29,800</b>	<b>\$30,923</b>	<b>\$48,460</b>	<b>\$50,560</b>

√Indicates lowest responsive and responsible bidder

**Recommendation**

This contract is being presented pursuant to a competitive bidding process. Under such process, the contract is to be awarded to the “lowest responsive and responsible bidder.” The lowest responsive and responsible bidder is the contractor: (i) whose bid substantially conforms to the material provisions of the bid specifications, (ii) who demonstrates the financial capacity and ability to undertake and complete the project in question in accordance with bid specifications, and (iii) whose bid price is lowest among the responsive and responsible bidders. Selecting a contractor on bases not set forth in the bid specifications can lead to challenges to the City’s award.

The Public Works Department has reviewed bids received for completeness and accuracy in accordance with the invitation to bid document. After receiving satisfactory reference checks, staff recommends that the contract for HVAC Duct Cleaning be awarded to the lowest responsive and responsible bidder, Indoor Air Technologies, in the amounts bid.

In addition to the cleaning costs, there will be an expense by the City for an independent outside environmental consultant to perform before and after air sampling and testing of the areas that will be cleaned. Funding is available in the FY2021/2022 Capital Budget.

**Votes Required to Pass:**

Simple majority

**DRAFT**



## **RESOLUTION**

**WHEREAS** the CITY OF CRYSTAL LAKE received and publicly opened bids for Municipal Complex HVAC Duct Cleaning Project on March 17, 2020; and

**WHEREAS** the bids were not awarded because of the onset of the COVID-19 pandemic; and

**WHEREAS** the CITY OF CRYSTAL LAKE received and publicly opened bids for Municipal Complex HVAC Duct Cleaning Project on March 23, 2021; and

**WHEREAS** several bidders withdrew bids due to reduction in project scope;

**WHEREAS** the CITY OF CRYSTAL LAKE bid the project again and received and publicly opened bids for Municipal Complex HVAC Duct Cleaning Project on August 17, 2021 and

**WHEREAS** the lowest responsive and responsible bidder is Indoor Air Technologies, Victor, NY.

**NOW THEREFORE BE IT RESOLVED BY THE MAYOR AND CITY COUNCIL OF THE CITY OF CRYSTAL LAKE** that the foregoing recitals are repeated and incorporated as though fully set forth herein; and

**BE IT FURTHER RESOLVED** that the bids of March 17, 2020 and March 23, 2021 are hereby rejected; and

**BE IT FURTHER RESOLVED** that the City Manager is authorized to execute a contract between the CITY OF CRYSTAL LAKE and Indoor Air Technologies for the Municipal Complex HVAC Duct Cleaning Project in the amounts bid; and

**BE IT FURTHER RESOLVED** that the City Manager is authorized to execute change orders for up to 10% of the contract amount and to approve warranted completion date change orders relating to the contract.



**DRAFT**

DATED this 7<sup>th</sup> day of September, 2021.

CITY OF CRYSTAL LAKE, an  
Illinois municipal corporation,

By: \_\_\_\_\_  
Haig Haleblian, MAYOR

SEAL

ATTEST

\_\_\_\_\_  
Nick Kachiroubas, CITY CLERK

PASSED: September 7, 2021

APPROVED: September 7, 2021



## Agenda Item No: 17

### City Council Agenda Supplement

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**Meeting Date:** September 7, 2021

**Item:** Proposal Award - Consultant Selection for Preliminary Design Engineering Services for new Deep Well 20 and modifications to Water Treatment Plant 4.

**Staff Recommendation:** Motion to award the proposal for Preliminary Design Engineering Services for new Deep Well 20 and modifications to Water Treatment Plant 4 to the most responsible and responsive proposer, Fehr Graham, and adopt a Resolution authorizing the City Manager to execute an agreement with Fehr Graham in the amount of \$183,670.00, execute change orders for up to 10 percent of the agreement amount, and approve necessary completion date change orders relating to the agreement.

**Staff Contact:** Michael Magnuson, Director of Public Works and Engineering

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**Background:**

Based on current and emerging Environmental Protection Agency (EPA) regulations at the State and Federal levels, it is desirable to install a new deep well in the vicinity of Water Treatment Plant 4 (Knaack Boulevard) and designate the existing three shallow wells as emergency back-up wells. The scope of the preliminary engineering includes a review of potential well sites within a mile of Water Treatment Plant 4. Once a site is selected, preliminary engineering will be developed for a transmission main and identification of required modifications to Water Treatment Plant 4 to accommodate the new water source. The next steps in this project include acquisition of property and easements, final engineering (bid plans and specifications) followed by contract construction and construction observation. These phases of the project will be included in upcoming budgets.

**Discussion:**

***Consultant Selection Process***

The Public Works Department followed the City's Purchasing Policy to secure a consulting firm to address this project. City staff conducted an open solicitation Request for Proposal (RFP) for preliminary design engineering services. The City received proposals from Fehr Graham and Stanley Consultants. Baxter and Woodman, Inc., and HR Green both responded that they do not have the technical staff currently available to complete the work.

Fehr Graham and Stanley Consultants have worked for the City previously with very good results. Staff performed a review of the proposals based on qualifications and then factored in cost to arrive

at the firm that provided the best value to the City (value analysis). The results of the value analysis are depicted below:

**Well #20 and WTP #4 Preliminary Engineering Consultant RFP's Value Analysis**

<b>Proposal Ranking</b> Excellent = 5 Very Good = 4 Good = 3 Acceptable = 2 Poor = 1 Unacceptable = 0	Committee's Relative Weight of Importance	Fehr Graham		Stanley Consultants	
		Rating (0-5)	Weighted Rating	Rating (0-5)	Weighted Rating
<b>Criteria</b>	<b>(1-10)</b>				
Past Performance of firm and/ or Project Manager on City Projects	8	4	32.00	4	32.00
Project Manager and firm experience	10	4	40.00	4	40.00
Experience and expertise of project team in their areas of speciality	10	4	40.00	4	40.00
General Understanding of Project Scope: Are key areas/scope items identified and a clear understanding of the work needed to address outlined in the scope?	8	4.5	36.00	4	32.00
Discussion/ explanation of Alternative Analysis process	4	4.5	18.00	4	16.00
Are the project hours by task appropriate and align with other consultant's submittals and past experience.	8	3	24.00	3	24.00
Project examples are pertinent and aligned with the proposed scope	8	4	32.00	4	32.00
Senior/ lead project team members have relevant experience in project examples	7	4	28.00	4	28.00
<b>Total Weighted Rating</b>	<b>63</b>		<b>250.00</b>		<b>244.00</b>
<b>PROPOSAL RANKING</b>			<b>3.97</b>		<b>3.87</b>
Total Cost			\$183,670.00		\$197,025
Relative Cost Ranking (Formula ) 3 = avg. cost, points awarded/deducted for fee compared to avg)			<b>3.11</b>		<b>2.89</b>
<b>TOTAL VALUE RANKING =</b>			<b>3.54</b>		<b>3.38</b>

Fehr Graham has experience in both deep and shallow water wells and has completed similar projects for Freeport, Pingree Grove and along with similar water treatment experience for the City of Rockford. Their staff includes both engineers and personnel experienced at running facilities (operator) that will provide valuable insight into the design and future function of the well and plant modification.

**Recommendation:**

This contract is being presented pursuant to a request for proposal (RFP) process. Under such process, the contract is to be awarded to the vendor whose proposal "will be the most advantageous to the City." City staff has reviewed the proposals received, and determined that the recommended contractor is the most well-suited for the project.

Based upon a review of the RFP requirements, qualifications, and cost, Fehr Graham is the most responsible and responsive proposer for the Preliminary Design Engineering Services for the new deep well 20 and modifications to Water Treatment Plant 4. Staff has satisfactorily worked with Fehr Graham on recent projects such as the McCormick Park area water main project, Wastewater Treatment Plant 2 Alum Tank replacement and the phosphorous reduction feasibility studies for both wastewater plants.

Funds have been budgeted for this work.

**Votes Required to Pass:**

Simple majority.



**DRAFT**



## **RESOLUTION**

**WHEREAS**, it is necessary to adequately plan for the City's future drinking water needs and satisfy State and Federal environmental regulations; and

**WHEREAS**, the City has identified the need to install a new deep well to meet these needs and publically sought Preliminary Design Engineering Services;

**NOW THEREFORE BE IT RESOLVED BY THE MAYOR AND CITY COUNCIL OF THE CITY OF CRYSTAL LAKE** that the foregoing recitals are repeated and incorporated as though fully set forth herein; and

**BE IT FURTHER RESOLVED** that the City Manager is authorized to execute an agreement with Fehr Graham for consulting preliminary design engineering services for new Deep Well 20 and Water Treatment Plant 4 modifications in the amount of \$183,670.00 and allowing for a 10 percent contingency; and

**BE IT FURTHER RESOLVED** that the City Manager is authorized to execute change orders for up to 10 percent of the contract amount and to approve warranted completion date change orders relating to the contract.

**DATED** this 7<sup>th</sup> day of September, 2021.

CITY OF CRYSTAL LAKE, an  
Illinois municipal corporation,

By: \_\_\_\_\_  
Haig Haleblian, MAYOR

**DRAFT**

SEAL

ATTEST

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Nick Kachiroubas, CITY CLERK

PASSED: September 7, 2021  
APPROVED: September 7, 2021



**Agenda Item No: 18**

**City Council  
Agenda Supplement**

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**Meeting Date:**

September 7, 2021

**Item:**

Proposal Award - Consultant Selection for Preliminary Engineering Services for the Re-establish Crystal Creek Project

**Staff Recommendation:**

Motion to award the proposal for Preliminary Engineering Services for the Re-establish Crystal Creek Project to the most responsible and responsive proposer, Christopher B. Burke Engineering (CBBEL), and adopt a Resolution authorizing the City Manager to execute an agreement with Christopher B. Burke Engineering in the amount of \$92,820.00, execute change orders for up to 10 percent of the agreement amount, and approve necessary completion date change orders relating to the agreement.

**Staff Contact:**

Michael Magnuson, P.E., Director of Public Works and Engineering

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**Background:**

Crystal Creek is an open channel from the outlet of the lake to the City limits near Rakow Road with one exception, the stretch between Highland Avenue and St. Andrews Lane, between South Elementary School and Lundahl Middle School, where the creek enters an undersized failing storm sewer. The area of the creek that is located in storm sewers causes a pinch point for water flowing through the creek. During heavy storms, water cannot get into the storm sewer and it floods areas upstream including the Crystal Lake Country Club parking lot and tennis courts and affects the Creek levels upstream of Country Club Road. Flooding also occurs through the school properties during heavy rain events. Reestablishing the creek will allow flood waters to flow in the designated creek area and reduce flooding to the surrounding area.

The exhibit below shows the sections of the creek that currently is open channel flow (blue dotted line), the recently replaced and upsized culverts (yellow rectangles) and the section that is currently in an undersized storm sewer (red dashed line)

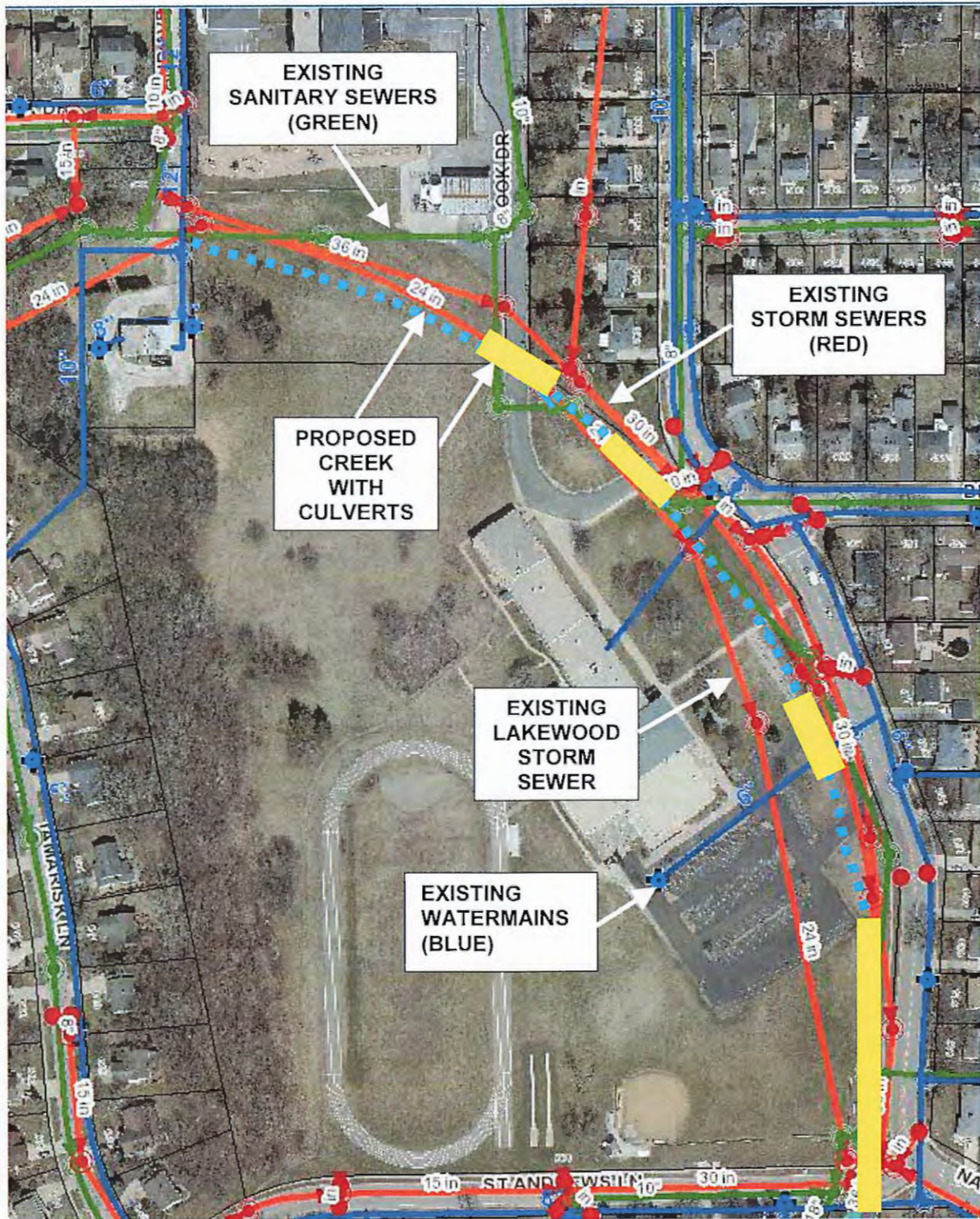




The proposed project will re-establish the open channel creek at this location and remove the existing storm sewers. The project is located in the regulatory floodplain and floodway of Crystal Creek and will require extensive coordination with the Army Corps of Engineers, Illinois Department of Natural Resources and the Illinois Environmental Protection Agency.

This project is very complex. In the same area that the creek runs underground in storm sewer is a Village of Lakewood storm sewer (lower than the creek) along with City of Crystal Lake sanitary sewer. The project will impact several driveways to the school and will be adjacent to an existing baseball field (see below). The preliminary engineering for this project requires not only expertise in drainage analysis, storm sewer design and environmental permitting, but also requires expertise in roadway design, parking lot design, and utility engineering (sanitary sewer).





EXISTING SEWERS AND WATER MAINS IN VICINTY OF PROPOSED CREEK RE-ESTABLISHMENT

The preliminary engineering is the first step towards implementing this project. After the preliminary engineering is complete, the permitting requirements for the project will be known, the cost estimate refined, the impacts identified (both environmental and school grounds impacts.) Enough information will have been gathered and analyzed to apply for Illinois Environmental Protection Agency (IEPA) Section 319 grant funding to help fund the project. Acquisition of easements, final engineering and permitting will follow next followed by construction. Extensive coordination with School District 47 will be required. Staff has already had preliminary discussions with District 47 staff.

### ***Consultant Selection Process***

The Public Works Department followed the City's Purchasing Policy to secure a consulting firm to address this project. City staff conducted an open solicitation Request for Proposal (RFP) whereby any consulting firm can submit. However, the RFP did outline specific experience and expertise requirements in the RFP to ensure proposing consultants had the qualifications to complete the preliminary engineering successfully. These requirements included experience with the Chicago District of the United States Army Corps of Engineers, hydraulic and hydrologic modeling (HEC-RAS and XP-SWMM computer models), and civil and geometric design.

An in-depth evaluation will be needed of the existing water main, sanitary sewer, Lakewood storm sewer, utilities, and school access drives. The selected consultant will need experience along with a creative design process to minimize impacts to all these existing facilities to ensure a final product that meets the needs of not only the City but the school as well.

Six firms submitted proposals. Of the six, two firms, Hey and Associates and Fehr Graham did not demonstrate in their proposal submittal that they have the full range of experience and expertise needed for this specific project. As noted above, this project is complex not only from a drainage analysis perspective, but all the various utilities present (water, sanitary) and the likely need to adjust the driveways and parking areas. It requires an engineering consulting firm with a broad range of civil engineering capabilities.

The consultants were evaluated on 10 qualification factors (scale 0-5 with 5 being the highest) and then evaluated based on cost. The two rankings (qualifications and cost) were then averaged to arrive at a final value ranking depicted below:



**Re-establish Crystal Creek Preliminary Engineering Proposal Value Analysis**

Performance Rating Excellent as Demonstrated in Proposal = 5 Very Good as Demonstrated in Proposal = 4 Good as Demonstrated in Proposal = 3 Acceptable as Demonstrated in Proposal = 2 Poor as Demonstrated in Proposal = 1 Qualification Not Demonstrated in Proposal = 0	Weight of Importance (1-10)	Alfred Benesch		Christopher B Burke Eng.		Fehr Graham		Gewalt Hamilton		GSG Consultants		Hey & Associates		
		Rating (0-5)	Weighted Rating	Rating (0-5)	Weighted Rating	Rating (0-5)	Weighted Rating	Rating (0-5)	Weighted Rating	Rating (0-5)	Weighted Rating	Rating (0-5)	Weighted Rating	
		Criteria												
Firm experience with Chicago District ACOE Permitting based on information provided in proposal	10	5	50.00	5	50.00	0	0.00	5	50.00	5	50.00	5	50.00	
Firm experience with HEC-RAS based on information provided in proposal	5	5	25.00	4	20.00	5	25.00	5	25.00	1	5.00	0	0.00	
Firm experience with XP-SWMM based on information provided in proposal	5	0	0.00	5	25.00	0	0.00	5	25.00	0	0.00	5	25.00	
Project manager experience with HEC-RAS in projects	5	0	0.00	2	10.00	1	5.00	4	20.00	5	25.00	0	0.00	
Project manager experience with XP-SWMM in projects	5	0	0.00	5	25.00	0	0.00	4	20.00	5	25.00	0	0.00	
Staff experience with projects involving geometric design of roadways and parking lots and roadway/pavement design based on information provided in proposal	10	3	30.00	3	30.00	3	30.00	3	30.00	5	50.00	0	0.00	
Staff experience with projects involving utility design (sanitary sewer and water) based on information provided in proposal	8	0	0.00	3	24.00	4	32.00	2	16.00	4	32.00	0	0.00	
Are the project hours by task appropriate and align with other consultant's submittals and past experience.	7	1	7.00	5	35.00	2	14.00	5	35.00	5	35.00	1	7.00	
Are adequate hours provided to fully develop design alternatives including identification of utility conflicts/adjustments/relocations and adjustments to roadways, parking lots, etc	7	2	14.00	2	14.00	3	21.00	2	14.00	3	21.00	3	21.00	
Experience with 319 Grant Funding	5	0	0.00	5	25.00	1	5.00	5	25.00	0	0.00	4	20.00	
<b>Total Weighted Rating</b>	<b>74</b>		<b>126.00</b>		<b>258.00</b>		<b>132.00</b>		<b>260.00</b>		<b>243.00</b>		<b>123.00</b>	
<b>PERFORMANCE RATING</b>			<b>1.70</b>		<b>3.49</b>		<b>1.78</b>		<b>3.51</b>		<b>3.28</b>		<b>1.66</b>	
<b>Total Cost</b>			<b>170,252.00</b>		<b>92,820.00</b>		<b>75,293.95</b>		<b>135,128.00</b>		<b>111,970.00</b>		<b>59,580.00</b>	
<b>Relative Cost Ranking (Formula) (3 points to average, prorate others)</b>			<b>1.25</b>		<b>3.41</b>		<b>3.90</b>		<b>2.23</b>		<b>2.88</b>		<b>4.34</b>	
<b>TOTAL VALUE RANKING =</b>			<b>1.5</b>		<b>3.4</b>		<b>2.8</b>		<b>2.9</b>		<b>3.1</b>		<b>3.0</b>	

Based on the above Value Ranking (combination of qualification and costs), Christopher B. Burke Engineering is the most responsive proposer possessing the needed qualifications to successfully complete the project while offering the most advantageous cost to the City.

**Recommendation:**

This contract is being presented pursuant to a request for proposal (RFP) process. Under such process, the contract is to be awarded to the vendor whose proposal “will be the most advantageous to the City” and not necessarily the lowest cost proposer. City staff has reviewed the proposals received, and determined that the recommended consultant is the most well-suited for the project.

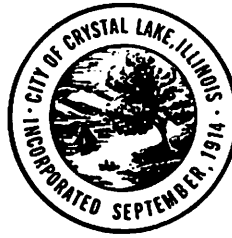
Staff has successfully worked with CBBEL on many projects over the last 10 years and has always been satisfied with their knowledgeable staff. Their office has obtained more than 2,000 Army Corp of Engineers permits and the project engineer has extensive experience with both types of hydraulic and hydrologic modeling. They have partnered with RES Great Lakes (AES) who has successfully assisted many municipalities in obtaining IEPA Section 319 grant funding. CBBEL also has a knowledgeable civil (utilities) and geometric design section (roadways and parking). CBBEL successfully completed design engineering on various City stormwater solutions projects including the Crystal Creek Culverts and the North Shore Drainage improvement. CBBEL has been the City’s stormwater consultant since 2008.

Funds have been budgeted for this work.

**Votes Required to Pass:**

Simple majority.

**DRAFT**



## **RESOLUTION**

**WHEREAS**, Crystal Creek enters two undersized failing storm sewers between Highland Avenue and St. Andrews Lane; and

**WHEREAS**, these storm sewers cause upstream flooding; and

**WHEREAS**, it is desirable to re-establish Crystal Creek to reduce flooding and improve water quality; and

**WHEREAS**, based on an evaluation of qualifications and cost through the Request for Proposal (RFP) process it recommended that the Proposal for the preliminary engineering be awarded to Christopher B. Burke Engineering;

**NOW THEREFORE BE IT RESOLVED BY THE MAYOR AND CITY COUNCIL OF THE CITY OF CRYSTAL LAKE** that the foregoing recitals are repeated and incorporated as though fully set forth herein; and

**BE IT FURTHER RESOLVED** that the City Manager is authorized to execute an agreement with Christopher B. Burke Engineering for consulting preliminary engineering services for the Re-establish Crystal Creek Project in the amount of \$92,820.00 and allowing for a 10 percent contingency; and

**BE IT FURTHER RESOLVED** that the City Manager is authorized to execute change orders for up to 10 percent of the contract amount and to approve warranted completion date change orders relating to the contract.

**DRAFT**

**DATED** this 7<sup>th</sup> day of September, 2021.

CITY OF CRYSTAL LAKE, an  
Illinois municipal corporation,

By: \_\_\_\_\_  
Haig Haleblian, MAYOR

SEAL

ATTEST

\_\_\_\_\_  
Nick Kachiroubas, CITY CLERK

PASSED: September 7, 2021

APPROVED: September 7, 2021





**Agenda Item No: 19**

**City Council  
Agenda Supplement**

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<b><u>Meeting Date:</u></b>	September 7, 2021
<b><u>Item:</u></b>	Intergovernmental Funding Agreement for the Pingree Train Station Non-Structural Facility Repairs
<b><u>Staff Recommendation:</u></b>	Motion to adopt a Resolution authorizing the City Manager to execute an intergovernmental agreement with Metra for the Pingree Train Station Non-Structural Facility Repairs including grant funding by Metra in the amount of \$941,032.78 to perform the station facility repairs.
<b><u>Staff Contact:</u></b>	Mike Magnuson, P.E., Director of Public Works and Engineering Eric Helm, Deputy City Manager Nick Hammonds, Assistant to the City Manager

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**Background:**

The Pingree Train Station is located at the northwest intersection of Pingree Road and Congress Parkway and is owned by Metra Rail. Since its opening in 2005, the station has experienced concrete settlement, weather degradation to various station elements, overgrowth of plants in the detention basin, and deterioration of the parking lot infrastructure. The City has been in contact with Metra regarding the repair of the station over the past several years and has requested that Metra address the items needing repair. Under the proposed 2021 IGA, Metra will be providing funds for the City to coordinate and perform repairs to non-structural elements of the Pingree station facility as noted in Table 1. Since Metra does not have the expertise necessary to make facility and site repairs, it is common for Metra to repair its facilities through grants to local governments.

In 2018, the City and Metra conducted a facility assessment of the Pingree station. This evaluation identified specific items in the parking lot and on the site that need repair. Following this evaluation, the City's engineering staff reviewed the evaluation parameters and prepared specifications for the station's ultimate repair. Repairs will not be performed on the building, stairways, and tunnel. **The costs for these improvements will be covered by grant funding provided by Metra pursuant to the attached IGA. No additional unreimbursed costs will be incurred by the City outside of the scope of the grant funding other than staff time to coordinate bidding and project oversight. Project oversight will not result in additional staff salary costs. The lowest responsive and responsible bidders will be selected to perform the work outlined in the attached scope.**

Repairs Cost As Outlined Per Location	\$635,159.00
Bidding, Engineering, and Construction Oversight	\$149,034.98
Contingency (20%)	\$156,838.80
<b>Total Repairs Cost Estimate:</b>	<b>\$941,032.78</b>

The attached repairs cost estimate details the line item repairs and costs as verified by City Engineering staff. City staff is confident that the repairs cost estimate is an accurate and sufficient estimate for the repairs. Metra will not reimburse the City for any amount above the repairs cost estimate of \$941,032.78, however, Metra may agree in writing to increase the amount of the grant funding, subject to the approval of Metra's Executive Director, in the event that the total repair cost exceeds the repairs cost estimate. Per the IGA, if the final repair cost is lower than the repairs cost estimate, the City will be reimbursed only for the expenses incurred.

This work includes only non-structural repairs to the Pingree station property. Additional repairs will be needed at a future date for items related to the station building, tunnel, stairs and ramps. Per Metra, these repairs will be coordinated and funded by Union Pacific. This repair work is anticipated to begin in 2022.

**Project Scope:**

City staff will conduct bidding and coordinate contractors to perform the work. Once the City has entered into contracts to perform the work, Metra will provide monthly progress payments to reimburse the City for completed repairs. It is anticipated that the City will put this project to bid this winter, with construction beginning in spring 2022. The following areas and repair work are included in the intergovernmental agreement.

**East and West Detention Basin:**

The east and west detention basins, also identified as Basin A and Basin C, will be cleared of overgrown shrubs and trees. Erosion control will be implemented after clearing is completed. In addition, fencing repairs and replacements will be performed for affected areas.

**Parking Lot and Entry Roadway:**

Crack sealing and new pavement markings will be completed for the parking lot. Ponding around parking islands and on sidewalks will be repaired. Settlement behind curbs and repairs to mortar frames will be addressed. In addition, erosion control will be performed for the landscaping at the south ramp.

**Recommendation:**

Staff recommends a motion to approve the Intergovernmental Funding Agreement for the Pingree Station Non-Structural Facility Repairs in the City of Crystal Lake with Metra and to authorize the City Manager to execute the necessary documents, including an agreement with Metra for the City to perform the facility repairs and be reimbursed by Metra through grant funding as provided in the attached IGA. The City's legal counsel has reviewed the agreement and finds it acceptable. The work will be publicly bid and brought back to the City Council for formal approval.

**Votes Required to Pass:**

Simple majority

**DRAFT**

Res. 21R-\_\_\_\_\_



**RESOLUTION**

BE IT RESOLVED BY THE MAYOR AND CITY COUNCIL OF THE CITY OF CRYSTAL LAKE that the City Manager be and he is hereby authorized and directed to execute an intergovernmental agreement with Metra providing for the Pingree Station Non-Structural Facility Repairs and accepting grant funding, pursuant to such agreement from Metra, in the amount of \$941,032.78 to perform the Pingree Station Non-Structural Facility Repairs.

DATED this 7<sup>th</sup> day of September, 2021.

CITY OF CRYSTAL LAKE, an  
Illinois Municipal Corporation,

By: \_\_\_\_\_  
Haig Haleblian, MAYOR

SEAL

ATTEST

\_\_\_\_\_  
Nick Kachiroubas, CITY CLERK

PASSED: September 7, 2021

APPROVED: September 7, 2021



**DRAFT**

**INTERGOVERNMENTAL FUNDING AGREEMENT FOR NON-STRUCTURAL REPAIRS  
TO THE PINGREE ROAD STATION PROPERTY IN THE CITY OF CRYSTAL LAKE**

**THIS INTERGOVERNMENTAL AGREEMENT (“Agreement”)** made and entered into this \_\_\_ day of \_\_\_\_\_, 2021, by and between the Commuter Rail Division of the Regional Transportation Authority, a division of an Illinois municipal corporation (“**Metra**”) and the City of Crystal Lake, an Illinois municipal corporation (“**Municipality**”).

**RECITALS**

A. The Constitution of the State of Illinois, Article VII, Section 10, provides that units of local municipalities and school districts may contract among themselves in any manner not prohibited by law or by ordinance.

B. The Intergovernmental Cooperation Act, 5 ILCS 220/1 et seq., authorizes public agencies in Illinois to exercise jointly with any other public agency any power or powers, privileges, functions or authority which may be exercised by a public agency, individually, and to enter into contracts for the performance of governmental services, activities, and undertakings.

C. Metra has the authority to cooperate with other governmental agencies and desires to contribute grant funds to the Municipality to conduct certain repairs to the signage, landscaping and pavement/surface of the of the commuter parking facility located at the Pingree Road Station (the “**Project**”).

D. The Municipality is authorized to cooperate with Metra in the exercise of its powers and agrees to perform, or cause to be performed, the Project at Metra’s Pingree Road commuter facility located on the Union Pacific NorthWest Line, in Crystal Lake (“**Premises**”).

E. Metra’s goal in providing the agreed upon grant funds to the Municipality is to assure its commuter station facility is in a state of good repair.

F. Metra has determined that it is in the best interest of the parties to provide the Municipality the necessary grant funding for the performance of the Project.

**AGREEMENT**

**NOW, THEREFORE**, for and in consideration of the foregoing Recitals, which are hereby incorporated into and made a part of this Agreement, the mutual covenants and agreements set forth herein, and other good and valuable consideration, the receipt and sufficiency of which are hereby acknowledged and accepted by the parties, the parties agree as follows:

**1. THE PROJECT.** The Municipality agrees, subject to the reimbursement provided pursuant to this Agreement, to undertake and complete the Project, approved by Metra and more specifically described on **Exhibit A**, attached to and made a part of this Agreement, (“**Project Scope and Cost Estimate**”).

2. **AMOUNT OF GRANT.** Metra agrees to provide grant funding to the Municipality in an amount not to exceed \$942,000 (“**Grant**” or “**Grant Funds**”). Metra, at its sole discretion, may agree in writing to increase the amount of the Grant Funding subject to the approval of Metra’s Executive Director, but in no event shall the total amount provided by Metra under this Agreement exceed the actual amounts paid or expenses incurred by the Municipality for performance and completion of the Project. Metra is not liable for any amount in excess of the amount of the Grant Funding. The Municipality agrees that it will provide, or cause to be provided, the cost of project elements which are not approved for Metra’s participation.

3. **METRA’S OBLIGATIONS.**

(a) Metra has reviewed and approved of the Project Scope and Cost Estimate, attached hereto as Exhibit A.

(b) Metra agrees to pay Municipality the Grant Funds pursuant to the terms and conditions of this Agreement.

(c) Metra reserves the right to inspect the Project at any and all stages of Work, as later defined herein, and the right to audit the funding transaction and use of said funds.

4. **MUNICIPALITY’S OBLIGATIONS.**

(a) Municipality shall be responsible for providing the Project Scope and Cost Estimate for the Project, unless Metra agrees in writing to provide such information. Metra will be under no obligation to pay for any work performed prior to Metra approving the Repairs Cost Estimate.

(b) Municipality shall be responsible for the performance of the Project elements (“**Work**”) or causing the Work to be performed in a good and workmanlike manner and in accordance with the Project Scope and this Agreement.

(c) Municipality agrees to comply with all applicable federal laws, state laws and regulations and shall obtain all necessary permits, licenses, consents and other approvals for the performance of the Work.

(d) To the fullest extent permitted by law, the Municipality agrees to indemnify, defend and hold harmless Metra, the Regional Transportation Authority (“**RTA**”), the Northeast Illinois Regional Commuter Railroad Corporation (“**NIRCRC**”) and the Union Pacific Railroad, a Utah corporation (“**UPRR**”), their respective directors, administrators, officers, agents, employees, successors, assigns and all other persons, firms and corporations acting on their behalf or with their authority, from and against any and all injuries, liabilities, losses, damages, costs, payments and expenses of every kind and nature (including, without limitation, court costs and attorneys’ fees) for claims, demands, actions, suits, proceedings, judgments, settlements arising out of or in any way relating to or occurring in connection with the Project or this Agreement. The indemnities contained in this Section shall survive termination of this Agreement.

(e) Municipality shall permit, and shall require its contractors to permit, Metra or its designated agents to inspect all work, materials, payrolls, and other data, and records with regard to the Project and to audit the books, records, and accounts of Municipality and its contractors with regard to the Project.

**5. JOINT OBLIGATIONS.**

(a) The parties agree to do all things reasonably necessary or appropriate to carry out the terms and provisions of this Agreement and to aid and assist each other in furthering the objectives of this Agreement, and the intent of the parties as reflected by the terms of this Agreement, including, without limitation, the enactment of such resolutions and ordinances, the execution of such permits, applications and agreements, and the taking of such other actions as may be necessary to enable the parties' compliance with the terms and provisions of this Agreement, and as may be necessary to give effect to the objectives of this Agreement and the intentions of the parties as reflected by the terms of this Agreement.

(b) Neither party shall assign this Agreement to any person or entity without the prior written consent of the other party.

(c) Municipality and Metra agree that this Agreement is for the benefit of the parties and not for the benefit of any third-party beneficiary. No third party shall have any rights or claims against Metra or the Municipality arising from this Agreement.

(d) The parties understand that a Railroad flagman may be required whenever Municipality or its contractor is performing the Work on Metra property or other railroad property for the purposes set forth herein, the cost of which will be borne by Metra. In the event it is determined flagging will be required pursuant to a work schedule ("**Schedule**") provided by Municipality, then Metra and Municipality agree to cooperate in scheduling the flagging to facilitate the Project. In the event the Work is being performed on the property of another railroad, but is being funded by Metra, then in that event, the Municipality or its contractor may need to enter into an Entry Agreement with the other railroad, and Metra agrees to provide the Municipality any such additional funding to compensate the Municipality for any costs associated with access to another railroad's property or the flagging required by the other railroad.

**6. NO OBLIGATIONS TO THIRD-PARTIES.** Neither Metra nor any state or federal funding agency shall be subject to any obligations or liabilities of contractors of the Municipality or their subcontractors or any other person not a party to this Agreement without Metra's specific consent. This limitation shall apply despite the fact that Metra concurred in or approved of the award of any contract, subcontract, or the solicitation thereof. Unless expressly authorized in writing by Metra, the Municipality agrees to refrain from executing any transfer of title, lease, lien, pledge, mortgage, encumbrance, contract, grant anticipation note, alienation, or other obligation that in any way would affect Metra's interest in any Project Facilities or obligating itself in any manner to any third-party with respect to Project Facilities.



**7. CONTRACTOR INDEMNIFICATION AND INSURANCE.**

(a) In all contracts executed by Municipality for the Project and performance of the Work on the Premises, or to be located on such Premises, Municipality will require appropriate clauses to be inserted requiring contractors to indemnify, hold harmless and defend Metra, the RTA the NIRCRC, and the UPRR, their directors, employees, agents, licensees, successors and assigns from and against any and all risks, liabilities, claims, demands, losses, and judgments, including court costs and attorneys' fees, arising from, growing out of, or related in any way to work performed by such contractor(s), or their officers, employees, agents or subcontractors, and their agents or employees.

(b) Municipality will further require its contractor to obtain any insurance that may be required by Metra and cause appropriate clauses to be inserted in all such contracts requiring contractors to procure and maintain comprehensive policies of insurance, insuring contractor, Metra, the RTA the NIRCRC, and the UPRR, their directors, employees, agents, successors and assigns from and against any and all risks, liabilities, claims, demands, losses and judgments, including court costs and attorneys' fees, arising from, growing out of or in any way related to the work performed or to be performed by such contractor(s), whether or not any such liability, claim, demand, loss or judgment is due to or arises from the acts, omissions or negligence of such contractor(s), or their officers, employees, agents or subcontractors and their agents or employees.

**8. ELIGIBLE COSTS.**

(a) Expenditures incurred by Municipality relating to the Project shall be reimbursable to the Municipality under the Project as "Eligible Costs" to the extent they meet the requirements set forth below:

- i) Be necessary in order to accomplish the Project; and
- ii) Be satisfactorily documented.

(b) In the event that it may be impractical to determine exact costs of indirect or service functions, Eligible Costs will include such allowances for these costs as may be approved in writing by Metra.

**9. PAYMENT BY METRA.** Metra will make progress payments to Municipality for Eligible Costs upon submittal of acceptable invoices to Metra along with any detailed information about the Work that may be required by Metra provided that invoices for the progress payments shall not be submitted more frequently than once each month. Metra shall process the invoice to verify that such costs are Eligible Costs incurred by Municipality and shall submit payment within 30 days of the date upon which such payment invoice was timely received. Municipality shall submit its final invoice(s) for actual costs incurred within 45 days after completion of the Project. Reimbursement of any cost pursuant to this Section shall not constitute a final determination by Metra of the allowability of such cost and shall not constitute a waiver of any violation of the terms of this Agreement committed by Municipality. Metra will make a final determination as to the allowability only after a final audit of the Project has been conducted.

**10. DOCUMENTATION OF PROJECT COSTS.** All costs charged to the Project, including any approved services contributed by Municipality or others, shall be supported by properly executed payrolls, time records, invoices, contracts, or vouchers evidencing in detail the nature and property of the charges.

**11. AUDIT AND INSPECTION.** Municipality shall permit, and shall require its contractors to permit, Metra, the RTA, the NIRCRC, or any other state or federal agency providing grant funds, or their designated agents, authorized to perform such audit and inspection, to inspect all work, materials, payrolls, and other data and records with regard to the Project, and to audit the books, records, and accounts of Municipality and its contractors with regard to the Project. Metra also may require the Municipality to furnish, at any time prior to close-out of the Project, audit reports prepared according to generally accepted accounting principles at Metra's expense. Municipality agrees to promptly comply with recommendations contained in Metra's final audit report. Any audit that may be required by Metra shall be at Metra's sole cost and expense.

**12. RIGHT OF METRA TO TERMINATE.** Upon written notice to Municipality, Metra reserves the right to suspend or terminate all or part of the financial assistance herein provided if Municipality is, or has been, in violation of the terms of this Agreement. Any failure to make progress which significantly endangers substantial performance of the Project within a reasonable time shall be deemed to be a violation of the terms of this Agreement. Termination of any part of the Grant Funds will not invalidate obligations properly incurred by Municipality and concurred in by Metra prior to the date of termination to the extent they are non-cancellable. The acceptance of a remittance by Metra of any or all Grant Funds previously received by Municipality or the closing out of Metra financial participation in the Project shall not constitute a waiver of any claim which Metra may otherwise have arising out of this Agreement. In the event of termination of this Agreement during the construction phase for reasons other than violation of the terms hereof by Municipality, Metra shall determine the most appropriate course of action to be taken with respect to the Project.

**13. PROJECT SETTLEMENT AND CLOSE-OUT.** Upon receipt of notice of successful completion of the Project or upon termination by Metra, Municipality shall cause a final audit to be performed of the Project to determine the allowability of costs incurred and make settlement of the Metra Grant. If Metra has made payments to Municipality in excess of the Total Project Cost of such Metra Grant or if Metra has advanced funds pursuant to an invoice submitted under Section 12 which exceed the Net Project Cost, Municipality shall promptly remit such excess funds to Metra. Project close-out occurs when Metra notifies Municipality and forwards the final Grant payment or when an appropriate refund of Metra Grant Funds has been received from Municipality and acknowledged by Metra. Grant Funds which have not been dispersed to the Municipality will automatically revert to Metra upon completion of the Project, provided that no outstanding invoices from the Municipality are pending submittal. Close-out shall be subject to any continuing obligations imposed on Municipality by this Agreement or contained in the final notification or acknowledgment from Metra.

**14. CONTRACTS AND PROJECT MANAGEMENT.** Municipality shall execute all contracts and perform all project management activities in accordance with the terms of this Agreement.

**15. COMPETITIVE BIDDING.** Municipality agrees to give full opportunity for free, open, and competitive bidding in accordance with state statutes, as applicable, and the Municipality's established rules, regulations and ordinances for each contract to be let by Municipality that requires constructing or furnishing of any materials, supplies, or equipment to be paid for with Grant Funds and Municipality shall give such publicity in its advertisements or calls for bids for each contract as will provide adequate competition. The award for each such contract shall be made by Municipality as soon as practicable to the lowest responsive and qualified bidder or as otherwise specifically approved by Metra.

**16. SETTLEMENT OF THIRD-PARTY CONTRACT DISPUTES OR BREACHES.** Metra has a vested interest in the settlement of disputes, defaults, or breaches involving any Metra-assisted third-party contracts. Metra retains a right to a proportionate share, based on the percentage of the Metra share committed to the Project, of any proceeds derived from any third-party recovery. Therefore, Municipality shall avail itself of all legal rights available under any third-party contract. Municipality shall notify Metra of any current or prospective litigation pertaining to any compromise or settlement of the Municipality's claim(s) involving any third-party contract, before making Metra assistance available to support that settlement. If the third-party contract contains a liquidated damages provision, any liquidated damages recovered shall be credited to the project account involved unless Metra permits otherwise.

**17. SEVERABILITY.** Metra and Municipality agree that if any provision of this Agreement is held invalid for any reason whatsoever, the remaining provisions shall not be affected thereby if such remainder would then continue to conform to the purposes, terms and requirements of applicable law.

**18. AMENDMENT.** Metra and Municipality agree that no change or modification to this Agreement or any Exhibits or attachments hereto, shall be of any force or effect unless such amendment is dated, reduced to writing, executed by both parties, and attached to and made a part of this Agreement. No work shall be commenced, and no costs or obligations incurred in consequence of any amendment to this Agreement or any attachments hereto unless and until such amendment has been executed and made a part of this Agreement and the Project Budget has been amended to conform thereto.

**19. COUNTERPARTS.** This Agreement may be simultaneously executed in several counterparts, each of which so executed shall be deemed to be an original, and such counterparts together shall constitute one and the same instrument.

**20. EXPENDITURE OF GRANT FUNDS.** Municipality agrees that the Grant Funds for this Project must be expended upon approved Project elements within 24 months of execution of the Grant contract. Unless otherwise specified in writing by Metra, all unexpended Grant Funds will automatically revert to Metra upon the expiration of this 24-month time period. In no event shall the term of this Agreement exceed 36 months from the date first mentioned above.

**21. ENTIRE AGREEMENT.** This Agreement represents the entire Agreement between Metra and Municipality and supersedes all prior negotiations and agreements. This Agreement shall be construed in accordance with the internal laws of the State of Illinois. This Agreement may be amended only by written instrument signed by both parties hereto.



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**22. NOTICES.** All notices, demands, elections, and other instruments required or permitted to be given or made by either party upon the other under the terms of this Agreement or any statute shall be in writing. Such communications shall be deemed to have been sufficiently served if sent by certified or registered mail with proper postage prepaid, hand delivered or sent by facsimile transmission, with proof of successful transmission sent by regular mail by the sending party at the respective addresses shown below, or to such other party or address as either party may from time to time furnish to the other in writing. Such notices, demands, elections and other instruments shall be considered delivered to recipient on the second business day after deposit in the U.S. Mail, on the day of delivery if hand delivered.

(a) Notices to Metra shall be sent to:

Commuter Rail Division  
547 W. Jackson Boulevard  
Chicago, Illinois 60661  
Attn: Chief of Staff  
Phone: (312) 322-6753

(b) Notices to Municipality shall be sent to:

City of Crystal Lake  
100 W. Woodstock Drive  
Crystal Lake, Illinois 60014  
Attn: City Manager  
Phone: (815) 459-2020

**23. GENERAL.**

(a) This Agreement and the rights and obligations accruing hereunder are binding upon the Parties and their respective heirs, legal representatives, successors and assigns. No waiver of any obligation or default of Municipality shall be implied from omission by Metra to take any action on account of such obligation or default, and no express waiver shall affect any obligation or default other than the obligation or default specified in the express waiver and then only for the time and to the extent therein stated.

(b) Section captions used in this Agreement are for convenience only and shall not affect the construction of this Agreement. Whenever the context requires or permits, the singular shall include the plural, the plural shall include the singular, and the masculine, feminine and neuter shall be freely interchangeable. In the event the time for performance hereunder falls on a Saturday, Sunday or holiday, the actual time for performance shall be the next business day.

(c) This Agreement shall be governed by the laws of the State of Illinois. This Agreement provides for the development and maintenance of real estate located within the State of Illinois and is to be performed within the State of Illinois. Accordingly, this Agreement, and all questions of interpretation, construction, and enforcement hereof, and all controversies hereunder shall be governed by the applicable statutory and common law of the State of Illinois.

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(d) This Agreement, together with the exhibits attached hereto (all of which are incorporated herein by this reference), constitutes the entire Agreement between the Parties with respect to the subject matter hereof.

(e) The execution, delivery of, and performance under this Agreement is pursuant to authority, validity and duly conferred upon the Parties and signatories hereto.

**IN WITNESS WHEREOF**, this Agreement is entered into by and between the Parties hereto as of the date and year first above written.

**CITY OF CRYSTAL LAKE:**

**COMMUTER RAIL DIVISION OF THE  
REGIONAL TRANSPORTATION  
AUTHORITY:**

By: \_\_\_\_\_  
Gary Mayerhofer, City Manager

By: \_\_\_\_\_  
James M Derwinski, CEO/Executive Director

**DRAFT**

**Exhibit A**  
**Project Scope and Cost Estimate**



**DRAFT**

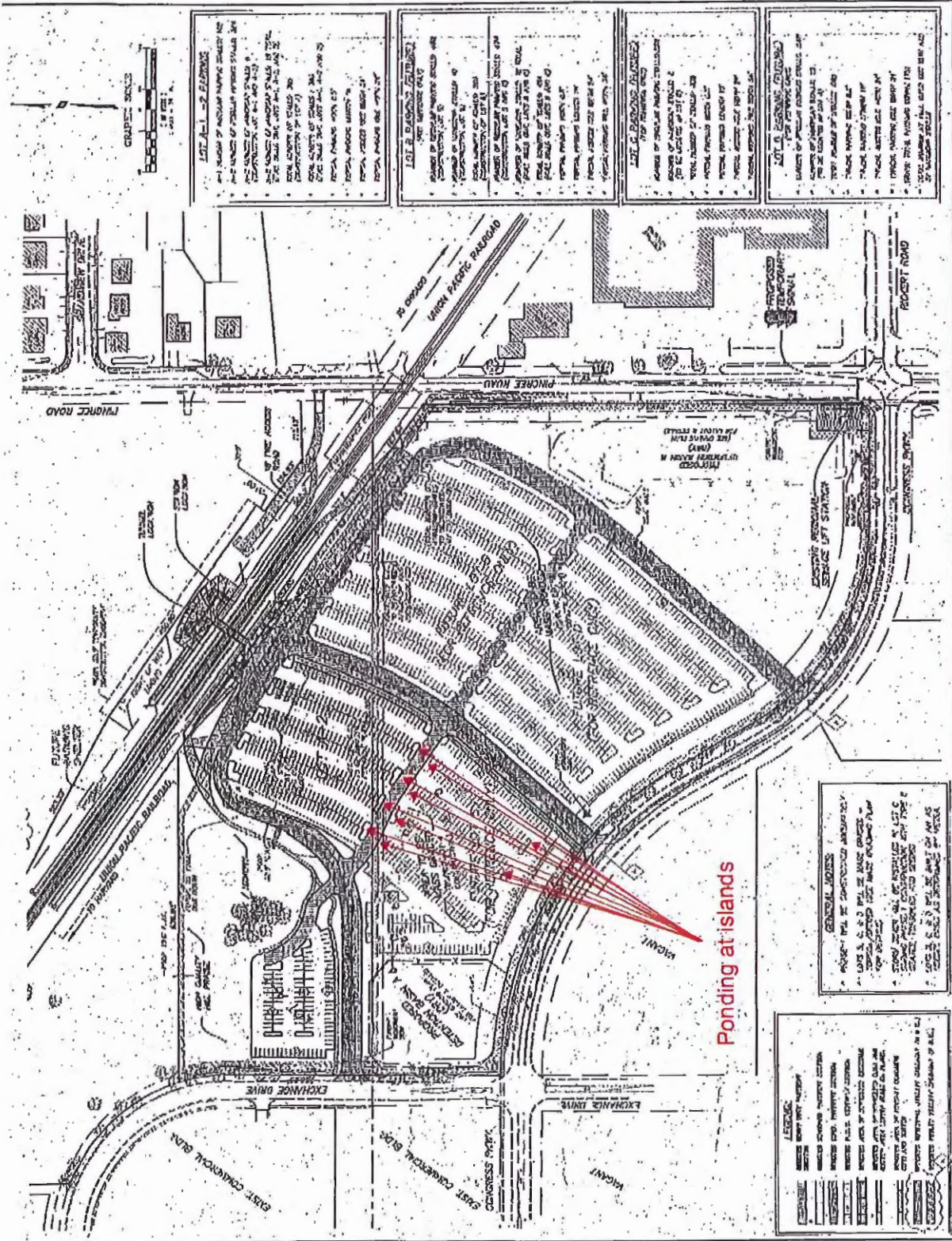
Pingree Station: Non-Structural Repairs

- Detention Basin A and C:
  - Clearing
  - Grubbing
  - Erosion Control
  - Tree Removal
- Parking Lot/Entry Roadway:
  - Crack Sealing
  - Storm Sewer: Mortar Frames
  - Pavement Markings
  - Backfill Settlement Behind Curbs
  - Signage Replacements
  - Tree Removal/Replacements
- Erosion Control:
  - Basins
  - Landscaping at West Ramp





**DRAFT**



**LOT 1 - 2 - 3 - 4 - 5 - 6 - 7 - 8 - 9 - 10 - 11 - 12 - 13 - 14 - 15 - 16 - 17 - 18 - 19 - 20 - 21 - 22 - 23 - 24 - 25 - 26 - 27 - 28 - 29 - 30 - 31 - 32 - 33 - 34 - 35 - 36 - 37 - 38 - 39 - 40 - 41 - 42 - 43 - 44 - 45 - 46 - 47 - 48 - 49 - 50 - 51 - 52 - 53 - 54 - 55 - 56 - 57 - 58 - 59 - 60 - 61 - 62 - 63 - 64 - 65 - 66 - 67 - 68 - 69 - 70 - 71 - 72 - 73 - 74 - 75 - 76 - 77 - 78 - 79 - 80 - 81 - 82 - 83 - 84 - 85 - 86 - 87 - 88 - 89 - 90 - 91 - 92 - 93 - 94 - 95 - 96 - 97 - 98 - 99 - 100**

**LOT 1 - 2 - 3 - 4 - 5 - 6 - 7 - 8 - 9 - 10 - 11 - 12 - 13 - 14 - 15 - 16 - 17 - 18 - 19 - 20 - 21 - 22 - 23 - 24 - 25 - 26 - 27 - 28 - 29 - 30 - 31 - 32 - 33 - 34 - 35 - 36 - 37 - 38 - 39 - 40 - 41 - 42 - 43 - 44 - 45 - 46 - 47 - 48 - 49 - 50 - 51 - 52 - 53 - 54 - 55 - 56 - 57 - 58 - 59 - 60 - 61 - 62 - 63 - 64 - 65 - 66 - 67 - 68 - 69 - 70 - 71 - 72 - 73 - 74 - 75 - 76 - 77 - 78 - 79 - 80 - 81 - 82 - 83 - 84 - 85 - 86 - 87 - 88 - 89 - 90 - 91 - 92 - 93 - 94 - 95 - 96 - 97 - 98 - 99 - 100**

**LOT 1 - 2 - 3 - 4 - 5 - 6 - 7 - 8 - 9 - 10 - 11 - 12 - 13 - 14 - 15 - 16 - 17 - 18 - 19 - 20 - 21 - 22 - 23 - 24 - 25 - 26 - 27 - 28 - 29 - 30 - 31 - 32 - 33 - 34 - 35 - 36 - 37 - 38 - 39 - 40 - 41 - 42 - 43 - 44 - 45 - 46 - 47 - 48 - 49 - 50 - 51 - 52 - 53 - 54 - 55 - 56 - 57 - 58 - 59 - 60 - 61 - 62 - 63 - 64 - 65 - 66 - 67 - 68 - 69 - 70 - 71 - 72 - 73 - 74 - 75 - 76 - 77 - 78 - 79 - 80 - 81 - 82 - 83 - 84 - 85 - 86 - 87 - 88 - 89 - 90 - 91 - 92 - 93 - 94 - 95 - 96 - 97 - 98 - 99 - 100**

**LOT 1 - 2 - 3 - 4 - 5 - 6 - 7 - 8 - 9 - 10 - 11 - 12 - 13 - 14 - 15 - 16 - 17 - 18 - 19 - 20 - 21 - 22 - 23 - 24 - 25 - 26 - 27 - 28 - 29 - 30 - 31 - 32 - 33 - 34 - 35 - 36 - 37 - 38 - 39 - 40 - 41 - 42 - 43 - 44 - 45 - 46 - 47 - 48 - 49 - 50 - 51 - 52 - 53 - 54 - 55 - 56 - 57 - 58 - 59 - 60 - 61 - 62 - 63 - 64 - 65 - 66 - 67 - 68 - 69 - 70 - 71 - 72 - 73 - 74 - 75 - 76 - 77 - 78 - 79 - 80 - 81 - 82 - 83 - 84 - 85 - 86 - 87 - 88 - 89 - 90 - 91 - 92 - 93 - 94 - 95 - 96 - 97 - 98 - 99 - 100**

**GENERAL NOTES**

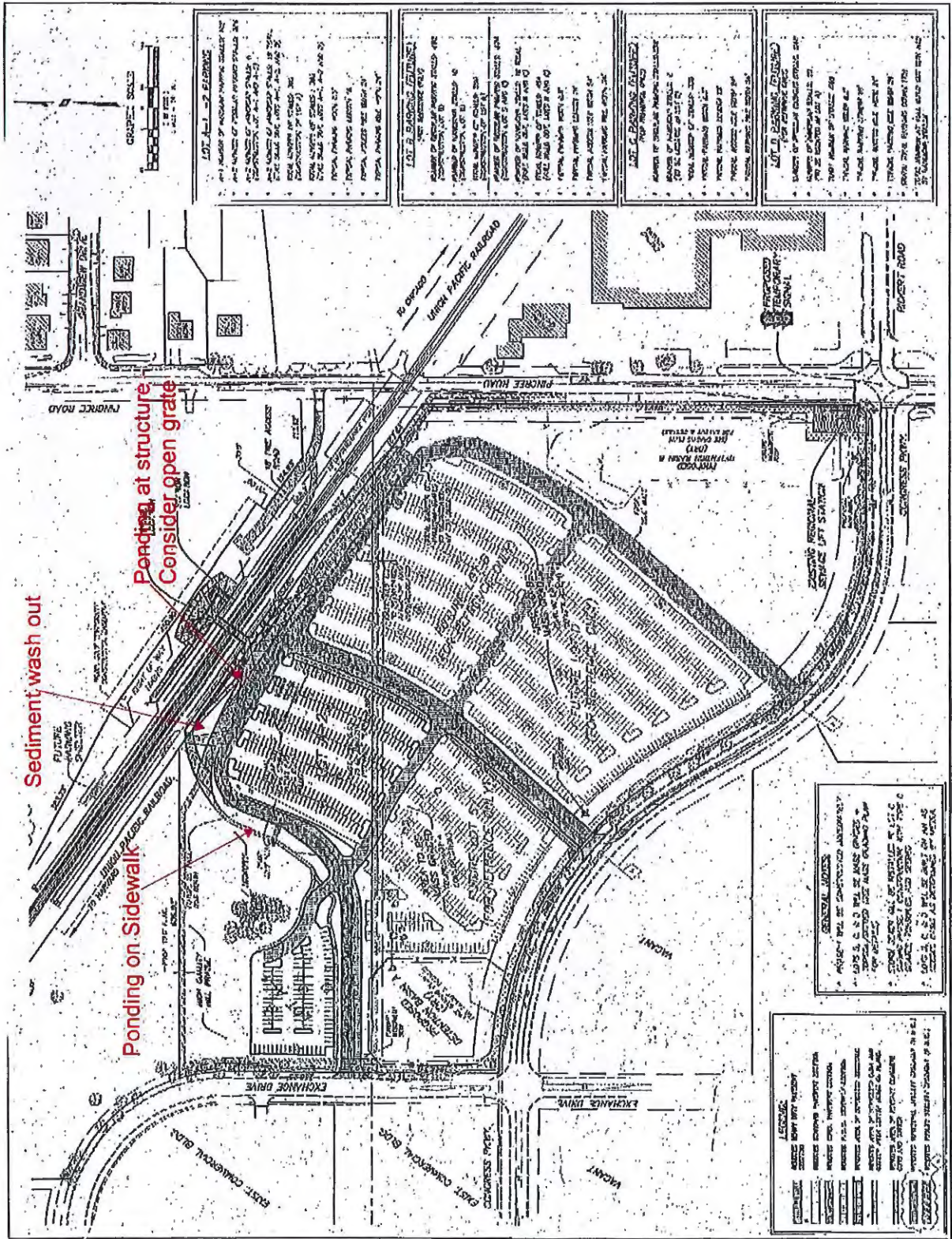
1. PROJECT HAS BE CONSTRUCTED ACCORDING TO THE PLAN SHEETS AND SPECIFICATIONS.
2. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, AND THE STANDARD SPECIFICATIONS FOR BRIDGE CONSTRUCTION, AND THE STANDARD SPECIFICATIONS FOR STRUCTURES.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES AND STRUCTURES.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING TREES AND LANDSCAPE.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING HISTORIC STRUCTURES AND MONUMENTS.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING CULTURAL RESOURCES.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING ARCHAEOLGICAL REMAINS.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING ANTHROPOLOGICAL REMAINS.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING GEOLOGICAL AND SOIL RESOURCES.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING PLANT AND ANIMAL RESOURCES.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING AIR AND WATER RESOURCES.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING VISUAL RESOURCES.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING SOUND RESOURCES.
15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING VIBRATION RESOURCES.
16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING CLIMATE RESOURCES.
17. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING SOCIAL RESOURCES.
18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING ECONOMIC RESOURCES.
19. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING CULTURAL RESOURCES.
20. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING HISTORIC RESOURCES.

**LEGEND**

- EXISTING BUILDING FOOTPRINT
- EXISTING DRIVEWAY
- EXISTING DRIVE
- EXISTING SIDEWALK
- EXISTING CURB
- EXISTING LANDSCAPE
- EXISTING TREES
- EXISTING UTILITIES
- EXISTING STRUCTURES
- EXISTING MONUMENTS
- EXISTING CULTURAL RESOURCES
- EXISTING ARCHAEOLGICAL REMAINS
- EXISTING ANTHROPOLOGICAL REMAINS
- EXISTING GEOLOGICAL AND SOIL RESOURCES
- EXISTING PLANT AND ANIMAL RESOURCES
- EXISTING AIR AND WATER RESOURCES
- EXISTING VISUAL RESOURCES
- EXISTING SOUND RESOURCES
- EXISTING VIBRATION RESOURCES
- EXISTING CLIMATE RESOURCES
- EXISTING SOCIAL RESOURCES
- EXISTING ECONOMIC RESOURCES
- EXISTING CULTURAL RESOURCES
- EXISTING HISTORIC RESOURCES

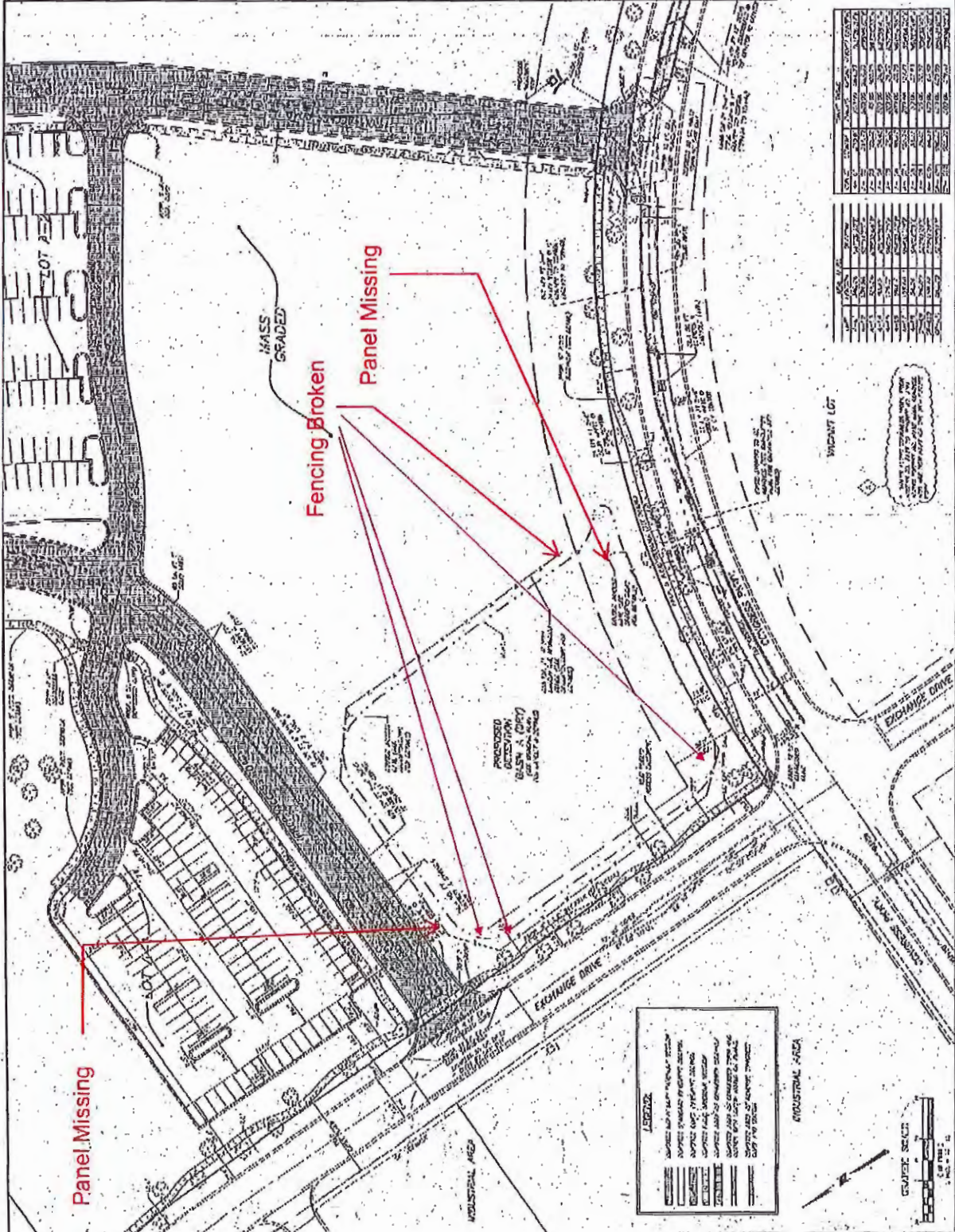
Ponding at islands







**DRAFT**



LOT NO.	AREA (SQ. FT.)	AREA (SQ. YD.)
1	10,000	1,111
2	12,000	1,333
3	15,000	1,667
4	18,000	2,000
5	20,000	2,222
6	22,000	2,444
7	25,000	2,778
8	28,000	3,111
9	30,000	3,333
10	32,000	3,556
11	35,000	3,889
12	38,000	4,222
13	40,000	4,444
14	42,000	4,667
15	45,000	5,000
16	48,000	5,333
17	50,000	5,556
18	52,000	5,778
19	55,000	6,111
20	58,000	6,444
21	60,000	6,667
22	62,000	6,889
23	65,000	7,222
24	68,000	7,556
25	70,000	7,778
26	72,000	8,000
27	75,000	8,333
28	78,000	8,667
29	80,000	8,889
30	82,000	9,111
31	85,000	9,444
32	88,000	9,778
33	90,000	10,000
34	92,000	10,222
35	95,000	10,556
36	98,000	10,889
37	100,000	11,111

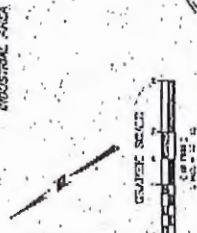
LOT NO.	AREA (SQ. FT.)	AREA (SQ. YD.)
38	102,000	11,333
39	105,000	11,667
40	108,000	12,000
41	110,000	12,222
42	112,000	12,444
43	115,000	12,778
44	118,000	13,111
45	120,000	13,333
46	122,000	13,556
47	125,000	13,889
48	128,000	14,222
49	130,000	14,444
50	132,000	14,667
51	135,000	15,000
52	138,000	15,333
53	140,000	15,556
54	142,000	15,778
55	145,000	16,111
56	148,000	16,444
57	150,000	16,667
58	152,000	16,889
59	155,000	17,222
60	158,000	17,556
61	160,000	17,778
62	162,000	18,000
63	165,000	18,333
64	168,000	18,667
65	170,000	18,889
66	172,000	19,111
67	175,000	19,444
68	178,000	19,778
69	180,000	20,000
70	182,000	20,222
71	185,000	20,556
72	188,000	20,889
73	190,000	21,111
74	192,000	21,333
75	195,000	21,667
76	198,000	22,000
77	200,000	22,222

VICINITY LOT

ALL LOTS TO BE ADJACENT TO THE EXCHANGE DRIVE

**LEGEND**

- EXISTING FENCE
- PROPOSED FENCE
- PROPOSED CONSTRUCTION
- PROPOSED DRIVE
- PROPOSED SIDEWALK
- PROPOSED UTILITY
- PROPOSED DRIVEWAY
- PROPOSED DRIVEWAY
- PROPOSED DRIVEWAY
- PROPOSED DRIVEWAY
- PROPOSED DRIVEWAY
- PROPOSED DRIVEWAY







**DRAFT**

**Exhibit B  
Repair Cost Estimate**



**CRYSTAL LAKE - PINGREE TRAIN STATION**  
**Master Summary Cost**

**DRAFT**

<b>PONDING ALONG EAST CURB LINE OF LOT A-2</b>		
NS	SAWCUT PAVEMENT (18' from curb)	500.00
NS	REMOVE PAVEMENT	10,000.00
NS	SUBBASE GRANULAR MATERIAL Type B CA-6 100% CRUSHED (4")	5,375.00
NS	BITUMINOUS BASE COURSE, 6"	36,000.00
NS	BITUMINOUS CONCRETE BINDER COURSE IL 19 N30 (1.5")	5,850.00
NS	BITUMINOUS CONCRETE SURFACE COURSE MIX "C" N30 (1.5")	4,950.00
NS	PERFORATED PVC PIPE (6")	6,500.00
<b>PONDING AROUND ISLANDS IN LOTS A-2 AND C</b>		
NS	SAWCUT	608.00
NS	REMOVE PAVEMENT, 3" FOR 3'	2,020.00
NS	LEVELING BINDER	990.00
NS	BITUMINOUS CONCRETE SURFACE COURSE MIX "C" N30 (1.5")	990.00
NS	REMOVE AND REPLACE CURB AND GUTTER WITH REVERSE CURB AND GUTTER	12,160.00
NS	BACKFILL ISLANDS WITH TOPSOIL (3" OVER 2')	126.00
NS	EROSION CONTROL BLANKET AND SEED	4,864.00
<b>SMALL TO INTERMEDIATE CRACKING IN LOT A-2</b>		
NS	CRACK SEAL	18,000.00
<b>LARGE CRACKING IN LOT C</b>		
NS	REMOVE SURFACE COURSE	124,440.00
NS	BITUMINOUS CONCRETE SURFACE COURSE MIX "C" N30 (1.5")	61,600.00
<b>SEDIMENT WASHOUT IN LANDSCAPE AREA</b>		
NS	BACKFILL WITH TOPSOIL (ASSUME 6" DEPTH IN 2280 SF)	5,500.00
NS	EROSION CONTROL BLANKET AND SEED	2,024.00
<b>PONDING ON SIDEWALK</b>		
NS	BACKFILL WITH 6" TOPSOIL (ASSUME 12' (6' EITHER SIDE) ALONG 270 LF)	2,600.00
NS	PCC SIDEWALK REMOVE AND REPLACE	14,850.00
NS	EROSION CONTROL BLANKET AND SEED	2,880.00
NS	REPLACE CLOSED GRATE WITH OPEN GRATE	100.00
<b>SETTLEMENT BEHIND BACKS OF CURB (ENTIRE SITE)</b>		
NS	BACKFILL WITH TOPSOIL (ASSUME 0-6" WEDGE BEHIND 6000 LF OF CURB AND GUTTER 6' WIDE)	14,000.00
NS	EROSION CONTROL BLANKET AND SEED	40,000.00
<b>MORTAR FRAMES AND RINGS IN STORM STRUCTURES</b>		
NS	MORTAR FRAMES AND RINGS	13,034.00

**DRAFT****CRYSTAL LAKE - PINGREE TRAIN STATION****Master Summary Cost**

<b>DETENTION BASIN A</b>		
NS	REPLACE MISSING FENCING (ASSUME FIVE 10' SECTIONS)	2,500.00
NS	REMOVE AND REPLACE FENCING TO ACCESS FOR CLEARING AND GRUBBING	3,000.00
NS	CLEARING AND GRUBBING	15,600.00
NS	EROSION CONTROL BLANKET AND SEED	50,400.00
<b>DETENTION BASIN C</b>		
NS	REMOVE AND REPLACE FENCING FOR ACCESS	10,000.00
NS	CLEARING AND GRUBBING	21,600.00
NS	EROSION CONTROL BLANKET AND SEED	69,600.00
NS	REPLACE 12" FES 200 AND GRATE	1,000.00
NS	REPLACE 15" FES 205 AND GRATE	1,200.00
NS	RESTORE 15" FES 210 AND GRATE	1,200.00
NS	REPLACE FES 215 RIPRAP CL A3	520.00
<b>PAVEMENT MARKINGS</b>		
NS	PARKING LOT STRIPING - 4"	16,270.00
NS	PARKING LOT STRIPING - 6"	
NS	THERMO PAVEMENT MARKING LETTER - SYMBOL	1,700.00
NS	THERMO PAVEMENT MARKING LINE 12"	108.00
NS	THERMO PAVEMENT MARKING LINE 24"	500.00
NS	LANDSCAPING / TREE REMOVAL	50,000.00
<b>SUBTOTAL COST</b>		<b>\$ 635,159.00</b>
Civil Engineering Construction Documents / Design 12%		76,219.08
Audit		2,500.00
Title Company Fees - Waivers		2,000.00
<b>BIDDING</b>		
Pre-Bid Meetings		
Bid Opening, Review Contractor Bids, Bid Recommendations		4,800.00
Civil Engineering Construction Observation- Regular Site Visits for percentage of work completed (10%)		63,515.90
<b>SUBTOTAL COST</b>		<b>\$ 149,034.98</b>
<b>NON-STRUCTURAL REPAIRS TOTAL COST</b>		<b>\$ 784,193.98</b>
<b>CONTINGENCY (20%)</b>		<b>\$ 156,838.80</b>
<b>TOTAL COST (WITH CONTINGENCY)</b>		<b>\$ 941,032.78</b>



**Agenda Item No: 20**

**City Council  
Agenda Supplement**

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**Meeting Date:**

September 7, 2021

**Item:**

Board and Commission Appointment – Sustainability Committee

**Council Discretion:**

Motion to approve the appointment of Lisa McLaughlin to fill a vacancy on the Sustainability Committee for a term expiring September 30, 2023.

**Staff Contact:**

Haig Haleblian, Mayor

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**Background:**

On November 17, 2015, the City Council formed the Sustainability Committee (“Committee”), replacing the Ad Hoc Clean Air Counts Advisory Committee established in 2007. The purpose of the Committee is to assist the City in promoting environmental sustainability within the City. As part of the Committee Charter, the City Council approved a total of nine (9) members to fulfill the Committee for three (3) year terms. There is currently one (1) vacancy on the Committee.

The application for Lisa McLaughlin is attached. Per the Committee Charter, to be a member on the Committee one must live in Crystal Lake, own a business in Crystal Lake, or attend a school in Crystal Lake. The Sustainability Committee has recommended Ms. McLaughlin’s appointment.

Lisa McLaughlin is the Director of Membership Development for the Crystal Lake Chamber of Commerce. She is interested in preserving the environment and promoting eco-friendly practices in the community. Ms. McLaughlin also has experience with horticulture and community gardening.

Appointments to the Sustainability Committee are nominated and confirmed by the Mayor and City Council.

**Votes Required to Pass:**

Simple majority.