



#2021-226

**GW Properties Wendy's and Guzman Y Gomez –
Final Planned Unit Development
Project Review for Planning and Zoning Commission**

<u>Meeting Date:</u>	January 19, 2022
<u>Request:</u>	Final Planned Unit Development for two new limited service restaurants.
<u>Location:</u>	5459 and 5501 Northwest Highway
<u>Acreage:</u>	Approximately 2.06 acres
<u>Existing Zoning:</u>	B-2 PUD General Commercial Planned Unit Development
<u>Surrounding Properties:</u>	North: B-2 PUD General Commercial South: M Manufacturing (Three Oaks Recreation Area) East: B-2 PUD General Commercial West: B-2 PUD General Commercial
<u>Staff Contact:</u>	Elizabeth Maxwell (815.356.3615)

Background:

- The site was the buildings and parking lot for the previous office building, then a used car lot for Pauley Toyota. The previous buildings have been demolished and the site is now vacant.
- The site sits along Route 14 and backs to the Three Oaks Recreation Area. It is part of the Three Oaks Recreation Area Planned Unit Development Overlay District. It is the intent of the Three Oaks PUD Overlay District to promote orderly and high-quality development that is compatible with the character and use of Three Oaks, consistent with and complementary of the area's recreational uses and amenities, and supportive of a strong and stable tax base.
- The Raising Canes was approved in 2020 and is currently in operation.
- This project received Preliminary PUD approval in June of 2021.
- The Final PUD is in substantial conformance with the approved Preliminary PUD.

Land Use Analysis:

ZONING

- The site is currently zoned B-2 PUD General Commercial. Limited Service Restaurant is a permitted use in the B-2 zoning district.

- The Comprehensive Land Use Plan designates this area as Commerce. The proposed restaurant uses fit in the Commerce land use designation.

SITE PLAN

- The site is along Northwest Highway east of the Three Oaks Recreation Area entrance. These two lots are between the existing Cassidy Tire and the Raising Cane's lot.
- The site backs to the north lake of the Three Oaks Recreation Area. A future boardwalk is planned behind these lots, which has been illustrated on these plans. Raising Canes has provided a 20-foot area reserved for the future boardwalk. To plan for the future amenity Wendy's is providing 20 feet and Guzman Y Gomez is providing 57 feet for easements.
- There are two access points into the site from Route 14, one on the Raising Cane's parcel and a shared access between the Wendy's parcel and Cassidy Tire. There is a shared cross access easement among all three properties.
- The traffic impact study suggests some minor changes and the addition of striping and signage to improve the site.

TRAFFIC STUDY

- A traffic study was completed by Gewalt Hamilton on behalf of Raising Cane's and contemplated another restaurant at this location.
- This traffic study has been updated and presented for review as part of the Final Planned Unit Development approval.

ELEVATIONS

- Wendy's elevations have been revised to include the knee wall along the side elevations under the window bays and to install a canopy over the side elevation entrance.
- Guzman Y Gomez elevations have been revised to:
 - Install a yellow canopy over the entrance doors,
 - Break up the charcoal brick on the West and South elevations with some additional Alamo colored brick,
 - Increased the width of the framing around the doors,
 - Added horizontal framing members to the windows, and
 - Installed soldier course brick of two rows along the tower element and one row under the remainder of the top cap giving the top a much thicker and finished look.

LANDSCAPE PLAN

- The landscape plan meets all the requirements.

SIGNAGE

- Both uses have wall signage that blends well with the architecture and the size and scale of the building.
- Guzman Y Gomez received a sign variation for wall signs and the mural up to 416.87 sf.

Findings of fact:

Final Planned Unit Development

The petitioner is requesting approval of a Final Planned Unit Development to allow the construction of two Limited Service Restaurant for a Wendy's and a Guzman Y Gomez. The Final PUD must be reviewed against the approved preliminary PUD conditions. The Final PUD is in substantial compliance with the approved preliminary PUD and the petitioners have provided revised plans to address the conditions of approval.

Comprehensive Land Use Plan 2030 Summary Review:

The Comprehensive Plan designates the subject property as Commercial, which allows for existing and future commercial and business uses. The following goal is applicable to this request:

Land Use – Commercial

Goal: Maintain a dynamic and sustainable base of commercial uses that provides a solid tax base, goods, services and jobs to the city as well as the surrounding region through coordination in the Unified Development Ordinance, Comprehensive Land Use Plan and Economic Development Strategic Plan.

This can be accomplished with the following supporting actions:

Supporting Action: Promote, retain and attract businesses that provide a diverse tax base.

Success Indicator: The number of new "chain store" occupancies.

Recommended Conditions:

If a motion to recommend approval of the petitioner's request is made it should be with the following conditions:

1. Approved plans, reflecting staff and advisory board recommendations, as approved by the City Council:
 - A. Application (GW Properties, received 01/13/22)
 - B. Site Plan (Design Studio 24, dated 07/28/21, received 12/21/21)
 - C. Landscape Plan (Kimley Horn, dated 05/18/21, received 12/21/21)
 - D. Wendy's Elevations (Wallin Gomez Architects, dated 06/21/21, received 12/21/21)
 - E. Wendy's Monument Sign (Custom Sign Center, dated 04/27/21, received 12/21/21)
 - F. Guzman Y Gomez Elevations (Interplan, dated 06/07/21, received 12/21/21)

 - G. Traffic Impact Study (Gewalt Hamilton, updated 6/30/21, received 12/21/21)
2. Conditions from Ordinance 7721 shall remain applicable unless modified by this request.
3. Elevations
 - A. Wendy's to install one or two rows of soldier course brick approximately 1/3 down from the top of the parapet on the rear portion where the building increases in height to help break up the expanse of brick.

- B. The Guzman Y Gomez brick color shall be Yankee Hill Matte Charcoal.
 - C. On the Guzman Y Gomez building the soldier course brick under the top cap should project at least 1 inch beyond the main portion of the elevation and the top cap should project at least 1 inch beyond this brick.
4. Traffic Study
- A. Add stop bars and signage as recommended in the Traffic Study to promote a safer pedestrian and cycling experience.
 - B. Improve the shared site access driveway, opposite Federal Drive:
 - i. Widen the driveway to the east to provide one inbound and two outbound lanes.
 - ii. Stripe the existing lanes as they should be marked as a separate left-turn lane and a shared through/right-turn lane.
 - iii. Install a stop sign at this driveway.
 - C. The entrance to the Wendy's site should provide a raised "truck apron" to give the appearance of a narrower entrance drive to help limit speed of inbound vehicles while still accommodating the larger delivery vehicles needed.
 - D. Install appropriate way-finding signage on site to direct traffic to various drive-throughs.
5. Wendy's and Guzman Y Gomez shall participate with the City during the construction of the boardwalk and related amenities like benches, gazebos, etc. along their property.
6. The Wendy's monument sign brick shall match the brick of the building.
7. The petitioner shall address all of the review comments and requirements of the Community Development, Fire Rescue, and Public Works Departments, as well as the City's Stormwater Consultant and the final approved Traffic Study.

PIQ Map





**City of Crystal Lake
Development Application
Ownership Sign-off Acknowledgement Form**

The following information is related to a development application. As the owner of the property in question, I (we) acknowledge that the information provided in the submittal was reviewed and approved.

Owner Information

Name: _____

Address: _____

Phone: _____

E-mail: _____

Project Name & Description: _____

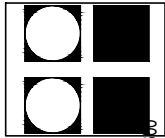
Project Address/Location: _____

Signature

 Mitch Goltz 1/13/2022

Owner: Print and Sign name Date

NOTE: If the property is held in a trust, the trust officer must sign this petition as owner. In addition, the trust officer must provide a letter that names all beneficiaries of the trust.



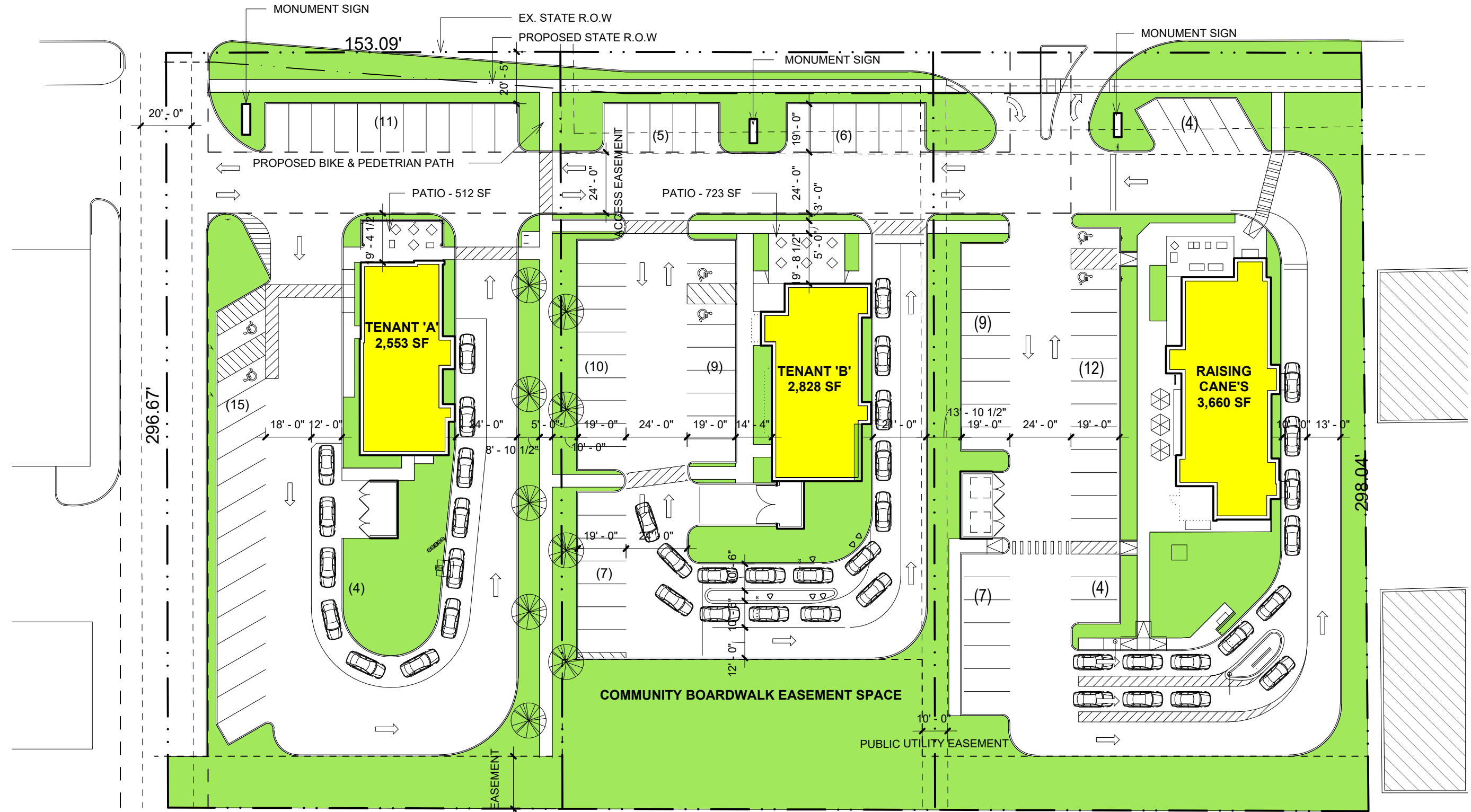
DESIGN STUDIO 24, LLC.
ARCHITECTS - DESIGNERS - PLANNERS
 2211 N. Elston Ave. Suite 304, Chicago, Illinois 60614 Fax: 847.886.7751 Tel.: 847.886.8966

GW PROPERTIES
PROPOSED SITE STUDY
 5417-5501 NORTHWEST HWY
 CRYSTAL LAKE, ILLINOIS

DATE: 07.28.2021
 MARK T. DIGANCI
 REG. ARCHITECT
 YK

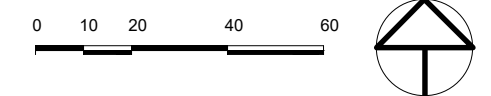
ST.1

NORTHWEST (U.S. ROUTE 14) HIGHWAY

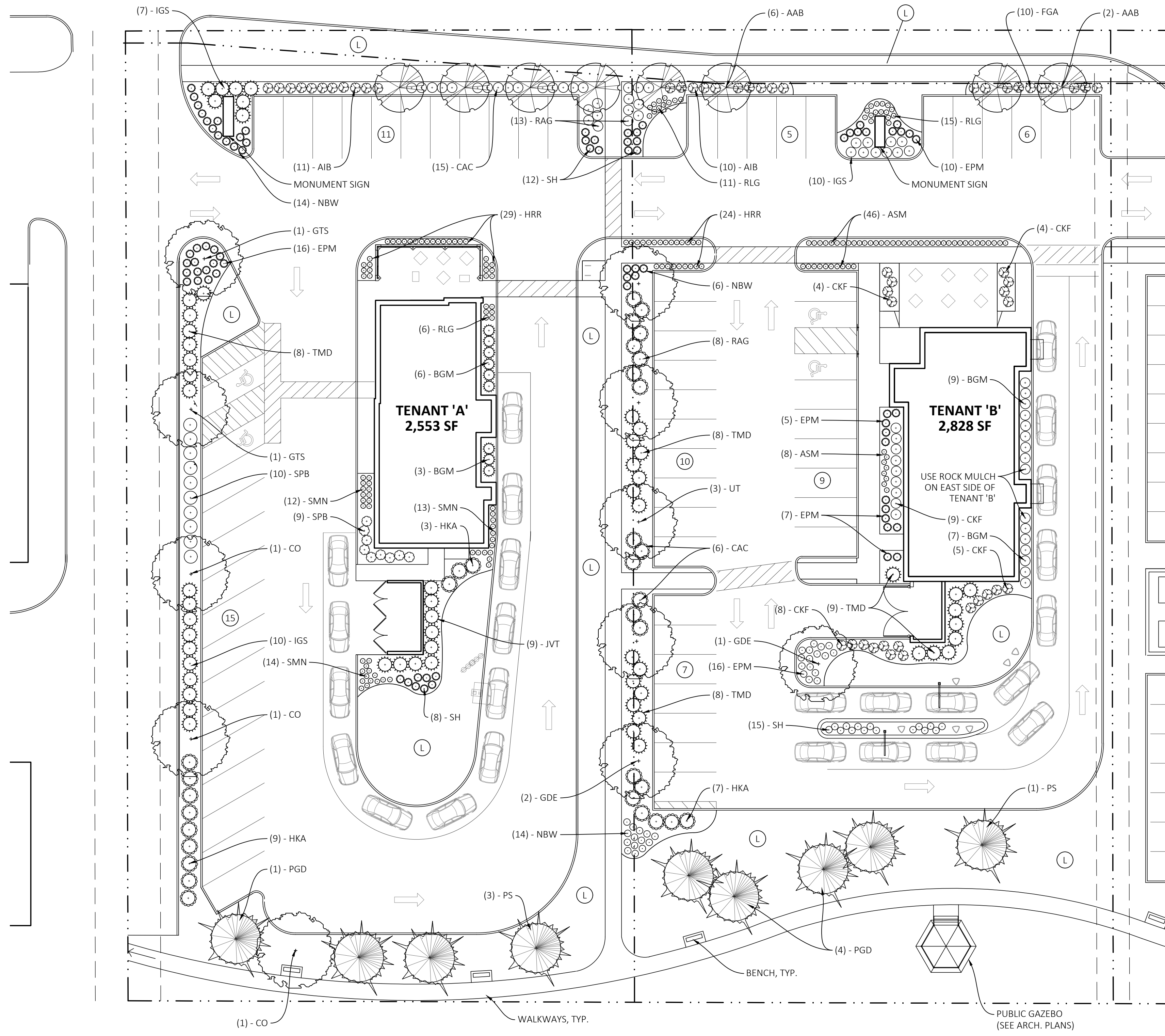


1 SITE PLAN - OVERALL
 1" = 40'-0"

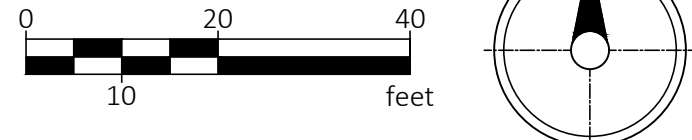
PARKING SPACE:
 WENDY'S - 26 SPACES (2 ADA STALLS)
 GYG - 41 SPACES (2 ADA STALLS)
 RAISING CANES - 36 SPACES (2 ADA STALLS)



NORTHWEST (U.S. ROUTE 14) HIGHWAY



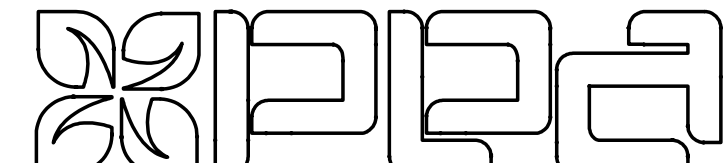
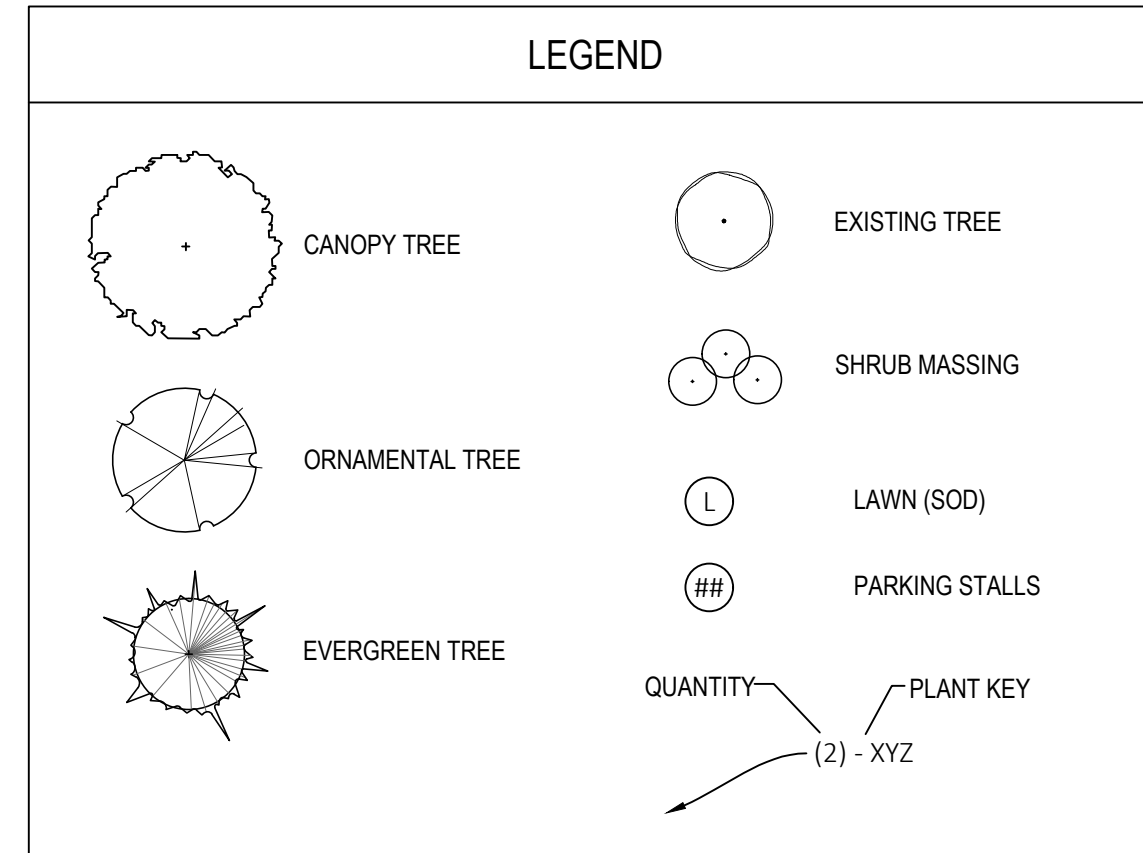
LANDSCAPE PLAN
SCALE: 1" = 20'-0"



LANDSCAPE ORDINANCE REVIEW: CRYSTAL LAKE, ILLINOIS

SPECIFIC ORDINANCE	CODE REQUIRES	CALCULATION	COMPLIANCE
SEC. 4-400.F.1.a.i	ONE (8) EIGHT-FOOT BY (19) NINETEEN-FOOT LANDSCAPE ISLAND, AS MEASURED BETWEEN BACK OF CURBS, CONTAINING ONE CANOPY OR ORNAMENTAL TREE AND FIVE SHRUBS, EVERY 10 PARKING SPACES AND AT THE ENDS OF EVERY PARKING ROW.	TENANT 'A': 26 PARKING SPACES TOTAL / 10 SPACES = 3 CANOPY TREES & 25 SHRUBS TENANT 'B': 37 PARKING SPACES TOTAL / 10 SPACES = 4 CANOPY TREES & 20 SHRUBS	5 TREES & 40 SHRUBS PROVIDED
SEC. 4-400.F.1.b.i	CONTINUOUS VISUAL SCREEN 15 FEET IN WIDTH USING BOTH DECIDUOUS AND EVERGREEN SPECIES PLANTED IN UNDLATING BEDS FORMING AN INTERESTING PATTERN REQUIRING ONE CANOPY, ORNAMENTAL OR EVERGREEN TREE AND 10 SHRUBS, WITH SHRUBS PLANTED AT VARIOUS HEIGHTS, AVERAGING THREE FEET IN HEIGHT, ALONG EVERY 40 LINEAL FEET.	NORTHWEST HWY (300 LF) / 40 LF = 7.5 8 TREES & 75 SHRUBS REQUIRED	8 ORNAMENTAL TREES (OVERHEAD WIRES PRESENT) & 70 SHRUBS PROVIDED. DRIVEWAYS AND SIGHT TRIANGLES PREVENT ADDITIONAL SHRUBS.
SEC. 4-400.F.4.a	FIVE-FOOT-WIDE LANDSCAPE AREA AROUND ALL PERIMETERS OF BUILDINGS CONTAINING BOTH DECIDUOUS AND EVERGREEN SPECIES OF TREE AND SHRUBS PROVIDING AN INTERESTING VISUAL PATTERN AROUND THE BUILDING.	N/A	PROVIDED
SEC. 4-400.F.6.a	THE AMOUNT OF LANDSCAPE AREA REQUIRED SHALL BE ONE SQUARE FOOT OF LANDSCAPE AREA PER ONE SQUARE FOOT OF SIGN AREA. THE LANDSCAPING SHALL BE LOCATED IN AN AREA RADIATING FROM THE BASE OF THE SIGN.	SIGN AREA = 260 SF / 10 SF = 26 SF LANDSCAPE AREA (13 SF TREES OR SHRUBS)	PROVIDED

PLANTING SCHEDULE						
KEY	QTY.	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	COMMENTS
DECIDUOUS TREES						
CO	3	CELTIS OCCIDENTALIS	COMMON HACKBERRY	2.5" CAL.		B&B SPECIMEN; 7'-0" MIN. BRANCHING HEIGHT
GDE	3	GYMNOCALADUS DIOCUS 'ESPRESSO'	ESPRESSO KENTUCKY COFFEETREE	2.5" CAL.		B&B SPECIMEN; 7'-0" MIN. BRANCHING HEIGHT
GTS	2	GLEDITSIA TRIACANTHOS 'SKYLINE'	SKYLINE HONEYLOCUST	2.5" CAL.		B&B SPECIMEN; 7'-0" MIN. BRANCHING HEIGHT
UT	3	ULMUS X TRIUMPH	TRIUMPH ELM	2.5" CAL.		B&B SPECIMEN; 7'-0" MIN. BRANCHING HEIGHT
EVERGREEN TREES						
JVT	9	JUNIPERUS VIRGINIANA 'TAYLOR'	TAYLOR EASTERN REDCEDAR	6'-8" HT.	4' O.C.	B&B SPECIMEN
PGD	5	PICEA GLAUCA 'DENSATA'	BLACK HILLS SPRUCE	6'-8" HT.	15' O.C.	B&B SPECIMEN
PS	4	PINUS STROBUS	EASTERN WHITE PINE	6'-8" HT.	30' O.C.	B&B SPECIMEN
ORNAMENTAL TREES						
AAB	8	AMELANCHIER 'AUTUMN BRILLIANCE'	AUTUMN BRILLIANCE SERVICEBERRY	8" HT.	15" O.C.	MULTI-STEM SPECIMEN
DECIDUOUS SHRUBS						
AIB	21	ARONIA MELANOCARPA 'MORTON'	IROQUOIS BEAUTY CHOKEBERRY	#3 CONT.	36" O.C.	INSTALL 30" HT., MINIMUM
CAC	21	CLETHRA ALNIFOLIA 'CALEB'	VANILLA SPICE SUMMERSWEET	#3 CONT.	48" O.C.	INSTALL 30" HT., MINIMUM
FGA	10	FOTHERGILLA GARDENII	DWARF FOTHERGILLA	#3 CONT.	36" O.C.	INSTALL 30" HT., MINIMUM
HKA	19	HYPERICUM KALMIANUM	KALM'S ST. JOHNSWORT	#3 CONT.	36" O.C.	INSTALL 30" HT., MINIMUM
RAG	21	RHUS AROMATICA 'GRO LOW'	GRO-LOW SUMAC	#3 CONT.	48" O.C.	
SPB	19	SYRINGA PENDA 'BLOOMERANG'	BLOOMERANG DWARF LILAC	#3 CONT.	36" O.C.	INSTALL 30" HT., MINIMUM
EVERGREEN SHRUBS						
BGM	25	BUXUS 'GREEN MOUNTAIN'	GREEN MOUNTAIN BOXWOOD	#3 CONT.	36" O.C.	
IGS	27	ILEX GLABRA 'SHAMROCK'	SHAMROCK INKBERRY	#3 CONT.	36" O.C.	INSTALL 30" HT., MINIMUM
TMD	33	TAXUS x MEDIA 'DENSIFORMIS'	DENSE YEW	#3 CONT.	48" O.C.	
PERENNIALS, GRASSES & GROUNDCOVERS						
ASM	54	ALLIUM 'MILLENIUM'	MILLENIUM ALLIUM	#1 CONT.	18" O.C.	
CKF	30	CALAMOGROSTIS X 'KARL FOERSTER'	KARL FOERSTER FEATHER REED GRASS	#3 CONT.	36" O.C.	
EPM	54	ECHINACEA 'CBG CONE2'	PIXIE MEADOWBRITE CONEFLOWER	#1 CONT.	24" O.C.	
HRR	53	HEMEROCALLIS 'ROSY RETURNS'	ROSY RETURNS DAYLILY	#1 CONT.	18" O.C.	
NBW	34	NEPETA 'BLUE WONDER'	BLUE WONDER CATMINT	#1 CONT.	24" O.C.	
RLG	32	RUDBECKIA 'LITTLE GOLDSTAR'	LITTLE GOLDSTAR BLACK-EYED SUSAN	#1 CONT.	18" O.C.	
SMN	37	SALVIA 'MAY NIGHT'	MAY NIGHT SALVIA	#1 CONT.	18" O.C.	
SH	35	SPOROBOLUS HETEROLEPIS	PRAIRIE DROPSSEED	#1 CONT.	24" O.C.	

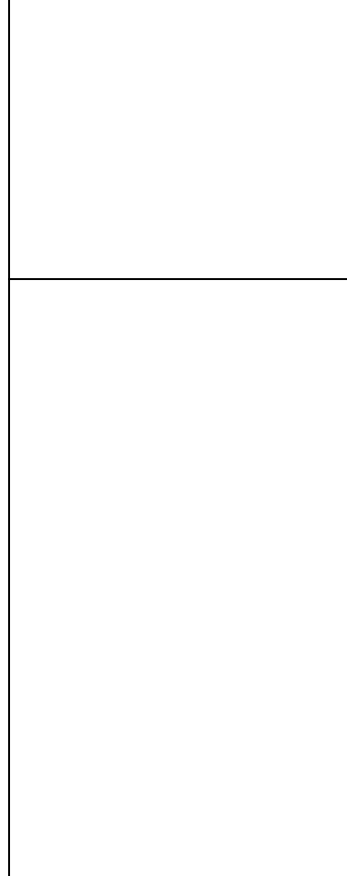


PLANNED ENVIRONMENT ASSOCIATES
P.O. BOX 2258
CHESTERTON, IN 46304
(219) 299-3383
www.pelanv.com

NO.	REVISIONS	DATE	BY

Kimley-Horn
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Lisle, IL 60532
WWW.KIMLEY-HORN.COM

SCALE: AS NOTED
DESIGNED BY: JRR
DRAWN BY: JRR
CHECKED BY: KMR



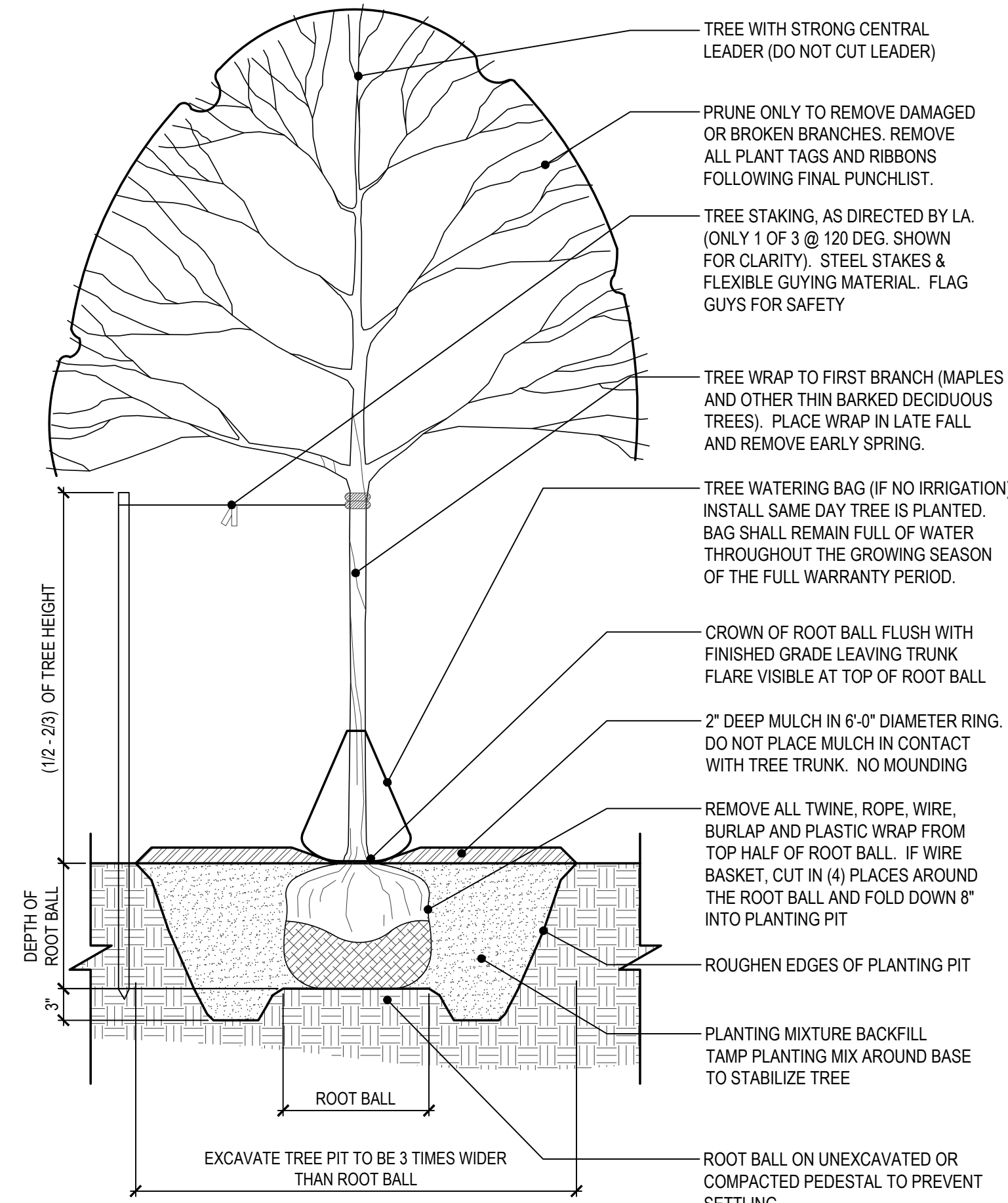
LANDSCAPE PLAN

GW CRYSTAL LAKE
5417 NORTHWEST HWY
CRYSTAL LAKE, IL 60014

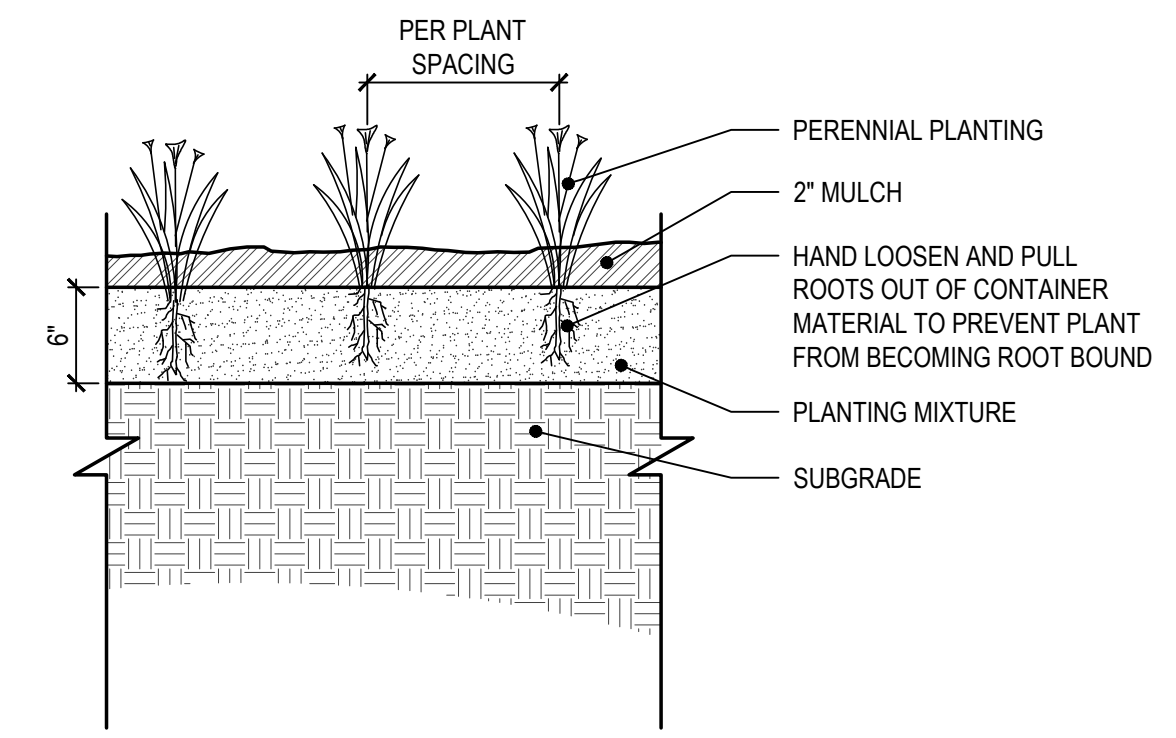
ORIGINAL ISSUE:
5/18/2021
KHA PROJECT NO.
168726001
SHEET NUMBER
L101

LANDSCAPE NOTES:

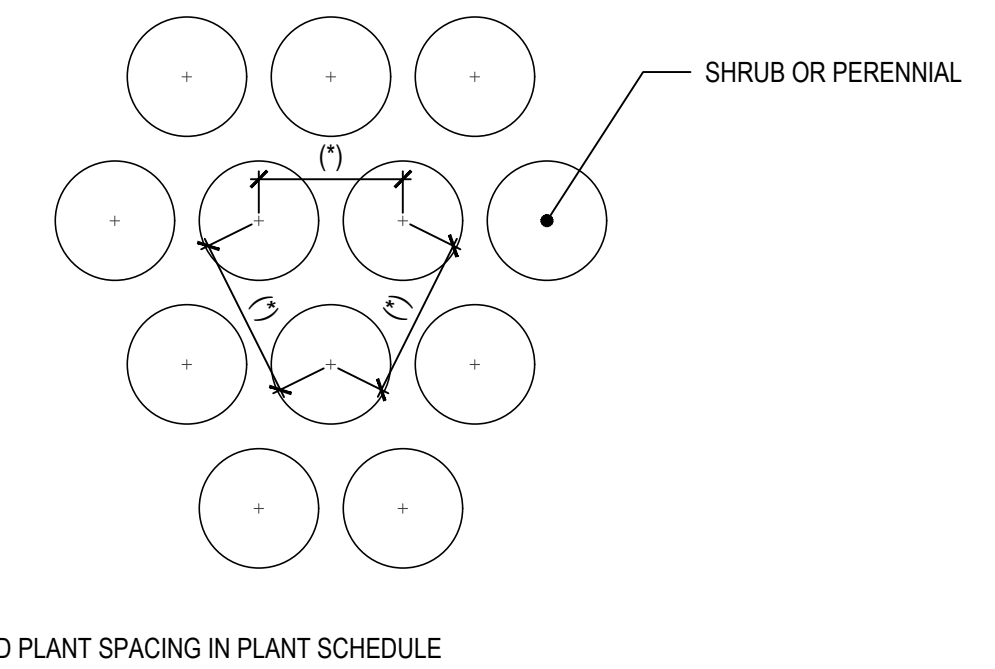
- SEE SHEET L101 FOR LANDSCAPE ORDINANCE REVIEW DATA TABLE & PLANTING SCHEDULE.
- THE LANDSCAPE CONTRACTOR SHALL OBTAIN AND PAY FOR ALL PERMITS AND FEES THAT MAY BE REQUIRED FOR HIS PORTION OF WORK.
- IN CASE OF DISCREPANCIES BETWEEN THE PLAN AND THE PLANT LIST, THE GRAPHIC SYMBOLS SHOWN ON THE PLAN SHALL DICTATE.
- NO SUBSTITUTIONS OF PLANT MATERIALS WILL BE ALLOWED. IF PLANTS ARE NOT AVAILABLE, THE CONTRACTOR SHALL NOTIFY LANDSCAPE ARCHITECT PRIOR TO BID IN WRITING. ALL PLANTS SHALL BE INSPECTED AND TAGGED WITH PROJECT I.D. AT NURSERY OR CONTRACTORS OPERATIONS PRIOR TO MOVING TO JOB SITE. PLANTS MAY BE INSPECTED AND APPROVED OR REJECTED ON THE JOB SITE BY LANDSCAPE ARCHITECT.
- LANDSCAPE CONTRACTOR SHALL NOTIFY LANDSCAPE ARCHITECT IN WRITING PRIOR TO BID DATE OF ANY PLANTS HE/SHE FEELS MAY NOT SURVIVE IN LOCATIONS NOTED ON PLANS.
- ALL PLANTS SHALL MEET OR EXCEED AMERICAN STANDARDS FOR NURSERY STOCK, 1986 EDITION, AS SET FORTH BY AMERICAN ASSOCIATION OF NURSERYMEN.
- ALL PLANT MATERIAL AND SEED SHALL BE PROVIDED FROM A NURSERY (WITHIN 200 MILES) WITH A SIMILAR PLANT HARDINESS ZONE AS PROJECT LOCATION
- PLANTS AND ALL OTHER MATERIALS TO BE STORED ON SITE WILL BE PLACED WHERE THEY WILL NOT CONFLICT W/ CONSTRUCTION OPERATIONS AND AS DIRECTED BY LANDSCAPE ARCHITECT.
- FINAL PLACEMENT OF PLANT MATERIALS, ETC., SHALL BE APPROVED BY LANDSCAPE ARCHITECT BEFORE PLANTING OPERATIONS ARE TO PROCEED. ALL TREE LOCATIONS SHALL BE MARKED WITH A WOOD STAKE INDICATING VARIETY AND SIZE OF TREE. ALL GROUND COVER AND PLANTING BED LINES SHALL BE MARKED W/ HIGHLY VISIBLE PAINT LINES W/ OCCASIONAL WOOD STAKES FOR REFERENCE. ALL STAKES SHALL BE REMOVED FOLLOWING PLANTING OPERATIONS. LANDSCAPE ARCHITECT RESERVES THE RIGHT TO ADJUST PLANT LOCATIONS ON SITE.
- IRRIGATION SYSTEM- PROVIDE AN ALTERNATIVE COST FOR A LANDSCAPE IRRIGATION SYSTEM. PROVIDE A BACKFLOW PREVENTER PER LOCAL CODE. PROVIDE A CONTROLLER. LAWN ZONES ARE TO BE COVERED BY SPRAY HEADS OR ROTARY HEADS. ALL SHRUBS BEDS ARE TO BE IRRIGATED WITH DRIP (NETAFIM).
- CONTRACTOR SHALL EXCAVATE A MINIMUM OF 2'-0" BELOW THE PAVING SURFACE.
- ALL SEED & SOD AREAS SHALL RECEIVE A MINIMUM OF 6" DEPTH OF TOPSOIL
- ALL PLANTING BEDS SHALL RECEIVE 6" DEPTH OF PREPARED SOIL.
- MULCH SHALL BE APPROVED BY LANDSCAPE ARCHITECT AND SHALL BE UNIFORM IN TEXTURE AND COLOR AND SHALL BE OBTAINED FROM SAWMILL OR LUMBERING OPERATION. NO UTILITY MULCH OR PROCESSED TREE TRIMMINGS WILL BE ALLOWED
 - ALL SHRUB PLANTING AREA SHALL BE COVERED WITH 3" LAYER OF SHREDDED HARDWOOD BARK MULCH.
 - ALL GROUND COVER BEDS SHALL BE COVERED WITH 2" LAYER OF SHREDDED HARDWOOD BARK MULCH.
- ALL LANDSCAPE PLANTINGS TO BE MAINTAINED BY CONTRACTOR FOR 60 DAYS MIN. FOLLOWING COMPLETE INSTALLATION AND FINAL INSPECTION BY LANDSCAPE ARCHITECT. MAINTENANCE SHALL INCLUDE WATERING, WEEDING, CULTIVATING, MULCHING, MOWING, AND ALL OTHER NECESSARY OPERATIONS REQUIRED FOR PROPER ESTABLISHMENT OF LAWNS AND PLANTINGS. CONTRACTOR TO SUBMIT UNIT PRICES ON EVERY TYPE OF WORK AS REQUIRED BY LANDSCAPE ARCHITECT.
- ALL LANDSCAPE PLANTINGS SHALL BE GUARANTEED FOR A PERIOD OF ONE YEAR FOLLOWING FINAL INSPECTION BY LANDSCAPE ARCHITECT. AT THE END OF THIS PERIOD, PLANT MATERIAL TERMED DEAD OR UNSATISFACTORY BY LANDSCAPE ARCHITECT SHALL BE REPLACED AT NO ADDITIONAL CHARGE BY THE LANDSCAPE CONTRACTOR. THE REPLACEMENTS SHALL ALSO BE GUARANTEED FOR 1 YEAR.
- AN APPROVED PRE-EMERGENT HERBICIDE SHALL BE APPLIED IN ALL PLANTING BEDS AT A RATE SPECIFIED BY MANUFACTURER FOR EACH PLANT VARIETY.



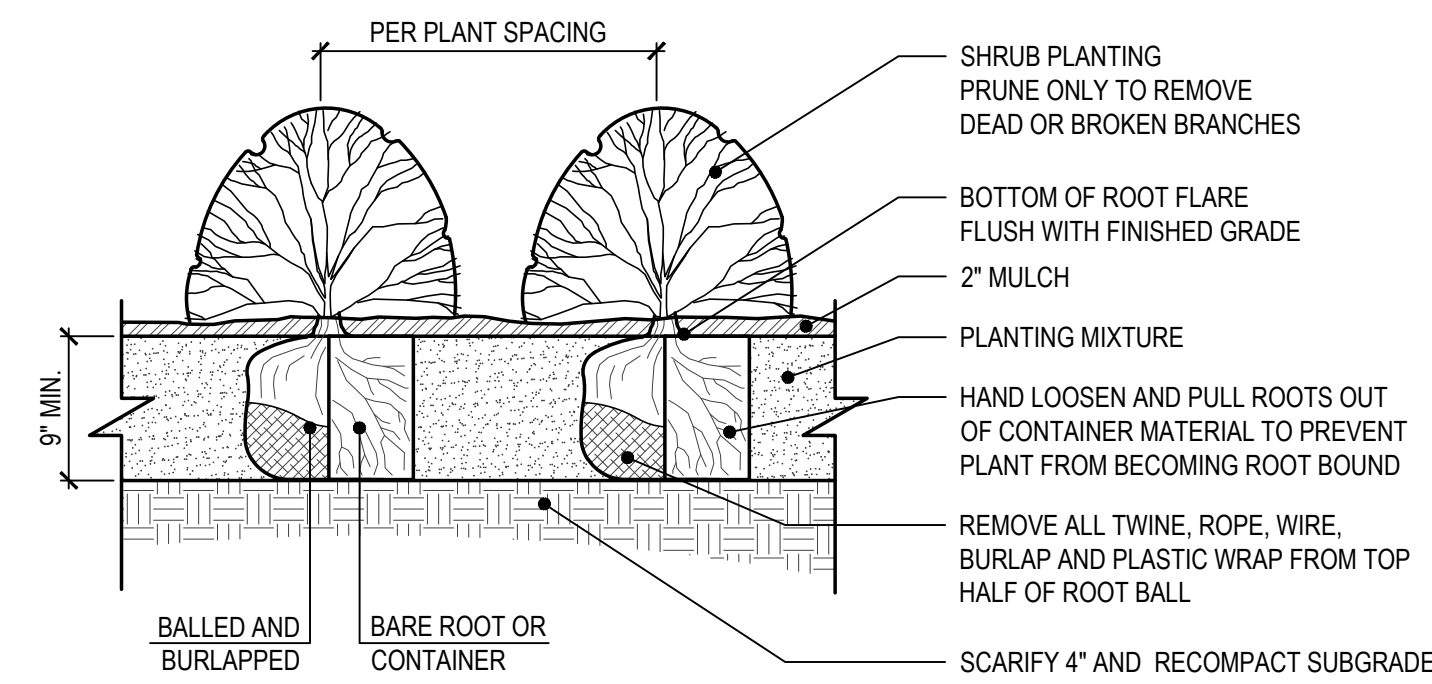
1 TREE PLANTING
SCALE: 1/2" = 1'-0"



3 PERENNIAL PLANTING
SCALE: 1" = 1'-0"



4 PLANT SPACING
NOT TO SCALE

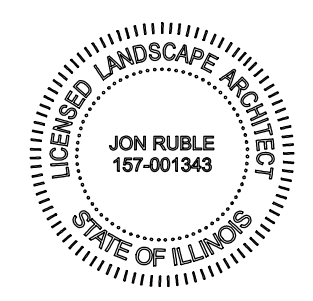


2 SHRUB PLANTING
SCALE: 1" = 1'-0"

NO.	REVISIONS	DATE	BY

Kimley-Horn
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SCALE: AS NOTED
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DRAWN BY: JRR
CHECKED BY: KMR



EXP: 8/31/2021

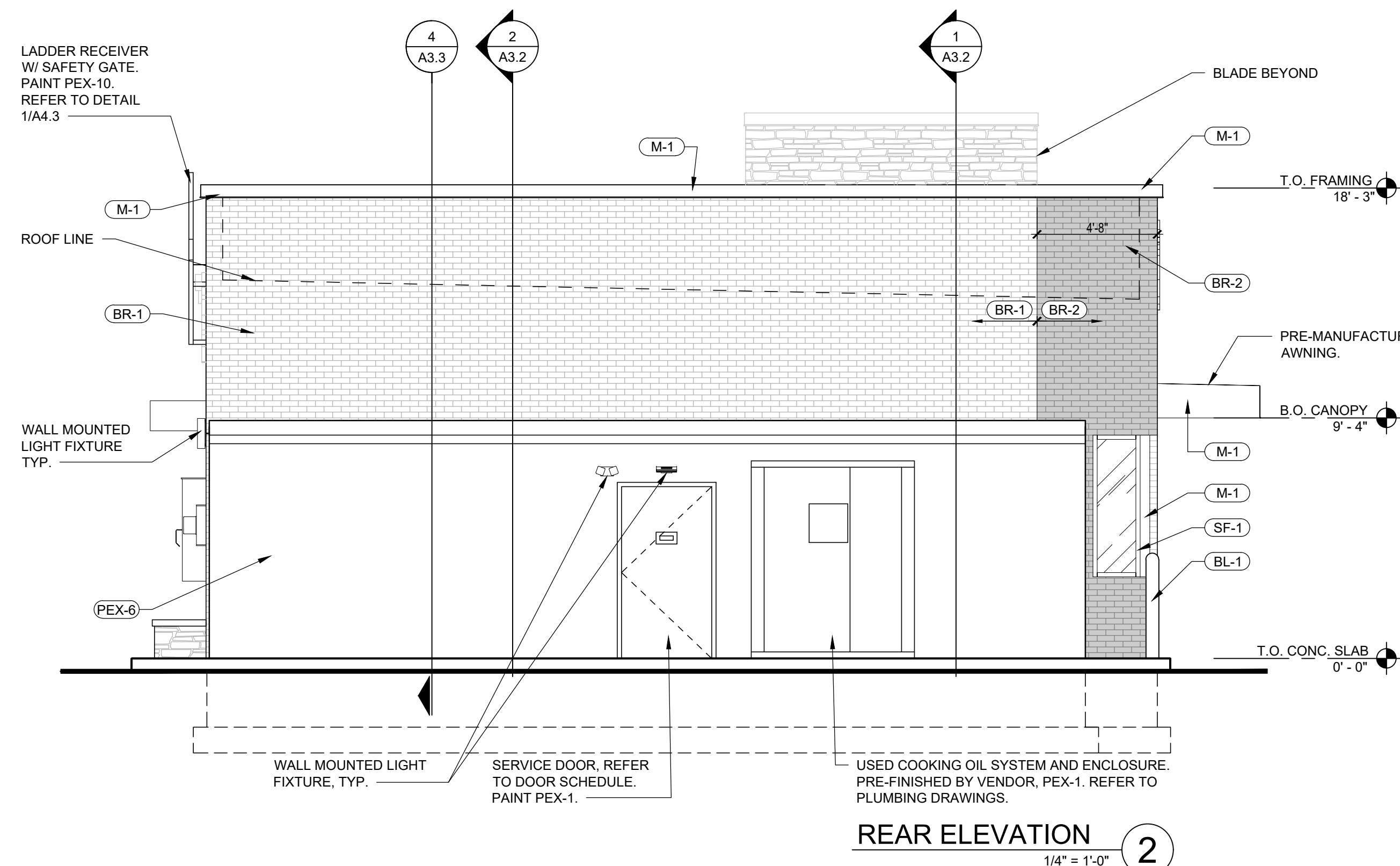
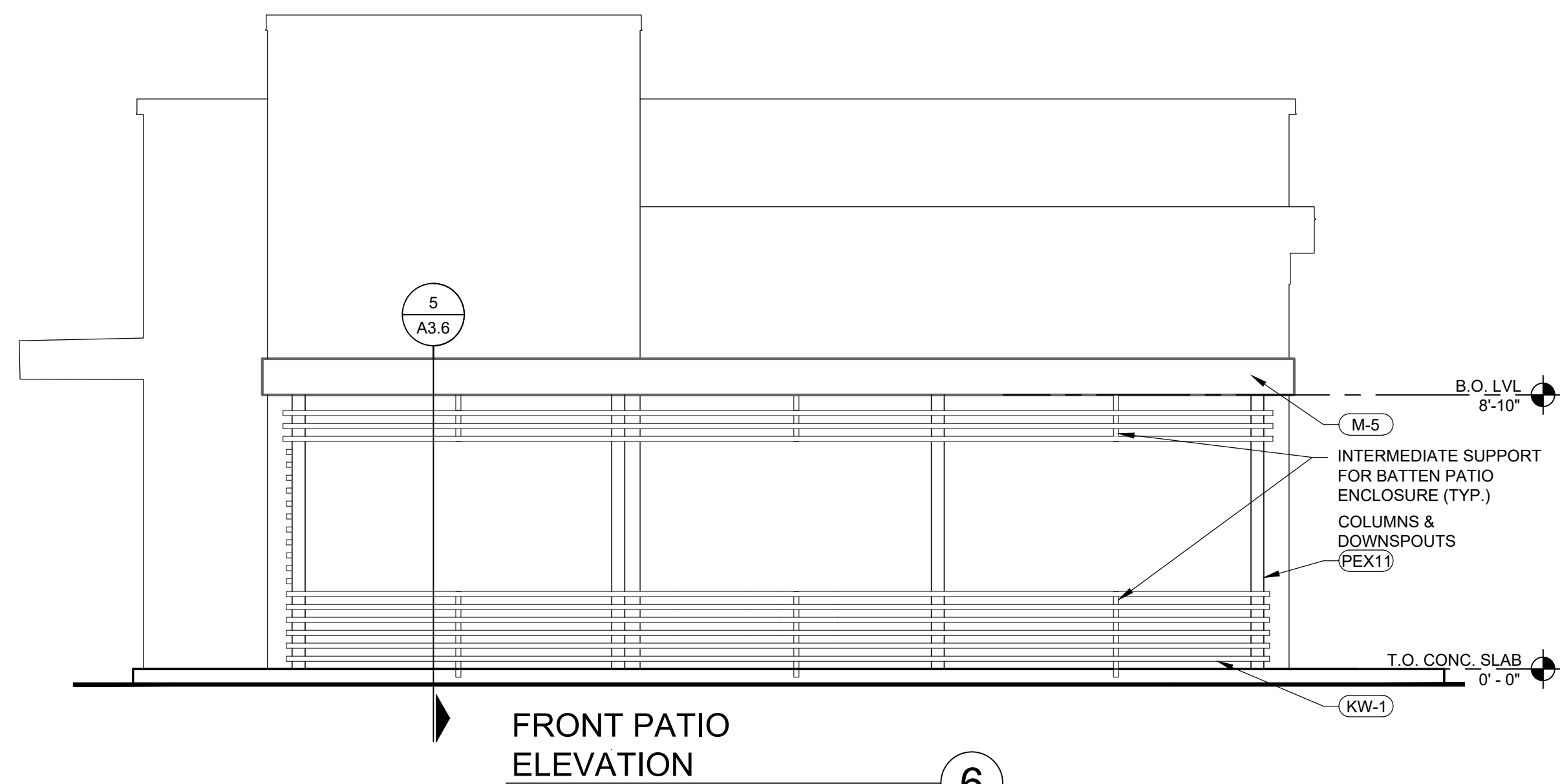
LANDSCAPE DETAILS

GW CRYSTAL LAKE
5417 NORTHWEST HWY
CRYSTAL LAKE, IL 60014

ORIGINAL ISSUE:
5/18/2021
KHA PROJECT NO.
168726001
SHEET NUMBER

L201



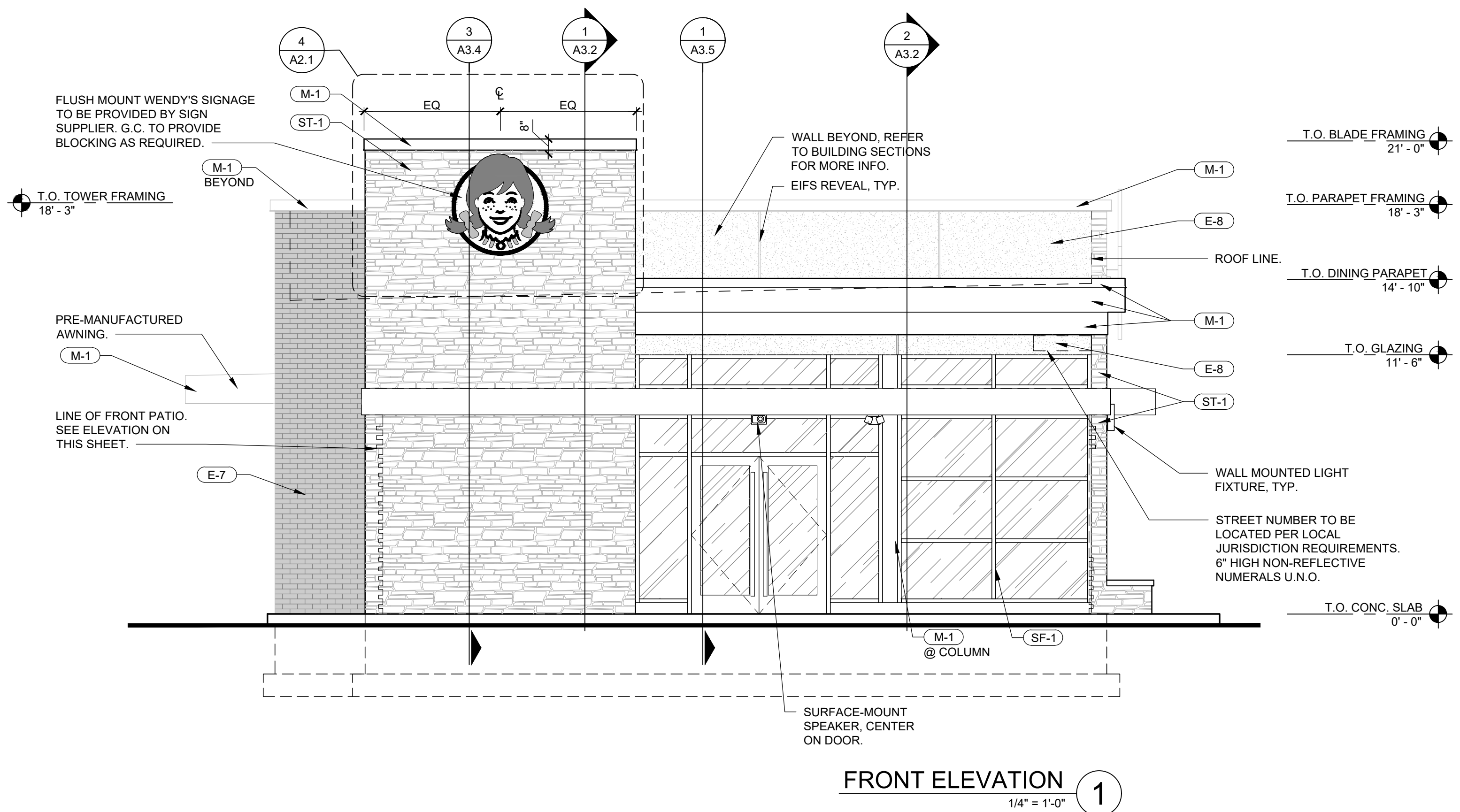
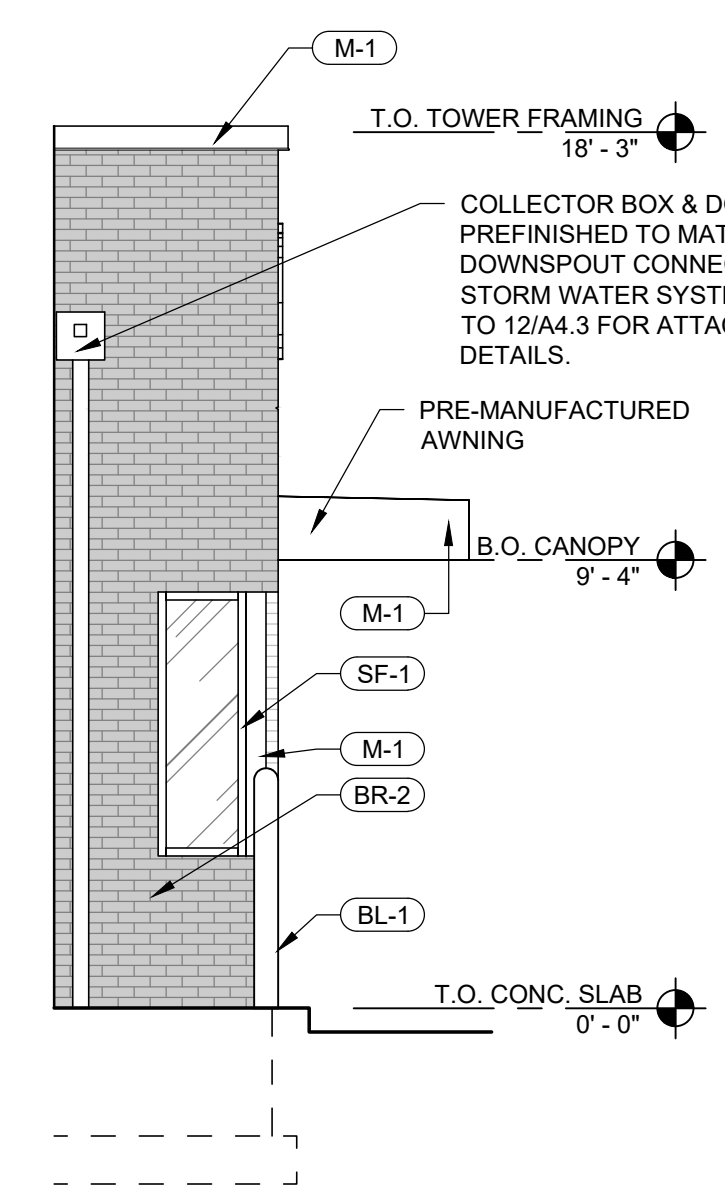
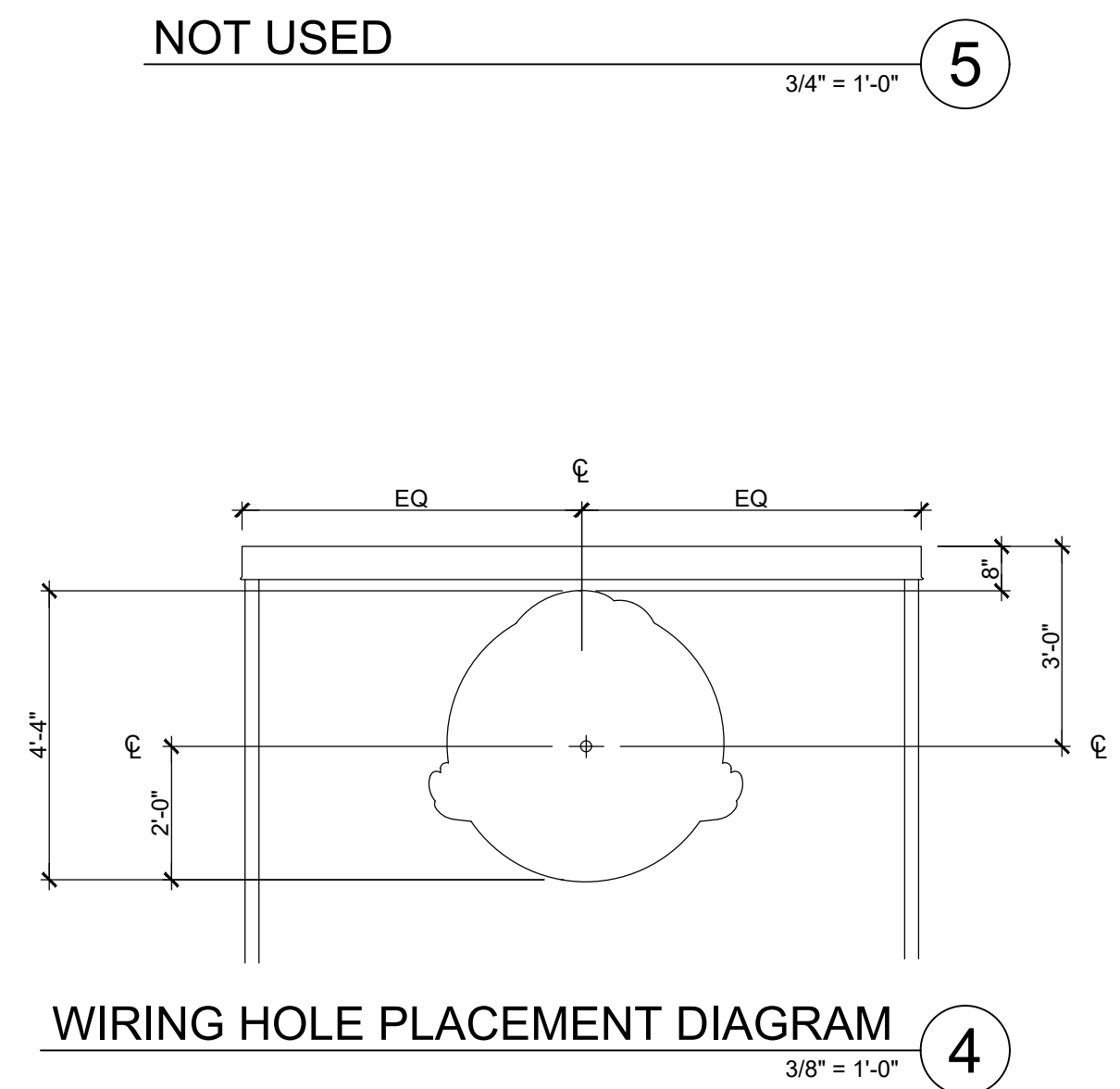


EXTERIOR FINISH SCHEDULE	
SF-1	STOREFRONT - "DARK BRONZE"
PJW	PICK-UP WINDOW TO MATCH STOREFRONT
M-1	PARAPET CAP/ BRAKE METAL - "DARK BRONZE"
KW-1	KNOTWOOD PREFINISHED ALUMINUM SIDING MATERIAL "LIGHT OAK"
BR-1	FACE BRICK
BR-2	FACE BRICK
ST-1	THIN STONE
E-8	EIFS - "TAN"
BL-1	BOLLARD SLEEVE - "YELLOW"
PEX-1	PAINT - DARK BRONZE (SEE PAINT SCHEDULE)
PEX-3	PRE-FINISHED DARK BRONZE SIGN CABINETS (SEE PAINT SCHEDULE)
PEX-6	PRE-FINISHED WALK-IN FREEZER / COOLER DARK BRONZE (SEE PAINT SCHEDULE)
PEX-10	PAINT / POWDER COAT - BLACK (SEE PAINT SCHEDULE)

SITE NUMBER:	13352
BASE MODEL:	(2.0) SMART 40+
ASSET TYPE:	FRAN./CORP.
CLASSIFICATION:	NEW BUILD
OWNER:	XXXX
BASE VERSION:	2020
UPGRADE CLASSIFICATION:	FRAN./CORP. NEW BUILD
PROJECT YEAR:	2020
FURNITURE PACKAGE:	UM BRITE
DRAWING RELEASE:	SUMMER 2020

Wallin - Gomez Architects, Ltd.
 711 South Dearborn Street
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 Chicago, IL 60605
 Tel: 312-427-4702
 Fax: 312-427-6611
 e-mail: admin@wgald.com

Wendy's.
 STORE #13352
 5505 NORTHWEST HIGHWAY
 CRYSTAL LAKE, IL 60014



REV.	DATE	DESCRIPTION

PLOT DATE: 6/21/2021
 PROJECT NUMBER: 20XXX
 DRAWN BY: CL
 CHECKED BY: DJW

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STATE OF ILLINOIS
 #001.010990
 REGISTERED ARCHITECT
 DONALD J. WALLIN
 EXPIRATION DATE: 11/30/2022

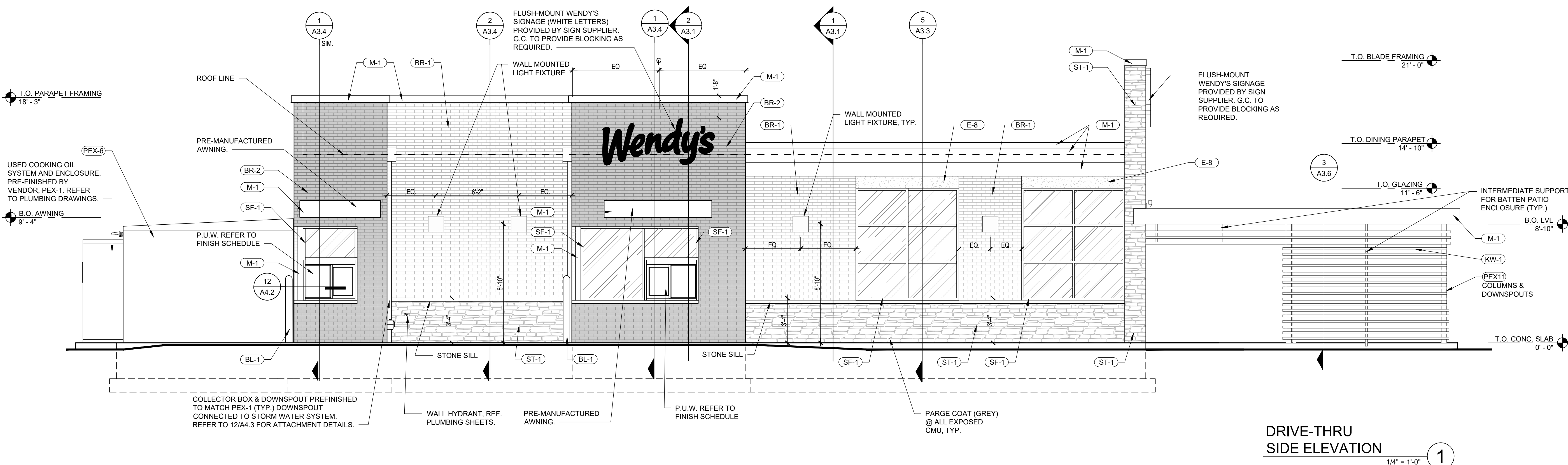
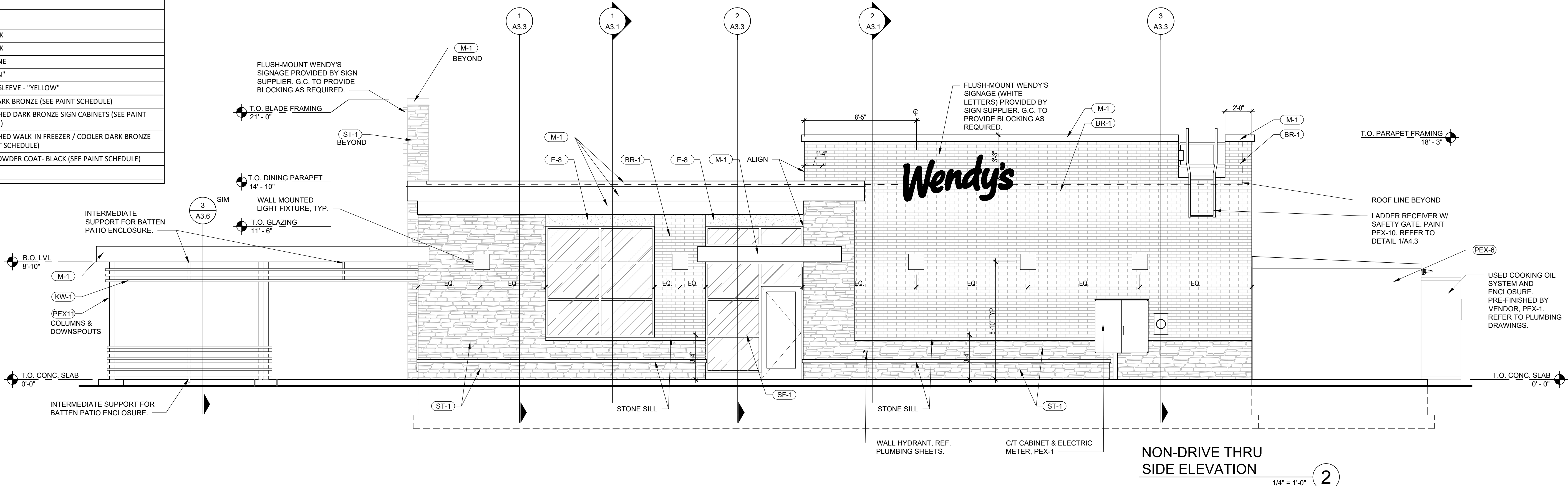
SHEET NAME
 EXTERIOR ELEVATIONS

SHEET NUMBER
A2.1

06/21/2021 5:58 PM, P:11, PENDING PROJECTS (ALL YEARS) Wendy's - Crystal Lake, IL Sheet S202105-11 Design/3D, SMART XXX-A2-1, Exterior Elevations.dwg, Lineman

EXTERIOR FINISH SCHEDULE

SF-1	STOREFRONT - "DARK BRONZE"
PUW	PICK-UP WINDOW TO MATCH STOREFRONT
M-1	PARAPET CAP/ BRAKE METAL - "DARK BRONZE"
KW-1	KNOTWOOD PREFINISHED ALUMINUM SIDING MATERIAL "LIGHT OAK"
BR-1	FACE BRICK
BR-2	FACE BRICK
ST-1	THIN STONE
E-8	EIFS - "TAN"
BL-1	BOLLARD SLEEVE - "YELLOW"
PEX-1	PAINT - DARK BRONZE (SEE PAINT SCHEDULE)
PEX-3	PRE-FINISHED DARK BRONZE SIGN CABINETS (SEE PAINT SCHEDULE)
PEX-6	PRE-FINISHED WALK-IN FREEZER / COOLER DARK BRONZE (SEE PAINT SCHEDULE)
PEX-10	PAINT / POWDER COAT - BLACK (SEE PAINT SCHEDULE)



SITE NUMBER:	13352
BASE MODEL:	(2.0) SMART 40+
ASSET TYPE:	FRAN./CORP.
CLASSIFICATION:	NEW BUILD
OWNER:	XXXX
BASE VERSION:	2020
UPGRADE CLASSIFICATION:	FRAN./CORP. NEW BUILD
PROJECT YEAR:	2020
FURNITURE PACKAGE:	UM BRITE
DRAWING RELEASE:	SUMMER 2020

Wallin - Gomez Architects, Ltd.
 711 South Dearborn Street
 Suite 606
 Chicago, IL 60605
 Tel: 312-427-4702
 Fax: 312-427-6611
 e-mail: admin@wgald.com

Wendy's
 STORE #13352
 5505 NORTHWEST HIGHWAY
 CRYSTAL LAKE, IL 60014

REV.	DATE	DESCRIPTION
		ISSUED FOR REVIEW

PLOT DATE: 6/21/2021
 PROJECT NUMBER: 20XXX
 DRAWN BY: CL
 CHECKED BY: DJW

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STATE OF ILLINOIS
 #0011010990
 REGISTERED ARCHITECT
 DONALD J. WALLIN
 EXPIRATION DATE: 11/30/2022

SHEET NAME
EXTERIOR ELEVATIONS

SHEET NUMBER
A2.2

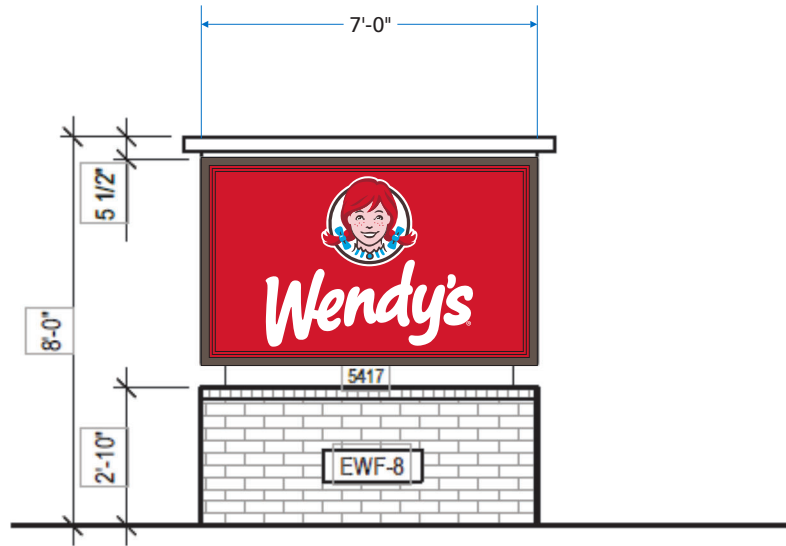
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NON-STANDARD MONUMENT OPTION A

Monument Sign Materials and Specifications

SCHEDULE OF EXTERIOR MATERIALS

MATERIALS	MANUFACTURER	MODEL NO.	COLOR	FINISH	DESCRIPTION
Brick Veneer	ACME	KING	Alpine: 763641	Heritage	MANUFACTURER: ACME, MODEL: KING, COLOR APLINE, FINISH: HERITAGE, ITEM # 763641 - MORTAR: SOLOMON COLORS, INC. 10X LIGHT BUFF. MORTAR JOINT PROFILE: WEATHER-STRUCK. 1/2 RUNNING BOND, REFER TO DETAIL.



3 MONUMENT SIGN ELEVATION
A4.20 SCALE: 1/4" = 1'-0"



ALL WIRING MEETS NEC 2017 SPECIFICATIONS. SIGN TO BE WIRED TO A STANDARD 120 VOLT / 20 AMP CIRCUIT. SIGN IS A U.L. LABELED PRODUCT
 THIS SIGN IS INTENDED TO BE INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS OF ARTICLE 600 OF THE NATIONAL ELECTRICAL CODE AND/OR OTHER APPLICABLE LOCAL CODES.
 THIS INCLUDES PROPER GROUNDING AND BONDING OF THE SIGN.

SIGN TYPE	SPECIFICATIONS	COLORS	SIGN SURVEY	ARTWORK
<input checked="" type="checkbox"/> DOUBLE FACE <input checked="" type="checkbox"/> ILLUMINATED <input checked="" type="checkbox"/> GROUND	- REPLACEMENT FACES: .150 PAN FORMED SOLAR GRADE POLYCARBONATE FACES WITH FLAT DECORATED GRAPHICS.	<input checked="" type="checkbox"/> CLASSIC BRONZE <input checked="" type="checkbox"/> PMS 186 RED <input type="checkbox"/> WHITE <input checked="" type="checkbox"/> PMS 186 RED HAIR <input checked="" type="checkbox"/> PMS 201 BURGUNDY HAIR <input checked="" type="checkbox"/> PMS 299 BLUE <input checked="" type="checkbox"/> PMS 698 PINK <input checked="" type="checkbox"/> PMS 439 BROWN	<input type="checkbox"/> FACE TRIMS <input type="checkbox"/> FASCIA COLOR <input type="checkbox"/> RADIUS DIM. <input type="checkbox"/> BUILDING DIM. <input type="checkbox"/> FACADE ANGLE <input type="checkbox"/> OTHER	<input checked="" type="checkbox"/> PROVIDED <input type="checkbox"/> REQUIRED If required, customer to provide Custom Sign Center, Inc. with camera ready or appropriate digital file. Please contact your salesperson or Design Staff for appropriate file types.

custom SIGN Center, Inc. Full Service Sign Company
 3200 Valleyview Drive Columbus, OH 43204
 PH: 614-279-6700 FX: 614-279-7525

FILENAME: NON-STANDARD REP FACES REV# SCALE: 1/4" = 1'
 DATE: 4/21/21 REV. DATE: DGNR: MDA SALES: T. SHEEHY Approval Signatures Required to insure that all spelling, colors, and specifications for sign(s) rendered meets customer & landlord approvals. CUSTOMER: _____ DATE: _____ LANDLORD: _____ DATE: _____
 This original design and all information herein are the property of Custom Sign Center, Inc. Artwork is subject to return if not purchased. Any unauthorized use is forbidden. © Copyright 2005 All rights reserved

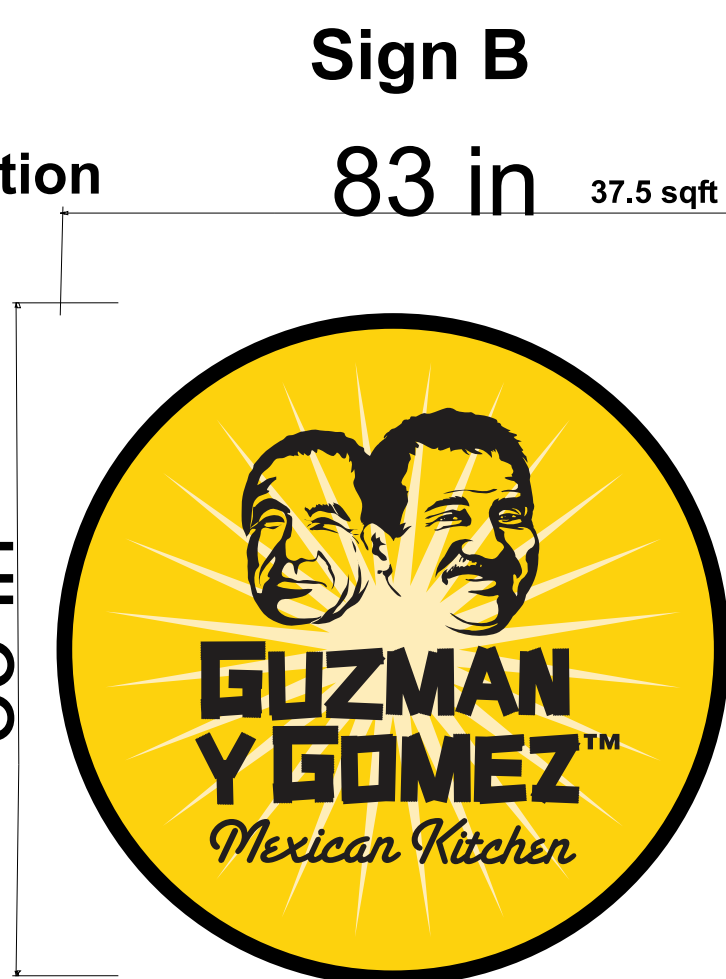
UL Listed channel Letters



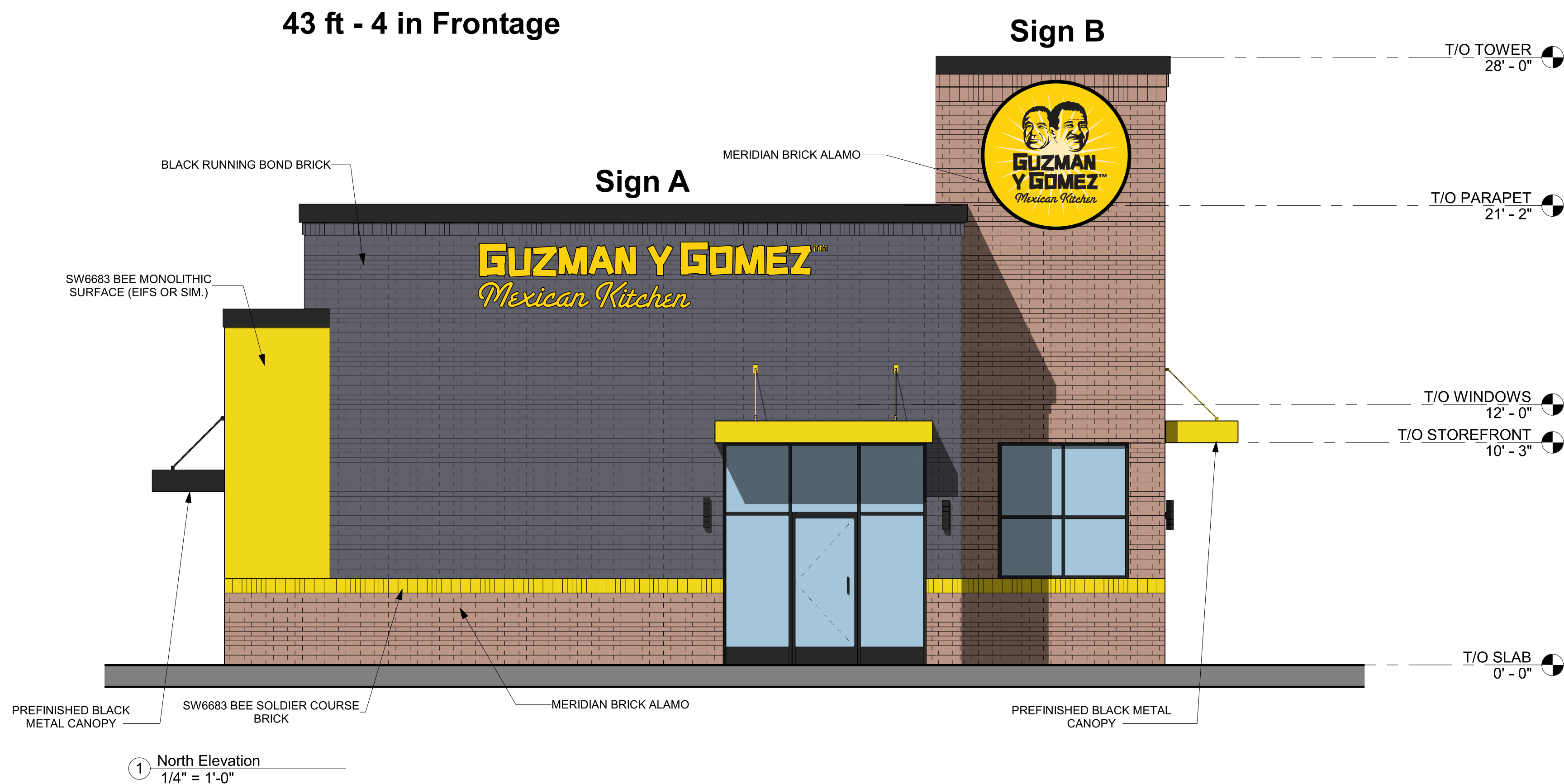
Illuminated LOGO
 plex face/LED internal illumination
 UL Listed

5in projection

radius = 41.5 in
 Pi (radius squared)
 41.5in X 41.5in X 3.14 (Pi)
 5,407.86 in / 144 = 37.7 sqft



43 ft - 4 in Frontage

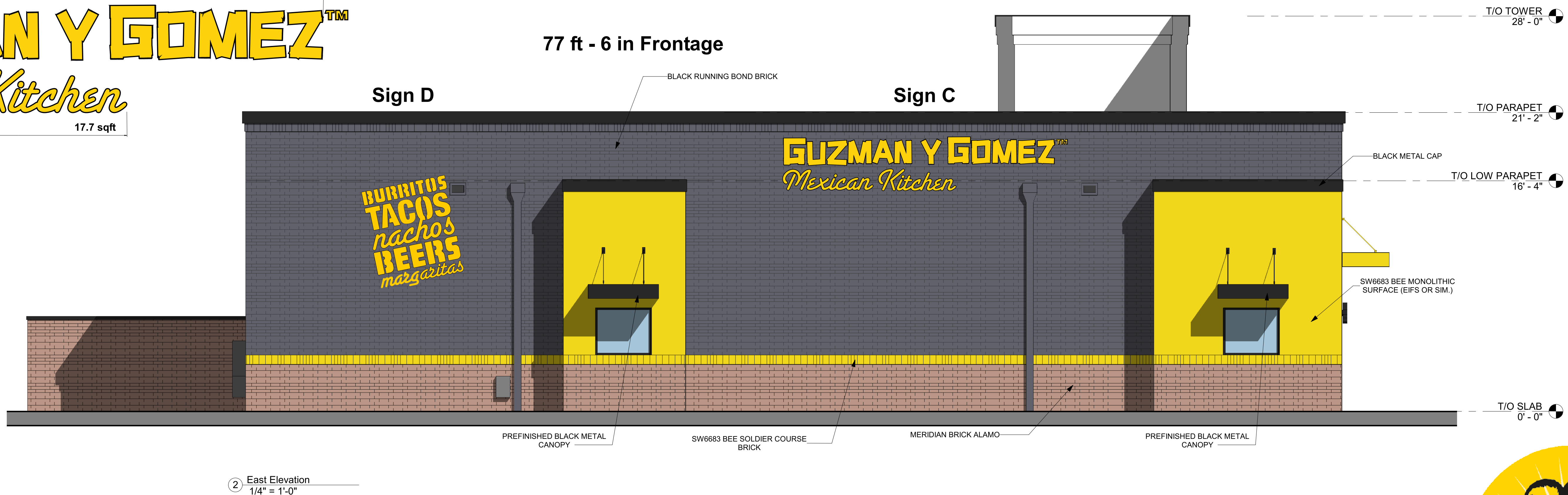


UL Listed channel Letters

Sign C



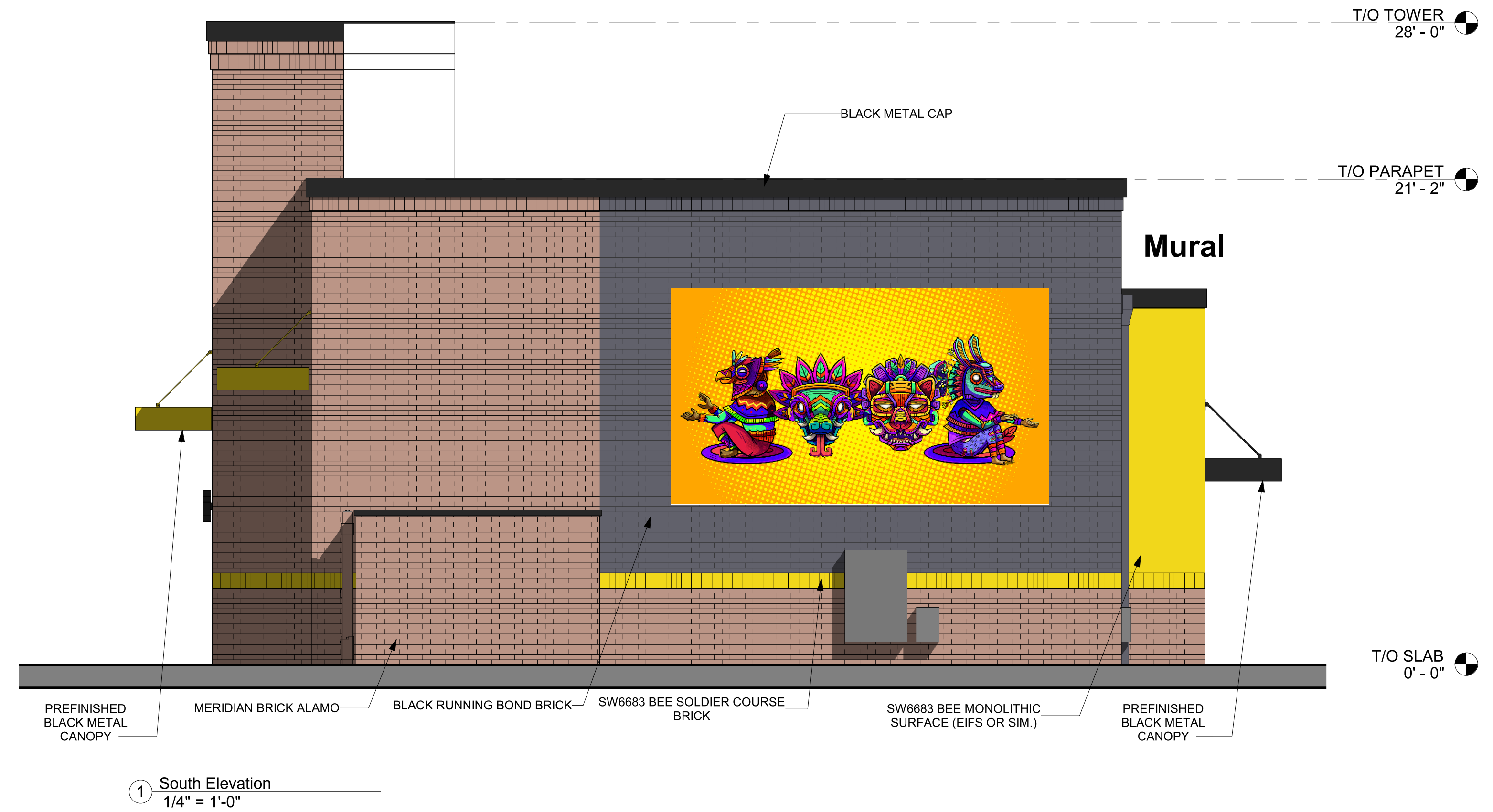
77 ft - 6 in Frontage



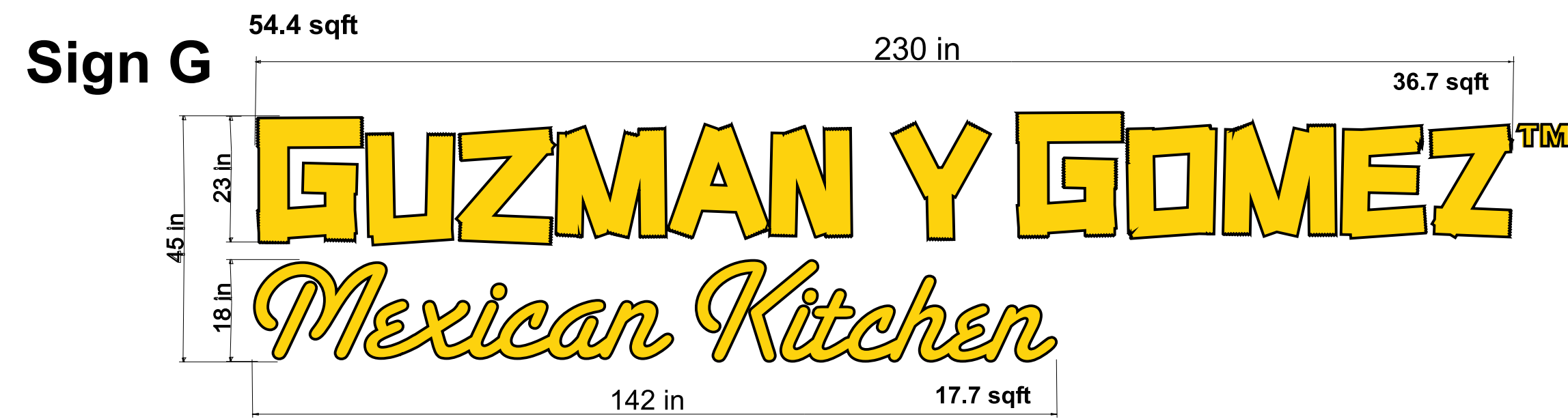
Sign D Mural Painted on fascia



Removed from plan
Sign E



UL Listed channel Letters



Sign F

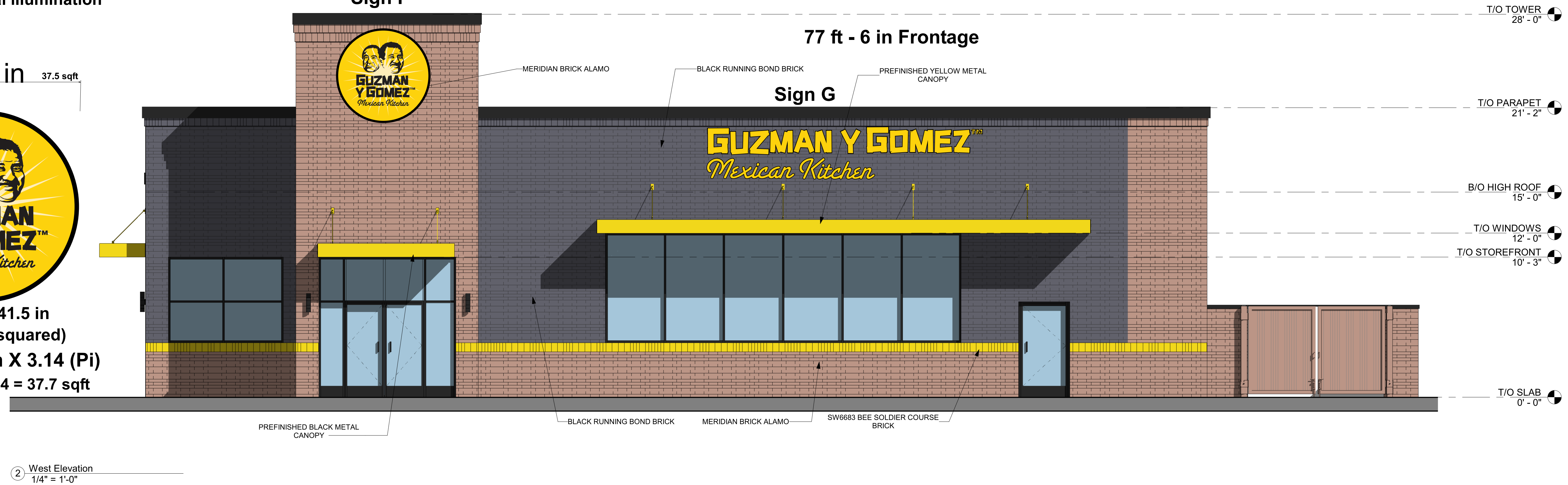
Illuminated LOGO
plex face/LED internal illumination
UL Listed

5in projection



Sign F

77 ft - 6 in Frontage



GW PROPERTIES PROPOSED DEVELOPMENT

5417-5501 NORTHWEST HWY
CRYSTAL LAKE, ILLINOIS

DESIGN STUDIO 24, LLC.
ARCHITECTS - DESIGNERS - PLANNERS
2211 N. Elston Ave. Suite 304, Chicago, Illinois 60614 Fax: 847.885.7751 Tel.: 847.885.8888

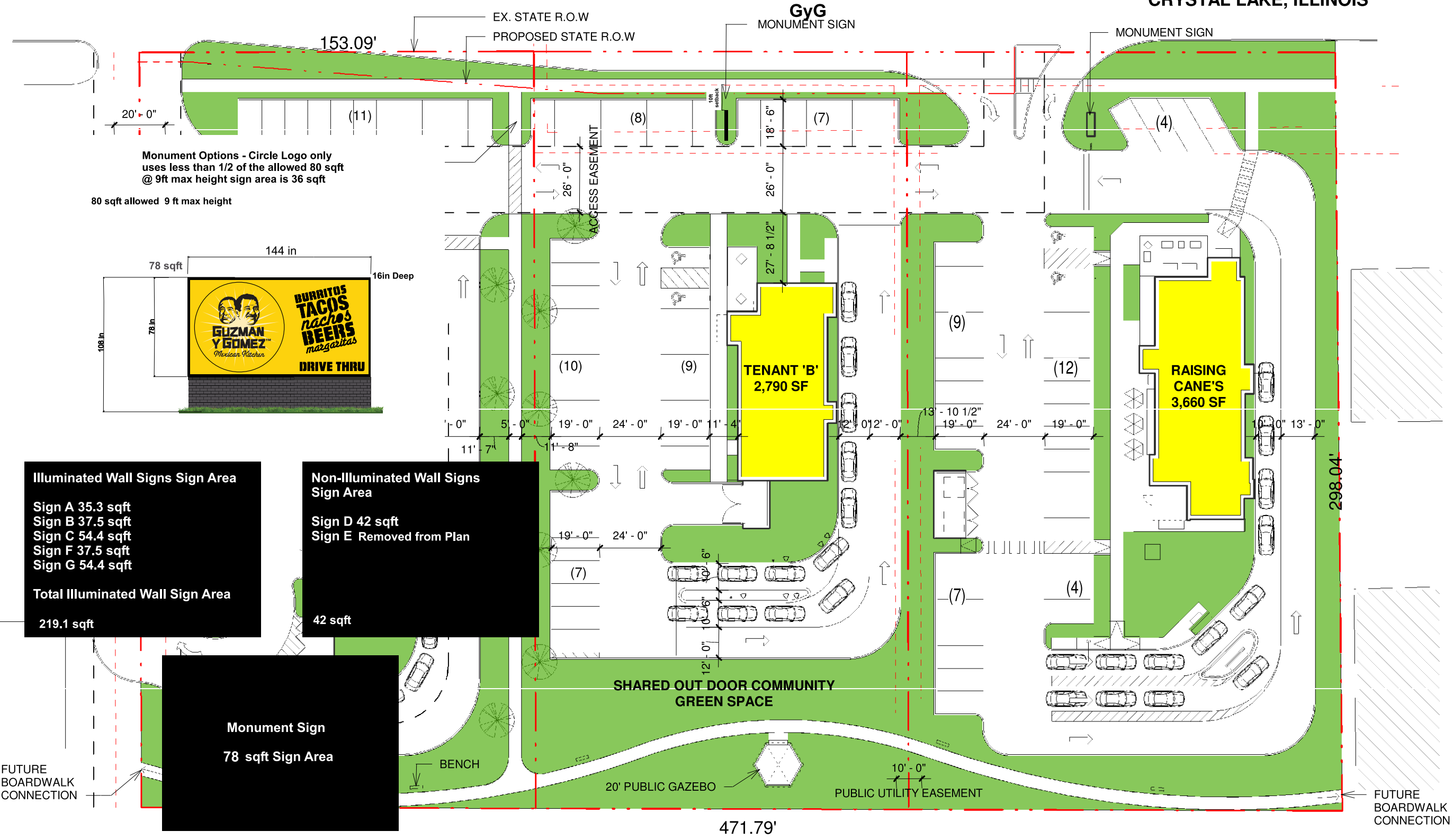
GW PROPERTIES
PROPOSED SITE STUDY
5417-5501 NORTHWEST HWY
CRYSTAL LAKE, ILLINOIS

DATE 03.24.2021

MARK T. DIGANCI
REG. ARCHITECT

A2

NORTHWEST (U.S. ROUTE 14) HIGHWAY



Monument Options - Circle Logo only
uses less than 1/2 of the allowed 80 sqft
@ 9ft max height sign area is 36 sqft

80 sqft allowed 9 ft max height

Illuminated Wall Signs Sign Area

- Sign A 35.3 sqft
- Sign B 37.5 sqft
- Sign C 54.4 sqft
- Sign F 37.5 sqft
- Sign G 54.4 sqft

Total Illuminated Wall Sign Area
219.1 sqft

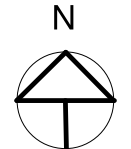
Non-Illuminated Wall Signs Sign Area

- Sign D 42 sqft
- Sign E Removed from Plan

42 sqft

Monument Sign
78 sqft Sign Area

PARKING SPACE:
WENDY'S - 26 SPACES (2 ADA STALLS)
GYG - 41 SPACES (2 ADA STALLS)
RAISING CANES - 36 SPACES (2 ADA STALLS)



1 SITE PLAN - OVERALL
1" = 40'-0"



Illuminated Wall Signs Sign Area

Sign A 35.3 sqft
 Sign B 37.5 sqft
 Sign C 54.4 sqft
 Sign F 37.5 sqft
 Sign G 54.4 sqft

Total Illuminated Wall Sign Area

219.1 sqft

Non-Illuminated Wall Signs Sign Area

Sign D 42 sqft
 Sign E Removed from Plan

42 sqft

Monument Sign

78 sqft Sign Area

FUTURE BOARDWALK CONNECTION

BENCH



PRELIMINARY ENGINEERING PLANS

GW CRYSTAL LAKE

5417 NORTHWEST HIGHWAY

CRYSTAL LAKE, IL 60014



UTILITY AND GOVERNING AGENCY CONTACTS

ENGINEERING DEPARTMENT
CITY OF CRYSTAL LAKE
100 WEST WOODSTOCK STREET
CRYSTAL LAKE, IL 60014
TEL: (815) 356-3614
CONTACT: KEVIN LILL

ROADWAY AUTHORITY
ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, IL 60196
TEL: (847) 705-4145
CONTACT: YELEINA I. HAYDEL

SANITARY SEWER SERVICE
CITY OF CRYSTAL LAKE
100 WEST WOODSTOCK STREET
CRYSTAL LAKE, IL 60014
TEL: (815) 356-3797
CONTACT: TBD

POWER COMPANY
COMED
350 S SECOND STREET
ELGIN, IL 60123
TEL: (847) 608-2382
CONTACT: JOSE "JAYVEE" ROLDAN III

STORM SEWER SERVICE
CITY OF CRYSTAL LAKE
100 WEST WOODSTOCK STREET
CRYSTAL LAKE, IL 60014
TEL: (815) 356-3797
CONTACT: ABIGAIL WILGREEN

NATURAL GAS COMPANY
NICOR GAS
TEL: (847) 598-4011
CONTACT: FORAM SHAH

TELEPHONE
AT&T
TEL: (815) 337-4732
CONTACT: DAVID SAINT GERMAIN

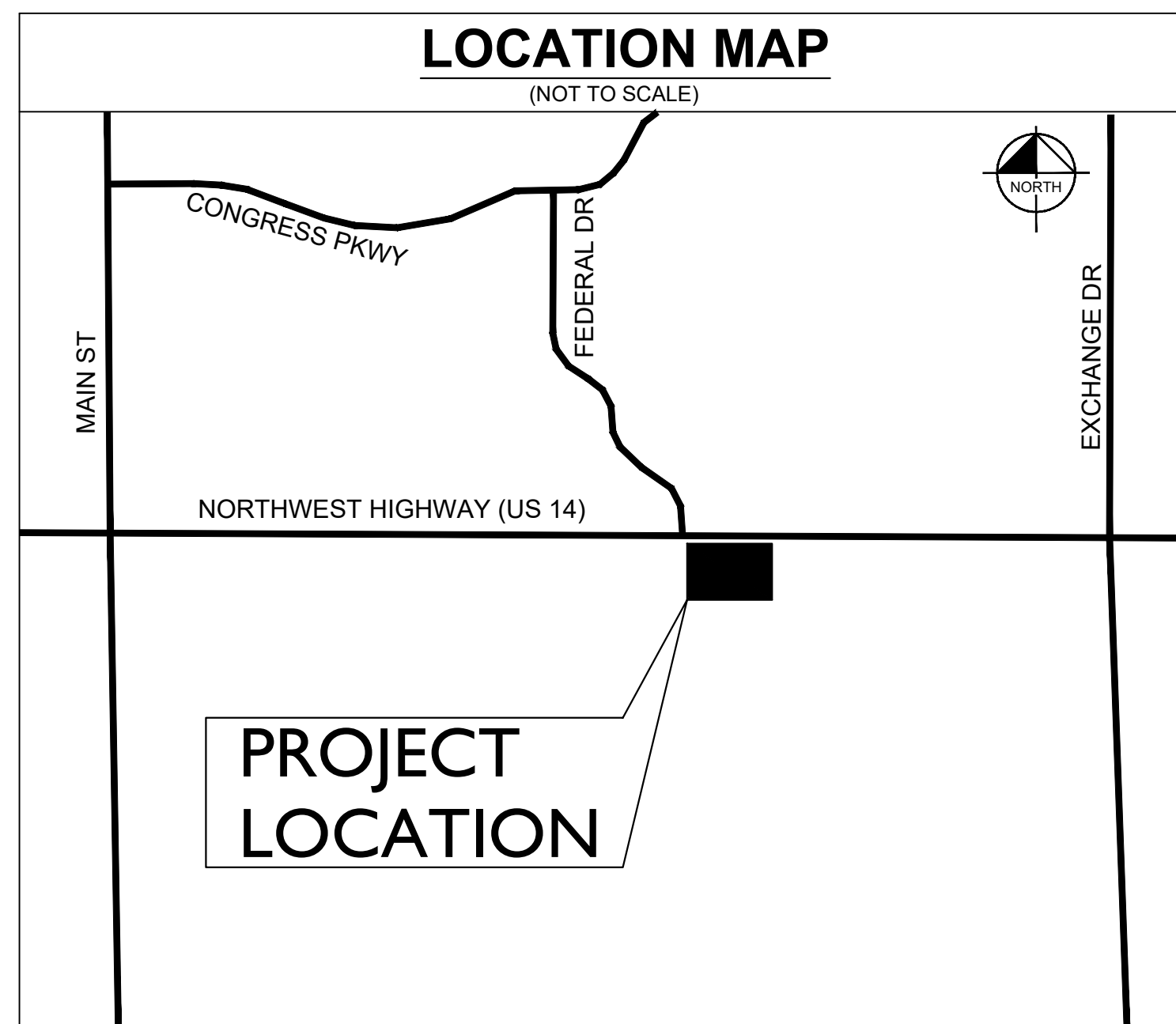
PROJECT TEAM

DEVELOPER
GW PROPERTIES
2211 N ELSTON AVE, SUITE 304
CHICAGO, IL 60614
TEL: (773) 382-0590
CONTACT: MITCH GOLTZ

CIVIL ENGINEER
KIMLEY-HORN AND ASSOCIATES, INC.
4201 WINFIELD ROAD, SUITE 600
WARRENVILLE, IL 60555
TEL: (630) 487-5550
EMAIL: ERIC.TRACY@KIMLEY-HORN.COM
CONTACT: ERIC TRACY, P.E.

LANDSCAPE ARCHITECT
PLANNED ENVIRONMENT ASSOCIATES
P.O BOX 2256
CHESTERTON, IN 46304
TEL: (219) 299-3383
CONTACT: JON RUBLE, P.L.A.

SURVEYOR
UNITED SURVEY SERVICE, LLC
7710 CENTRAL AVENUE
RIVER FOREST, IL 60305
TEL: (847) 299-1010
E-MAIL: USURVEY@USANDCS.COM



Sheet List Table	
Sheet Number	Sheet Title
C0.0	TITLE SHEET
V0.0	SURVEY (BY OTHERS)
C1.0	GENERAL NOTES
C1.1	CITY STANDARD NOTES
C2.0	EXISTING CONDITIONS & DEMO PLAN
C3.0	SITE PLAN
C4.0	EROSION CONTROL PLAN
C4.1	EROSION CONTROL NOTES & DETAILS
C5.0	GRADING PLAN
C6.0	UTILITY PLAN
C7.0	CONSTRUCTION DETAILS
C7.1	CONSTRUCTION DETAILS

LEGAL DESCRIPTION

PARCEL 1:
LOT 1 IN PAULY SUBDIVISION, BEING A SUBDIVISION OF PART OF THE NORTHWEST QUARTER OF SECTION 9, TOWNSHIP 43 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 7, 2008 AS DOCUMENT 2008R0052561, IN THE CITY OF CRYSTAL LAKE, IN MCHENRY COUNTY, ILLINOIS.

PARCEL 2:
LOT 2 IN PAULY SUBDIVISION, BEING A SUBDIVISION OF PART OF THE NORTHEAST QUARTER OF SECTION 9, TOWNSHIP 43 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 7, 2008 AS DOCUMENT 2008R0052561, IN THE CITY OF CRYSTAL LAKE, IN MCHENRY COUNTY, ILLINOIS.

PARCEL 3:
COMMENCING AT THE NORTHWEST CORNER OF THE NORTHEAST 1/4 OF SECTION 9, TOWNSHIP 43 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, RUNNING THEN EAST ALONG THE NORTH LINE OF SAID NORTHEAST 1/4, 410 FEET FOR THE PLACE OF BEGINNING; THENCE SOUTH PARALLEL TO THE WEST LINE OF SAID NORTHEAST 1/4, 330 FEET TO A POINT; THENCE EAST PARALLEL TO THE NORTH LINE OF SAID NORTHEAST 1/4, 155 FEET TO A POINT; THENCE NORTH PARALLEL TO THE WEST LINE OF SAID NORTHEAST 1/4, 330 FEET TO A POINT ON THE NORTH LINE OF SAID NORTHEAST 1/4; THENCE WEST ALONG SAID NORTH LINE 155 FEET TO THE PLACE OF BEGINNING (EXCEPTING AND RESERVING THEREFROM THAT PART OF LAND DEDICATED FOR STATE HIGHWAY PURPOSES), IN MCHENRY COUNTY, ILLINOIS ALSO EXCEPTING THEREFROM: THAT PART OF THE NORTHEAST 1/4 OF SECTION 9, TOWNSHIP 43 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: COMMENCING AT THE NORTHWEST CORNER OF THE NORTHEAST 1/4 OF SECTION 9, RUNNING THENCE EAST ALONG THE NORTH LINE OF SAID NORTHEAST 1/4, ON AN ASSUMED BEARING OF SOUTH 89 DEGREES 59 MINUTES 37 SECONDS EAST, 410 FEET, THENCE SOUTH 02 DEGREES 14 MINUTES 06 SECONDS EAST, PARALLEL TO THE WEST LINE LINE OF SAID NORTHEAST 1/4, 30.02 FEET TO THE PLACE OF BEGINNING, THENCE CONTINUING SOUTH 02 DEGREES 14 MINUTES 06 SECONDS EAST, PARALLEL WITH THE SAID WEST LINE OF SAID NORTHEAST 1/4, A DISTANCE OF 3.30 FEET, THENCE NORTH 89 DEGREES 40 MINUTES 36 SECONDS EAST, A DISTANCE OF 137.10 FEET; THENCE NORTH 89 DEGREES 55 MINUTES 36 SECONDS EAST, 17.87 FEET; THENCE NORTH 02 DEGREES 14 MINUTES 06 SECONDS WEST, 2.48 FEET; THENCE NORTH 89 DEGREES 59 MINUTES 37 SECONDS WEST, A DISTANCE OF 155.00 FEET, TO THE PLACE OF BEGINNING, SITUATED IN MCHENRY COUNTY, ILLINOIS.

BENCHMARKS

SITE BENCHMARKS:
(LOCATIONS SHOWN ON SURVEY)

NORTHWEST BONNET BOLT ON FIRE HYDRANT.
ELEVATION=913.66 (NAVD 88).

PROFESSIONAL ENGINEER'S CERTIFICATION

I, ERIC J. TRACY, A LICENSED PROFESSIONAL ENGINEER OF ILLINOIS, HEREBY CERTIFY THAT THIS SUBMISSION, PERTAINING ONLY TO THE "C" SERIES CIVIL SHEETS LISTED ABOVE, WAS PREPARED ON BEHALF OF GW PROPERTIES BY KIMLEY-HORN AND ASSOCIATES, INC. UNDER MY PERSONAL DIRECTION. THIS TECHNICAL SUBMISSION IS INTENDED TO BE USED AS AN INTEGRAL PART OF AND IN CONJUNCTION WITH THE PROJECT SPECIFICATIONS AND CONTRACT DOCUMENTS.

DATED THIS ____ DAY OF _____, A.D., 2021.

ILLINOIS LICENSED PROFESSIONAL ENGINEER 062-067482
MY LICENSE EXPIRES ON NOVEMBER 30, 2021

Kimley-Horn
© 2021 KIMLEY-HORN AND ASSOCIATES, INC.
4201 WINFIELD ROAD, SUITE 600
WARRENVILLE, IL 60555
WWW.KIMLEY-HORN.COM

SCALE:
AS NOTED
DESIGNED BY: JRS
DRAWN BY: JRS
CHECKED BY: EJT

TITLE SHEET

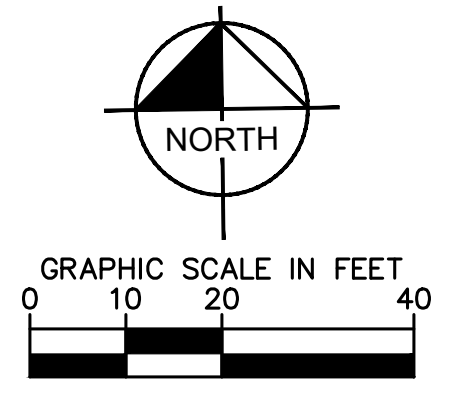
GW CRYSTAL LAKE
5417 NORTHWEST HWY
CRYSTAL LAKE, IL 60014

ORIGINAL ISSUE:
X/XX/21
KHA PROJECT NO.
168726001

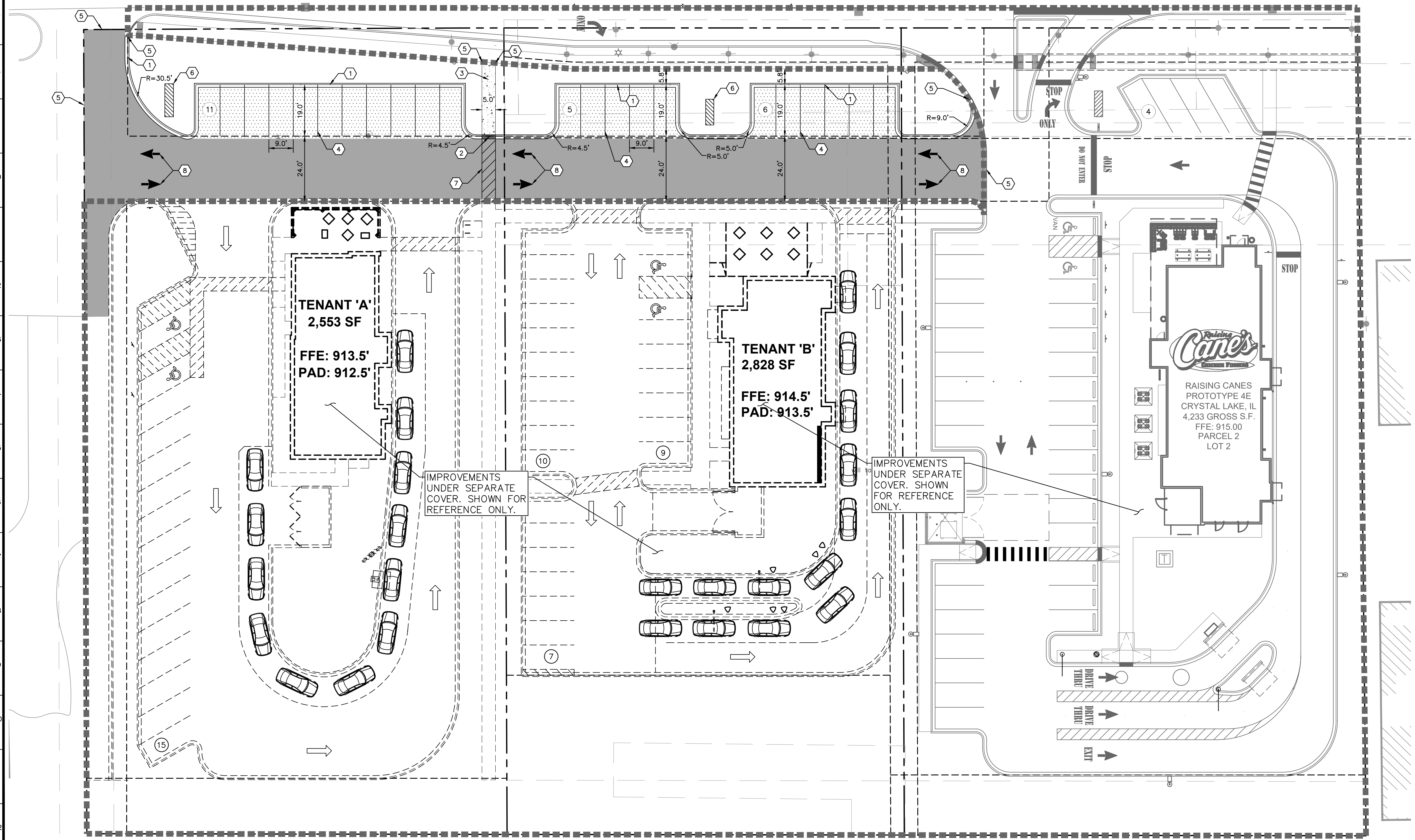
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Drawing name: K:\GIS\DEV\168726001_GW Properties-Crystal Lake, IL\2 Design\CAD\PlanSheets\00.0 TITLE SHEET.dwg Layout Aug 06, 2021 12:11pm By: Eric Tracy
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Drawing name: K:\GIS_DEVELOPMENT\Projects-Crystal Lake, IL\Design\CAD\PlanSheets\C3.0 SITE PLAN.dwg Layout1 Aug 06, 2021 12:12pm by: Eric.Trosy
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**IL ROUTE 14 (NORTHWEST HIGHWAY)
NORTHWEST HIGHWAY**



KEY NOTES

- 1 B6.12 CONCRETE CURB AND GUTTER, TYP. (SEE DETAILS)
- 2 DEPRESSED CURB AND GUTTER
- 3 CONCRETE SIDEWALK, TYP. (SEE DETAILS)
- 4 4" WIDE PAINTED SOLID LINE, TYP.
- 5 CONNECT TO EXISTING PAVEMENT, SIDEWALK, CURB, TYP.
- 6 MONUMENT OR PYLON SIGN (SEE ARCHITECTURAL PLANS FOR DETAILS)
- 7 CROSSWALK STRIPING (SEE DETAILS)
- 8 DIRECTIONAL ARROW (SEE DETAILS)
- 9 LIGHT POLES SHOWN FOR COORDINATION ONLY (SEE SITE LIGHTING PLANS)

PAVING AND CURB LEGEND

	STANDARD DUTY ASPHALT PAVEMENT SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
	HEAVY DUTY ASPHALT PAVEMENT SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
	CONCRETE SIDEWALK SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
	LIMITS OF SCOPE
	STANDARD PITCH CONCRETE CURB AND GUTTER
	REVERSE PITCH CONCRETE CURB AND GUTTER
	CONCRETE DEPRESSED CURB AND GUTTER

PARKING SUMMARY

TENANT A	
PARKING SPACES REQUIRED (CITY STANDARD)	= 26 SPACES (1 SPACE/100 SF GFA)
PARKING SPACES PROVIDED	= 24 SPACES
ACCESSIBLE PARKING SPACES REQUIRED	= 2 SPACES
ACCESSIBLE PARKING SPACES PROVIDED	= 2 SPACES
TOTAL PARKING SPACES PROVIDED	= 26 SPACES
TENANT B	
PARKING SPACES REQUIRED (CITY STANDARD)	= 29 SPACES (1 SPACE/100 SF GFA)
PARKING SPACES PROVIDED	= 35 SPACES
ACCESSIBLE PARKING SPACES REQUIRED	= 2 SPACES
ACCESSIBLE PARKING SPACES PROVIDED	= 2 SPACES
TOTAL PARKING SPACES PROVIDED	= 37 SPACES

FEMA NOTE

PER FLOOD INSURANCE RATE MAP PANEL NO. 1711C0335J, THE SITE OR A PORTION OF THE SITE IS LOCATED IN ZONE X, AREA OF MINIMAL FLOOD HAZARD.

No.	REVISIONS	DATE	BY
Kimley»Horn			
© 2021 KIMLEY-HORN AND ASSOCIATES, INC. 4201 WINFIELD ROAD, SUITE 600 WARRENVILLE, IL 60555 WWW.KIMLEY-HORN.COM			
SCALE:	DESIGNED BY: JRS	DRAWN BY: JRS	CHECKED BY: EJT
SITE PLAN			
GW CRYSTAL LAKE			
5417 NORTHWEST HWY CRYSTAL LAKE, IL 60014			
ORIGINAL ISSUE: X/XX/21			
KHA PROJECT NO. 168726001			
SHEET NUMBER			
C3.0			

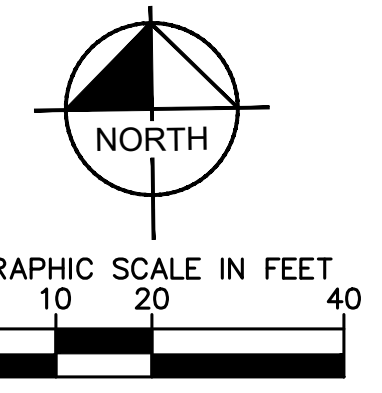
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EROSION CONTROL SCHEDULE AND SEQUENCING:

- I. ROUGH GRADING CONSTRUCTION ENTRANCE/EXIT, SILT FENCE PROTECTION, CONCRETE WASHOUT AREA AND TREE PROTECTION SHALL BE INSTALLED PRIOR TO THE INITIATION OF ROUGH GRADING, AS NEEDED. TEMPORARY EROSION CONTROL MEASURES TO BE INSTALLED UPON COMPLETION OF ROUGH GRADING AND AS NECESSARY THROUGHOUT CONSTRUCTION.
- II. UTILITY INSTALLATION ALL PRIOR EROSION CONTROL MEASURES INSTALLED ABOVE TO BE MAINTAINED AS NECESSARY DURING UTILITY INSTALLATION. STORM STRUCTURE INLET PROTECTION SHALL BE INSTALLED AS STORM DRAINAGE SYSTEM IS CONSTRUCTED.
- III. PAVING ALL PRIOR EROSION CONTROL MEASURES INSTALLED ABOVE TO BE MAINTAINED AS NECESSARY DURING PAVING AND THROUGHOUT THE REMAINDER OF THE PROJECT.
- IV. FINAL GRADING/SOIL STABILIZATION/LANDSCAPING ALL TEMPORARY EROSION CONTROL MEASURES TO BE REMOVED AT THE CONCLUSION OF THE PROJECT AS DIRECTED BY THE LOCAL MUNICIPALITY.

EROSION CONTROL LEGEND

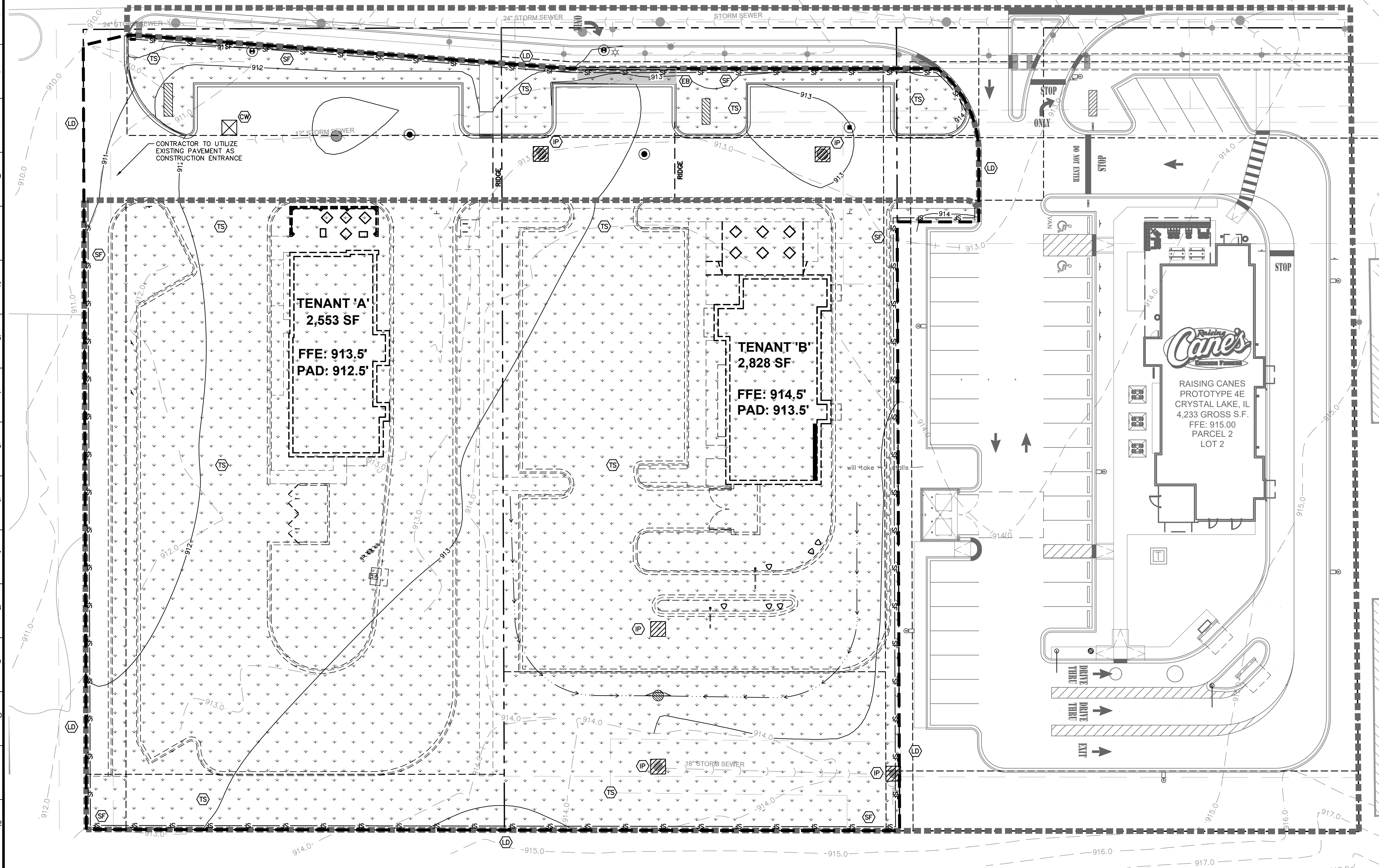
	TS TEMPORARY SEEDING (SEE EROSION CONTROL DETAILS)
	EB TEMPORARY EROSION CONTROL BLANKET (SEE EROSION CONTROL NOTE #7)
	CE CONSTRUCTION ENTRANCE (SEE EROSION CONTROL DETAILS)
	SF SILT FENCE (SEE EROSION CONTROL DETAILS)
	IP INLET PROTECTION (SEE EROSION CONTROL DETAILS)
	CW CONCRETE WASHOUT (SEE EROSION CONTROL DETAILS) (TO BE DETERMINED BY CONTRACTOR)
	SS TEMPORARY SOIL STOCKPILE
	LD LIMITS OF DISTURBANCE
	LIMITS OF SCOPE
	EXISTING CONTOURS
	PROPOSED CONTOURS



EROSION CONTROL NOTES

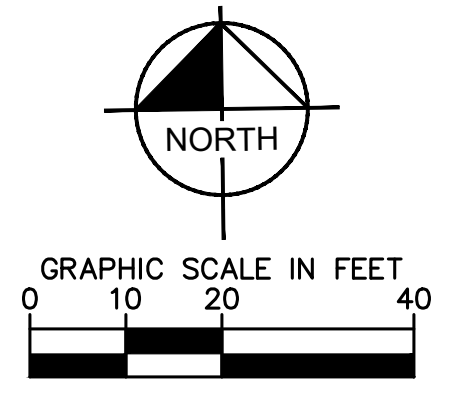
1. CONSTRUCTION ENTRANCE SHALL BE LOCATED SO AS TO PROVIDE THE LEAST AMOUNT OF DISTURBANCE TO THE FLOW OF TRAFFIC IN AND OUT OF THE SITE. ADDITIONALLY, CONSTRUCTION ENTRANCE SHALL BE LOCATED TO CONDUCE WITH THE PHASING OF THE PAVEMENT REPLACEMENT.
2. POST CONSTRUCTION STORM WATER POLLUTION CONTROL MEASURES INCLUDE STABILIZATION BY PERMANENT PAVING, DRAINAGE SYSTEM STRUCTURE, OR LANDSCAPING.
3. TEMPORARY AND PERMANENT STABILIZATION PRACTICES AND BMP'S SHALL BE INSTALLED AT THE EARLIEST POSSIBLE TIME DURING THE CONSTRUCTION SEQUENCE. AS AN EXAMPLE, PERIMETER SILT FENCE SHALL BE INSTALLED BEFORE COMMENCEMENT OF ANY GRADING ACTIVITIES. OTHER BMP'S SHALL BE INSTALLED AS SOON AS PRACTICABLE AND SHALL BE MAINTAINED UNTIL FINAL SITE STABILIZATION IS ATTAINED. CONTRACTOR SHALL ALSO REFERENCE CIVIL AND LANDSCAPE PLANS SINCE PERMANENT STABILIZATION IS PROVIDED BY LANDSCAPING, THE BUILDING(S), AND SITE PAVING.
4. BMP'S HAVE BEEN LOCATED AS INDICATED ON THIS PLAN IN ACCORDANCE WITH GENERALLY ACCEPTED ENGINEERING PRACTICES IN ORDER TO MINIMIZE SEDIMENT TRANSFER. FOR EXAMPLE: SILT FENCES LOCATED AT TOE OF SLOPE AND INLET PROTECTION FOR INLETS RECEIVING SEDIMENT FROM SITE RUN-OFF.
5. THE PLACEMENT OF EROSION/SEDIMENTATION CONTROLS SHALL BE IN ACCORDANCE WITH THE APPROVED EROSION AND SEDIMENTATION CONTROL PLAN.
6. ANY MAJOR VARIATION IN MATERIALS OR LOCATIONS OF CONTROLS OR FENCES FROM THOSE SHOWN ON THE APPROVED PLANS WILL REQUIRE A REVISION AND MUST BE APPROVED BY THE REVIEWING ENGINEER, ENVIRONMENTAL SPECIALIST, OR ARBORIST AS APPROPRIATE. MAJOR REVISIONS MUST BE APPROVED BY THE PLANNING AND DEVELOPMENT DEPARTMENT AND THE DRAINAGE UTILITY DEPARTMENT. MINOR CHANGES OR ADDITIONAL CONTROL MEASURES TO BE MADE AS FIELD REVISIONS TO THE EROSION AND SEDIMENTATION CONTROL PLAN MAY BE REQUIRED BY THE ENVIRONMENTAL INSPECTOR DURING THE COURSE OF CONSTRUCTION TO CORRECT CONTROL INADEQUACIES AT NO ADDITIONAL COST TO THE OWNER.
7. CONTRACTOR SHALL PLACE EROSION CONTROL BLANKET (NORTH AMERICAN GREEN S150BN OR APPROVED EQUAL) ON ALL SITE AREAS WITH SLOPES GREATER THAN 4:1, AND IN THE BOTTOM AND SIDE SLOPES OF ALL SWALES.
8. PRIOR TO FINAL ACCEPTANCE, HAUL ROADS AND WATERWAY CROSSINGS CONSTRUCTED FOR TEMPORARY CONTRACTOR ACCESS MUST BE REMOVED, ACCUMULATED SEDIMENT REMOVED FROM THE WATERWAY AND THE AREA RESTORED TO THE ORIGINAL GRADE AND CONDITION. ALL LAND CLEARING SHALL BE DISPOSED OF IN APPROVED SPOIL DISPOSAL SITES.
9. PERMANENT FINAL PLANT COVERING OR STRUCTURES SHALL BE INSTALLED PRIOR TO FINAL ACCEPTANCE.
10. ALL CONTROL DEVICES THAT FUNCTION SIMILARLY TO SILT FENCE OR FIBER ROLLS MUST BE REPAIRED, REPLACED OR SUPPLEMENTED WITH EFFECTIVE CONTROLS WHEN THEY BECOME NONFUNCTIONAL OR THE SEDIMENT REACHES ONE-THIRD THE HEIGHT OF THE DEVICE. THESE REPAIRS MUST BE MADE WITHIN 24 HOURS OF THE RAINFALL EVENT OR AS SOON AS FIELD CONDITIONS ALLOW ACCESS.
11. ALL SEDIMENT DELTAS AND DEPOSITS MUST BE REMOVED FROM SURFACE WATERS, DRAINAGE WAYS, CATCH BASINS AND OTHER DRAINAGE SYSTEMS. ALL AREAS WHERE SEDIMENT REMOVAL RESULTED IN EXPOSED SOIL MUST BE RESTABILIZED. THE REMOVAL AND STABILIZATION MUST TAKE PLACE IMMEDIATELY, BUT NO MORE THAN 7 DAYS AFTER THE RAINFALL EVENT UNLESS PRECLUDED BY LEGAL, REGULATORY OR ACCESS CONSTRAINTS. ALL REASONABLE EFFORTS MUST BE USED TO OBTAIN ACCESS. ONCE ACCESS IS OBTAINED, REMOVAL AND STABILIZATION MUST TAKE PLACE IMMEDIATELY, BUT NO MORE THAN 7 DAYS LATER. CONTRACTOR IS RESPONSIBLE FOR CONTACTING ALL APPROPRIATE AUTHORITIES AND RECEIVING THE APPLICABLE PERMITS PRIOR TO CONDUCTING ANY WORK.
12. ACCUMULATIONS OF TRACKED AND DEPOSITED SEDIMENT MUST BE REMOVED FROM OFF-SITE PAVED SURFACES WITHIN 24 HOURS OR SOONER IF REQUIRED. SEDIMENT TRACKING MUST BE MINIMIZED BY THE APPROPRIATE MANAGEMENT PRACTICE, LIKE A DEDICATED SITE EXIT WITH AN AGGREGATE SURFACE OR DESIGNATED OFFSITE PARKING AREA. CONTRACTOR IS RESPONSIBLE FOR STREET SWEEPING AND/OR SCRAPING IF YOUR PRACTICES ARE NOT ADEQUATE TO PREVENT SEDIMENT FROM BEING TRACKED FROM THE SITE.
13. SURFACE WATERS, DRAINAGE DITCHES AND CONVEYANCE SYSTEMS MUST BE INSPECTED FOR SEDIMENT DEPOSITS.
14. THE CONTRACTOR SHALL INSTALL AND MAINTAIN ALL EROSION CONTROL MEASURES AS INDICATED ON THIS SHEET IN ACCORDANCE WITH THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP) PREPARED BY KIMLEY-HORN AND ASSOCIATES, INC. THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING THE PROVISIONS INDICATED IN THE SWPPP, INCLUDING EROSION CONTROL MEASURES AND INSPECTION FREQUENCY, AS REQUIRED BY THE IEPA NPDES PHASE II PERMIT PROGRAM REQUIREMENTS.
15. PUMPING SEDIMENT LADEN WATER INTO ANY STORMWATER FACILITY THAT IS NOT DESIGNATED TO BE A SEDIMENT TRAP, DRAINAGEWAY, OR OFFSITE AREA EITHER DIRECTLY OR INDIRECTLY WITHOUT FILTRATION IS PROHIBITED.
16. SOIL STOCKPILES SHALL NOT BE LOCATED IN A DRAINAGEWAY, FLOOD PLAN AREA OR A DESIGNATED BUFFER, UNLESS OTHERWISE APPROVED. UNDER SPECIFIC CONDITIONS TO BE ESTABLISHED BY THE DIRECTOR OR ADMINISTRATOR.
17. STOCKPILES TO REMAIN IN PLACE FOR MORE THAN THREE DAYS SHALL BE PROVIDED WITH SESC MEASURES. MATERIAL IS TO BE HAULED OFF IMMEDIATELY AND LEGALLY IF NO STOCKPILE IS TO REMAIN IN PLACE.
18. ALL TEMPORARY SESC MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED. TRAPPED SEDIMENT AND OTHER DISTURBED SOILS RESULTING FROM TEMPORARY MEASURES SHALL BE PROPERLY DISPOSED OF PRIOR TO PERMANENT STABILIZATION.
19. WATER REMOVED FROM TRAPS, BASINS, AND OTHER WATER HOLDING DEPRESSIONS OR EXCAVATIONS MUST FIRST PASS THROUGH A SEDIMENT CONTROL AND/OR FILTRATION DEVICE. WHEN DEWATERING DEVICES ARE USED, DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION.
20. SITE STABILIZATION REQUIREMENTS ARE AS FOLLOWS:
 - 20.1. WHERE THE INITIATION OF STABILIZATION MEASURE BY THE 7TH DAY AFTER CONSTRUCTION ACTIVITY TEMPORARILY OR PERMANENTLY CEASES ON A PORTION OF THE SITE IS PRECLUDED BY SNOW COVER, STABILIZATION MEASURE SHALL BE INITIATED AS SOON AS PRACTICABLE.
 - 20.2. WHERE CONSTRUCTION ACTIVITY WILL RESUME ON A PORTION OF THE SITE WITHIN 14 DAYS FROM WHEN ACTIVITIES CEASED, (E.G. THE TOTAL TIME PERIOD THAT CONSTRUCTION ACTIVITY IS TEMPORARILY CEASED IS LESS THAN 14 DAYS) THEN STABILIZATION MEASURES DO NOT HAVE TO BE INITIATED ON THAT PORTION OF THE SITE BY THE 7TH DAY AFTER CONSTRUCTION ACTIVITY TEMPORARILY CEASED.

**IL ROUTE 14 (NORTHWEST HIGHWAY)
NORTHWEST HIGHWAY**



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SCALE:	DESIGNED BY: JRS	DRAWN BY: JRS	
CHECKED BY: EJT	<p>EROSION CONTROL PLAN</p>		
<p>GW CRYSTAL LAKE</p> <p style="font-size: x-small;">5417 NORTHWEST HWY CRYSTAL LAKE, IL 60014</p>			
<p>ORIGINAL ISSUE: X/XX/21</p> <p>KHA PROJECT NO. 168726001</p> <p>SHEET NUMBER</p>			<p>C4.0</p>

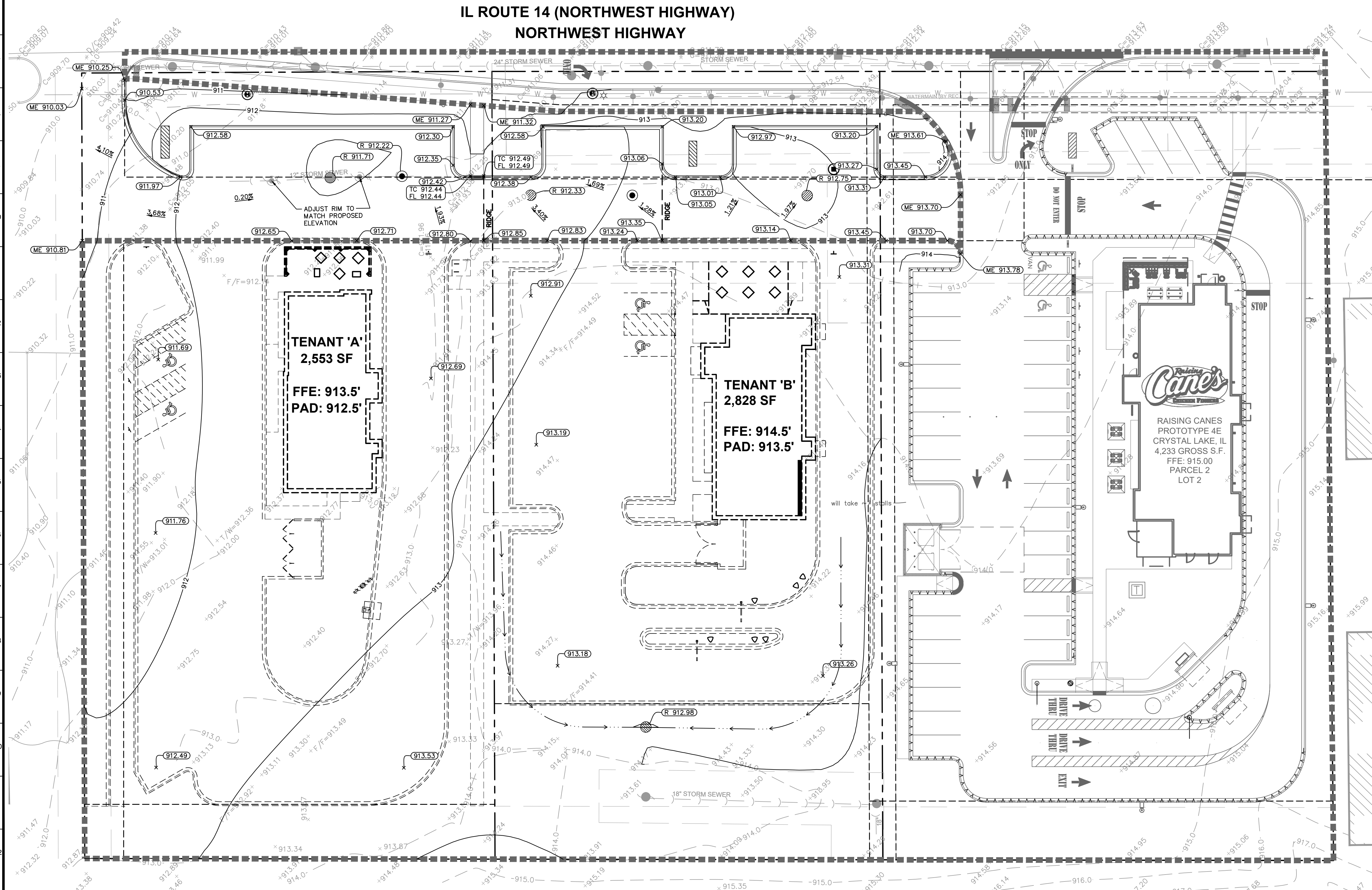
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- ### GRADING NOTES
- CONTRACTOR TO VERIFY ALL EXISTING TOPOGRAPHY AND STRUCTURES ON THE SITE AND IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES PRIOR TO STARTING WORK.
 - ALL PAVEMENT SPOT GRADE ELEVATIONS AND RIM ELEVATIONS WITHIN OR ALONG CURB AND GUTTER REFER TO FLOW LINE ELEVATIONS UNLESS OTHERWISE NOTED.
 - ALL ELEVATIONS SHOWN DEPICT FINISHED GRADE UNLESS OTHERWISE NOTED. GENERAL CONTRACTOR TO COORDINATE WITH EXCAVATION, LANDSCAPE AND PAVING SUBCONTRACTORS REGARDING TOPSOIL THICKNESS FOR LANDSCAPE AREAS AND PAVEMENT SECTION THICKNESS FOR PAVED AREAS TO PROPERLY ENSURE ADEQUATE CUT TO ESTABLISH SUBGRADE ELEVATIONS.
 - NO EARTHEN SLOPE SHALL BE GREATER THAN 3:1, UNLESS OTHERWISE NOTED.
 - MAXIMUM SLOPE IN ACCESSIBLE PARKING SPACES AND LOADING ZONES SHALL NOT EXCEED 2.0% IN ALL DIRECTIONS.
 - MAXIMUM RUNNING SLOPE SHALL NOT EXCEED 5% AND CROSS SLOPE SHALL NOT EXCEED 2% ON ALL SIDEWALKS AND ACCESSIBLE ROUTES.
 - WHEN NATURAL FLOW OF DRAINAGE IS AWAY FROM CURB, CONTRACTOR TO INSTALL REVERSE GUTTER PITCH.
 - MATCH EXISTING ELEVATIONS AT THE PROPERTY LIMITS.

GRADING LEGEND

TP = TOP OF PAVEMENT	EP = EDGE OF PAVEMENT	FL = FLOW LINE	TC = TOP OF CURB	ME = MATCH ELEVATION	TF = TOP OF FOUNDATION	R = RIM ELEVATION	TW = TOP OF WALL	FG = FINISHED GRADE	TS = TOP OF STAIRS	BS = BOTTOM OF STAIRS	
PROPOSED CONTOUR	EXISTING CONTOUR	RIDGE LINE	SLOPE AND FLOW DIRECTION	100-YEAR OVERLAND OVERFLOW ROUTE	DETENTION BASIN 100-YEAR EMERGENCY OVERLAND OVERFLOW ROUTE	PROPOSED SWALE	REVERSED RETAINING WALL	REVERSED PITCH CURB AND GUTTER	ACCESSIBLE ROUTE	RIP RAP (SEE DETAILS)	LIMITS OF SCOPE



<h2 style="margin: 0;">GRADING PLAN</h2>	<p style="font-size: small;"> ORIGINAL ISSUE: X/XX/21 KHA PROJECT NO. 168726001 SHEET NUMBER C5.0 </p>
<p style="font-size: small;"> GW CRYSTAL LAKE 5417 NORTHWEST HWY CRYSTAL LAKE, IL 60014 </p>	<p style="font-size: small;"> REVISIONS NO. DATE BY </p>

Drawing name: K:\GIS_DEVELOPMENT\168726001_GW Properties_Crystal Lake, IL\X Design\CAD\PlanSheets\06.0 UTILITY PLAN.dwg Layout1 Aug 08, 2021 12:13pm by: Eric Treacy
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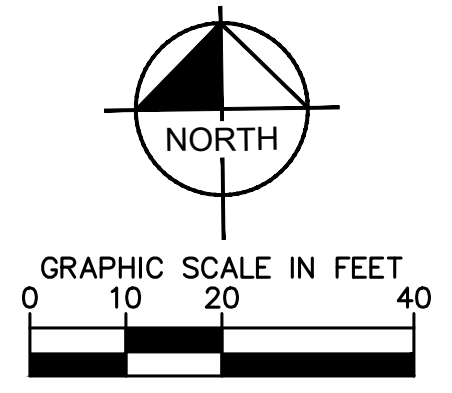
STORM STRUCTURE TABLE	
STRUCTURE NAME:	DETAILS:
D1	5' DIA. STORM MANHOLE CONTRACTOR TO VERIFY LOCATION AND INVERTS OF EXISTING STORM SEWER PRIOR TO ORDERING MATERIAL. RIM: 912.22 INV IN: 889.85 (E, 12") INV OUT: 889.77 (W, 12") INV OUT: 889.85 (S, 12")
D2	4' DIA. STORM MANHOLE RIM: 912.33 INV OUT: 890.09 (W, 12")
D3	4' DIA. STORM MANHOLE RIM: 912.75 INV IN: 890.56 (S, 12") INV OUT: 908.72 (W, 12")
D4	STORM SERVICE STUB. CAP FOR FUTURE CONNECTION. RIM: 908.94 INV IN: 890.14 (N, 12")
D5	STORM SERVICE STUB. CAP FOR FUTURE CONNECTION. RIM: 894.64 INV OUT: 890.79 (N, 12")

STORM STRUCTURE TABLE	
STRUCTURE NAME:	DETAILS:
D6	MH - OPEN LID RIM: 912.98 INV OUT: 910.79 (S, 12")
D7	4' DIA. STORM MANHOLE RIM: 913.08 INV IN: 908.40 (E, 12") INV OUT: 907.49 (N, 12") INV OUT: 907.49 (S, 12")
D8	STORM SERVICE STUB. CAP FOR FUTURE CONNECTION. RIM: 911.57 INV IN: 907.71 (N, 12")
ED1	RAISE EXISTING INLET RIM TO 912.58' RIM: 911.71

SANITARY STRUCTURE TABLE	
STRUCTURE NAME:	DETAILS:
S1	4' DIA. SANITARY MANHOLE RIM: 912.99 INV IN: 907.05 (S, 6") INV OUT: 907.05 (W, 6")

SANITARY STUB TABLE	
STRUCTURE NAME:	DETAILS:
S2	SAN SERVICE STUB. CAP FOR FUTURE CONNECTION. INV OUT: 907.37 (N, 6")
S3	SAN SERVICE STUB. CAP FOR FUTURE CONNECTION. INV OUT: 906.58 (NE, 6")

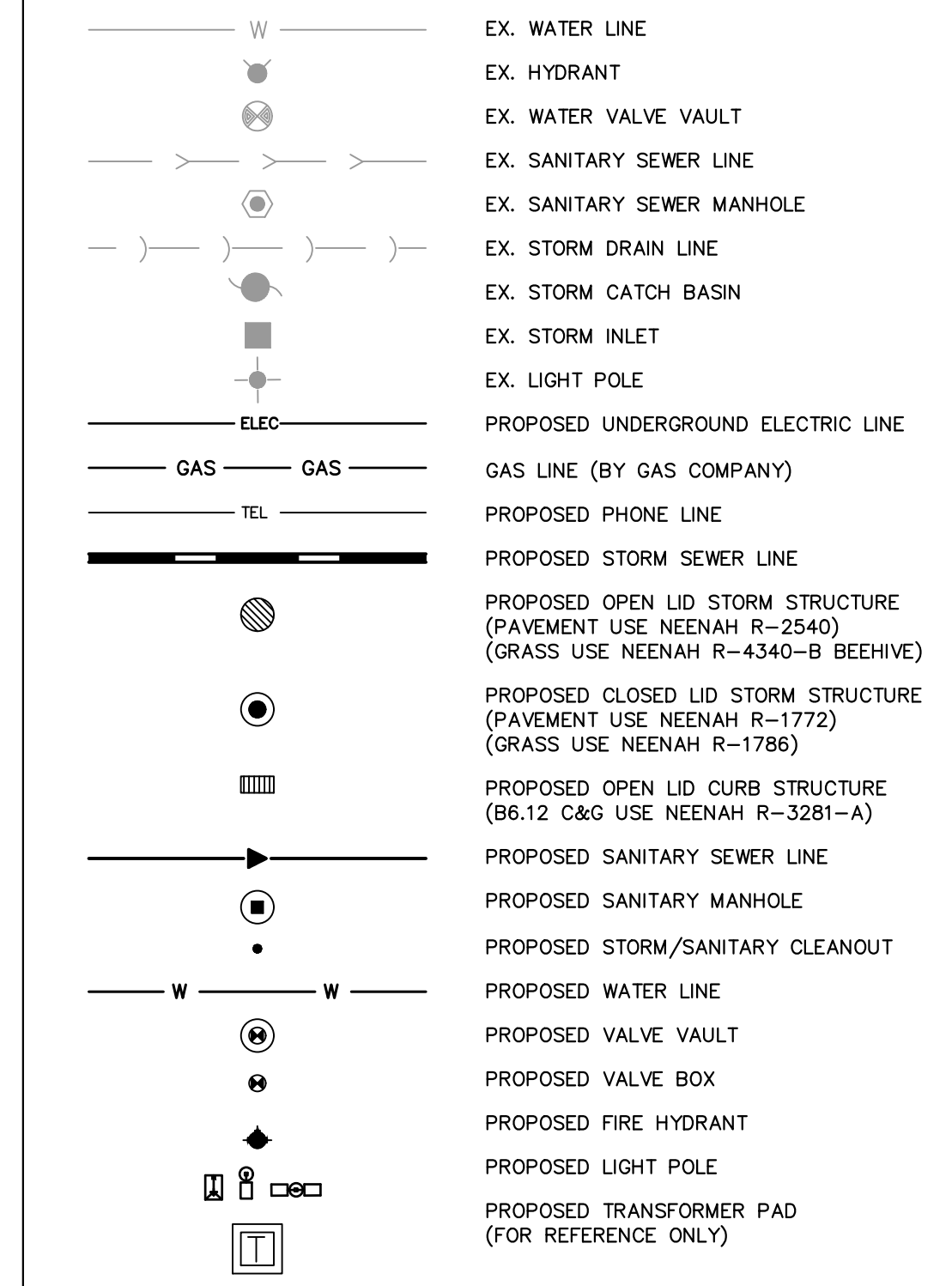
WATER STRUCTURE TABLE	
STRUCTURE NAME:	DETAILS:
W1	PRESSURE CONNECT TO EX. WATERMAIN PER CITY STANDARDS. CONTRACTOR TO FIELD VERIFY LOCATION, DEPTH, AND SIZE OF EX. UTILITIES PRIOR TO ORDERING MATERIALS. FG ELEV: 911.16
W2	STUB FG ELEV: 912.42
W3	PRESSURE CONNECT TO EX. WATERMAIN PER CITY STANDARDS. CONTRACTOR TO FIELD VERIFY LOCATION, DEPTH, AND SIZE OF EX. UTILITIES PRIOR TO ORDERING MATERIALS. FG ELEV: 912.07
W4	STUB FG ELEV: 913.04



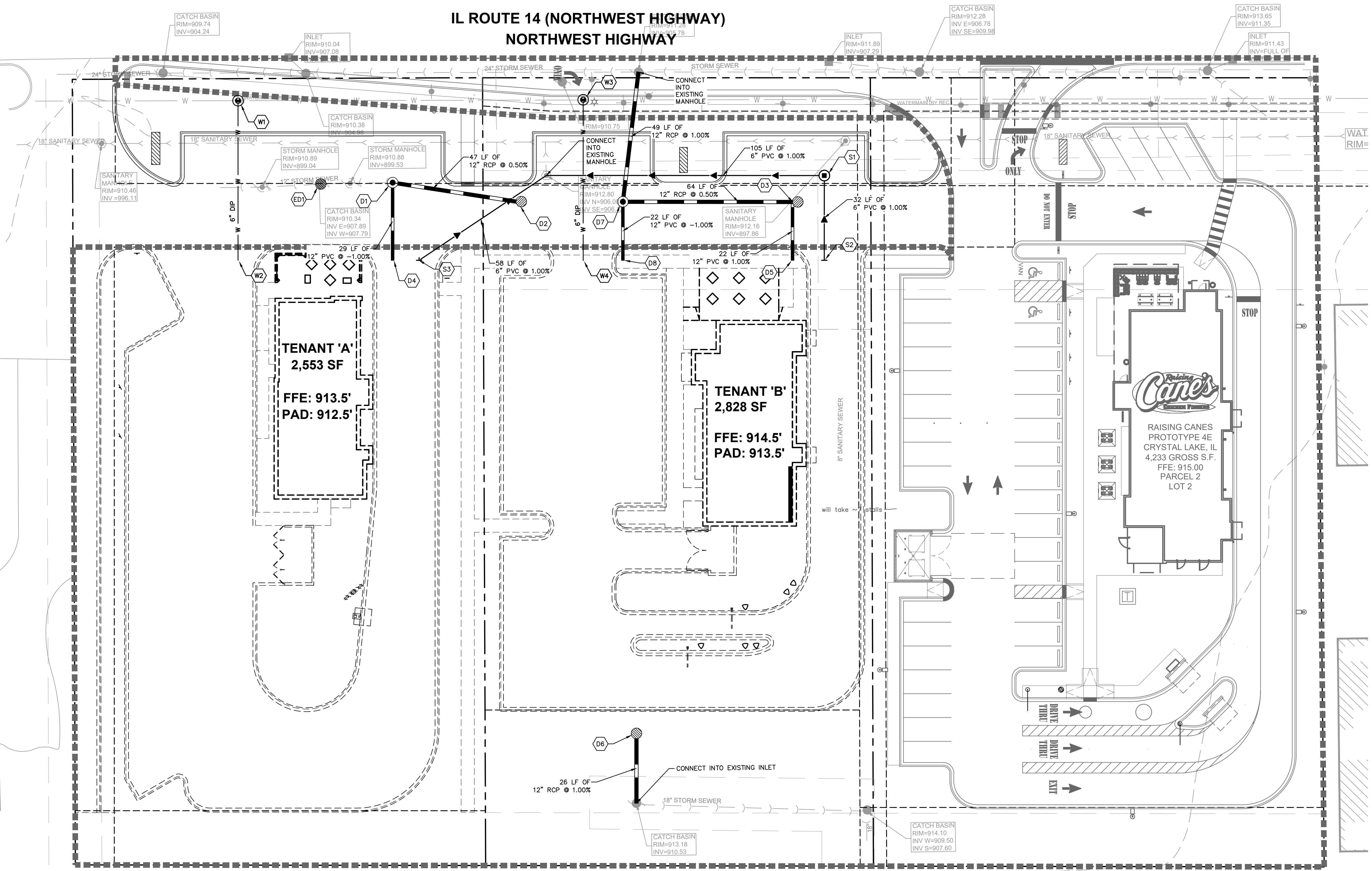
UTILITY NOTES

- ALL WATER LINES \geq 3" SHALL BE DUCTILE IRON PIPE, CLASS 52.
- ALL SANITARY SEWER LINES SHALL BE PVC MEETING, ASTM D-3034 SDR 26 EXCEPT FOR SANITARY SEWER THAT CROSSES ABOVE WATER MAIN, THIS PIPE SHALL BE AWWA C900 (UNLESS WATER MAIN CASING IS UTILIZED). PROVIDE 42" MINIMUM COVER.
- CONTRACTOR SHALL COORDINATE ANY DISRUPTIONS TO EXISTING UTILITY SERVICES WITH ADJACENT PROPERTY OWNERS.
- ALL ELECTRIC AND TELEPHONE EXTENSIONS INCLUDING SERVICE LINES SHALL BE CONSTRUCTED TO THE APPROPRIATE UTILITY COMPANY SPECIFICATIONS. ALL UTILITY DISCONNECTIONS SHALL BE COORDINATED WITH THE DESIGNATED UTILITY COMPANIES.
- CONSTRUCTION SHALL NOT START ON ANY PUBLIC UTILITY SYSTEM UNTIL WRITTEN APPROVAL HAS BEEN RECEIVED BY THE ENGINEER FROM THE APPROPRIATE GOVERNING AUTHORITY AND CONTRACTOR HAS BEEN NOTIFIED BY THE ENGINEER.
- CONTRACTOR TO CALL "JULIE" (1-800-892-0123) TO COORDINATE FIELD LOCATIONS OF EXISTING UNDERGROUND UTILITIES BEFORE ORDERING MATERIALS OR COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES IMMEDIATELY.
- PRIOR TO THE CONSTRUCTION OF OR CONNECTION TO ANY STORM DRAIN, SANITARY SEWER, WATER MAIN OR ANY OTHER UTILITIES, THE CONTRACTOR SHALL EXCAVATE, VERIFY AND CALCULATE ALL POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE ENGINEER AND THE OWNER/DEVELOPER OF ANY CONFLICT OR REQUIRED DEVIATIONS FROM THE PLAN. NOTIFICATION SHALL BE MADE A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER AND ITS CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THAT THE CONTRACTOR FAILS TO MAKE SUCH NOTIFICATION. THE MUNICIPALITY SHALL BE NOTIFIED OF ANY AND ALL CHANGES TO THE DESIGN PLANS.
- CONTRACTOR SHALL COMPLY COMPLETELY WITH THE LATEST STANDARDS OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL USE SUPPORT SYSTEMS, SLOPING, BENCHING AND OTHER MEANS OF PROTECTION. THIS IS TO INCLUDE, BUT NOT LIMITED TO ACCESS AND EGRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH PERFORMANCE CRITERIA AS REQUIRED BY OSHA.
- CONTRACTOR TO AVOID DISRUPTION OF ANY ADJACENT TENANT'S TRAFFIC OPERATIONS DURING INSTALLATION OF UTILITIES.
- ALL DIMENSIONS ARE TO CENTERLINE OF PIPE OR CENTER OF MANHOLE UNLESS NOTED OTHERWISE.
- SEE ARCHITECTURAL AND MEP PLANS FOR EXACT UTILITY CONNECTION LOCATIONS AT BUILDING.
- LIGHT POLES SHOWN FOR COORDINATION PURPOSES ONLY AND DO NOT REPRESENT ACTUAL SIZE. SEE SITE LIGHTING PLANS BY OTHERS FOR MORE INFORMATION.
- SEE DETAILS FOR LOCATING STORM STRUCTURES WITHIN THE CURB LINE.
- STORMWATER FACILITIES MUST BE FUNCTIONAL BEFORE BUILDING CONSTRUCTION BEGINS.

UTILITY LEGEND



IL ROUTE 14 (NORTHWEST HIGHWAY) NORTHWEST HIGHWAY



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KHA PROJECT NO. 168726001			
SHEET NUMBER			
C6.0			

Traffic Impact Study

5417-5501 Northwest Hwy Mixed-Use Development Crystal Lake, Illinois

June 11, 2019
Updated July 13, 2020
Updated July 30, 2021

Prepared for:



Prepared by:

Dan Brinkman, P.E., PTOE
Assistant Director of Transportation Services

GHA GEWALT HAMILTON
ASSOCIATES, INC.

Executive Summary

The following study represents the third update to a Traffic Impact Study prepared by Gewalt Hamilton Associates, Inc. (GHA) for the proposed development to be located at 5417-5507 Northwest Highway. Our Initial study in June 2019 included a variety of commercial and retail uses in addition to a single drive-through restaurant. The subsequent July 2020 update considered a two-phased development comprised of the Raising Canes drive through restaurant and a to-be-determined sit down style family restaurant. The latest iteration that follows considers the Raising Canes Restaurant currently under construction (although the site plan provided includes a larger building that previously considered) and two additional drive-through restaurant uses.

Some key considerations and findings of the updated study include:

- Traffic has been projected out to 2027 (in lieu of 2026) to reflect a 5-year buildout beyond development of the additional drive-through restaurants.
- Trip generations have been updated for both Phases of the development and include the larger Raising Canes building shown on the site plan provided.
- Traffic analyses have been updated to reflect the more intense nature of the development than previously considered. However, given the nature of the restaurants proposed and their limited breakfast offerings, it is very likely that the traffic volumes are overstated for the Morning Peak Hour.
- Recommendations to accommodate traffic impacts remain generally the same as previous studies with some additional emphasis to widen the shared access on the Cassidy Tire site to a true 3-lane (35-ft wide) access capable of providing two outbound lanes. Furthermore, given the anticipated queueing at this location some supplemental signage and pavement markings should be considered to keep the intersection clear and mitigate potential of backing up traffic onto US Route 14.
- Overall, the impacts of the latest development iteration can be accommodated by the area roadways, with the understanding that unsignalized left turns in and out of the development will have longer than desirable wait times and associated queues. These delays and queues however are typical and to be expected for side streets and minor access points located along major arterials such as US Route 14.
- Stacking requirements for the two new proposed drive-through restaurants meet the City's requirements. There may still be times when queues exceed the limits shown on the site plans, but we anticipate that after the initial "newness" wears off, all queues and stacking can be contained on site and out of the public right of way.

This update has been prepared by



Daniel P Brinkman, PE, PTOE
Assistant Director of Transportation Services

Part I. Introduction and Project Context

Gewalt Hamilton Associates, Inc. (GHA) has conducted a Traffic Impact Study for the proposed restaurant development to be located at 5417-5507 Northwest Highway (US Route 14) in Crystal Lake, Illinois. The approximately 3.20-acre site formerly contained Pauly Toyota. Access to the site was provided via three full access driveways on US Route 14, as well as a fourth shared access with the adjacent Cassidy Tire use, aligned opposite Federal Drive.

An approximately 3,700 square-foot, Raising Cane's fast food restaurant with drive-through window is currently under construction on the eastern ± 1.2 -acres and is referred to as Phase I for the balance of this report. The Raising Canes site is to be accessed via a proposed three-quarter (left-in/right-in/right-out) driveway on US Route 14, with future cross-access provided to the adjacent parcel to the west and the Cassidy Tire site.

Per the July 28, 2021 Overall Site Plan prepared by Design Studio 24, LLC the development of the western ± 2.0 -acres will be developed with two additional drive-through restaurants (a *Wendy's* and a *Gomez Y Guzman [GYG]*) of approximately 2,550 and 2,850 square feet, respectively. These additional restaurants are referred to as Phase II. Upon full build-out, access to the site will be provided via a cross-connection to the three-quarter access, as well as the existing shared access with Cassidy Tire; the remaining access driveways along US Rte 14 serving the site will be closed. This is an effective access management strategy to improve safety and progression along corridors.

The following summarizes our findings and provides various recommendations for your consideration. *Appendices* referenced are in the Technical Addendum at the end of this document.

Part II. Background Information





Site Location Map and Roadway Inventory

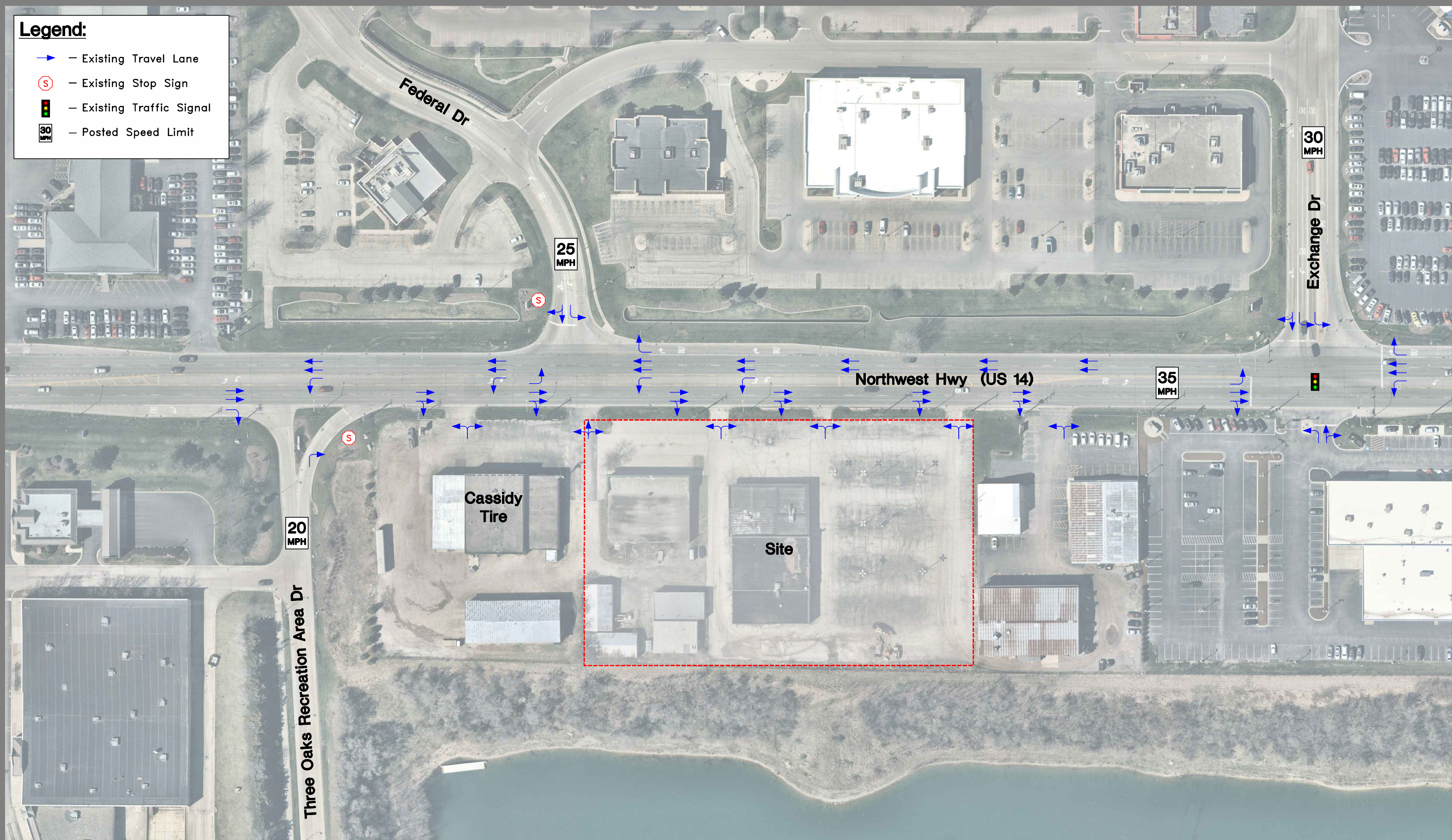
Exhibit 1 provides a site location map. The existing traffic operations in the site area are illustrated on *Exhibit 2*. *Appendix A* provides a photo inventory of the site vicinity prior to the commencement of construction of Raising Canes. Pertinent comments to the adjacent roadways include:

Northwest Highway (US Route 14)

- US Route 14 is an east-west, Principal Arterial under the jurisdiction of the Illinois Department of Transportation (IDOT).
- US Route 14 provides a five-lane cross section (two through lanes in each direction, with a center lane for left-turns), with a posted speed limit of 35 miles per hour (mph) in the vicinity of the site.
- A dedicated right-turn lane is provided in the eastbound direction at its unsignalized intersection with Three Oaks Recreation Area Drive and in the westbound direction at its unsignalized intersection with Federal Drive, as well as at its signalized intersection with Exchange Drive.
- US Route 14 is designated as a Strategic Regional Arterial (SRA) route. SRA routes are designed to carry higher traffic volumes at higher travel speeds through access control and traffic signal installation / spacing. It is also designated as a Class II Truck route.
- The Annual Average Daily Traffic (AADT), year 2019, on US Route 14 is 27,800 vehicles with 1,675 trucks (6.0%).

Legend:

-  - Existing Travel Lane
-  - Existing Stop Sign
-  - Existing Traffic Signal
-  - Posted Speed Limit



Part III. Traffic Evaluation

Future Site Characteristics

Proposed Development Plan

The Overall Site Plan prepared by Design Studio 24, LLC is provided in **Appendix E**. As shown, the development will consist of an approximately 3,700 square-foot, Raising Cane's fast food restaurant with drive-through window (Phase I), accessed via a proposed three-quarter (left-in/right-in/right-out) driveway on US Route 14, with future cross-access provided to the adjacent parcel to the west.

Per the July 28, 2021 Overall Site Plan the development of the western ±2.0-acres will be developed with two additional drive-through restaurants (a *Wendy's* and a *Gomez Y Guzman*) of approximately 2,550 and 2,850 square feet, respectively. These additional restaurants are referred to as Phase II. Upon full build-out, access to the site will be provided via a cross-connection to the three-quarter access, as well as the existing shared access with Cassidy Tire; the remaining access driveways along US Rte 14 serving the site will be closed. This is an effective access management strategy to improve safety and progression along corridors.

Trip Generation

Table 2 summarizes the traffic generation calculations for the proposed development. Trip generation rates published by the Institute of Transportation Engineers (ITE) in the 10th Edition of the Manual *Trip Generation* were used to determine the anticipated traffic generated by the proposed mix of uses. See **Appendix F** for excerpts from the ITE manual.

Not all vehicle trips expected to be generated by the proposed project represent new trips on the study area roadway system. Studies have shown that for restaurant developments, a substantial portion of the site-generated vehicle trips are already present in the adjacent passing stream of traffic or are diverted from another route to the proposed site. Based on data presented in the ITE *Trip Generation Handbook, 3rd Edition*, the average pass-by trip percentage for the proposed uses is between 43 and 50 percent. However, to provide a conservative analysis, a 20 percent reduction for pass-by traffic was applied to the development site-generated trips, consistent with IDOT guidelines for the preparation of traffic impact studies on SRA routes. It should be noted that the volume of pass-by traffic does not reduce the total trips generated and those trip volumes will still be realized as turning movements at the site driveways.

In addition, for multi-use developments, or developments with cross-access and shared access driveways, internal trips between the land uses within the development are expected to occur. These are multi-purpose trips from one land use within the development to another land use within the same and/or adjacent development (e.g. a customer purchases something from the adjacent automotive store, Cassidy Tire, and eats at one of the restaurants). However, to provide a conservative analysis scenario, no reduction for internal capture was applied.

Table 2: Trip Generation Calculations

Land Use	Size	ITE Code	Weekday Peak Hours						Saturday Midday			Daily			
			Morning			Evening			In	Out	Sum	In	Out	Sum	
			In	Out	Sum	In	Out	Sum							
Phase I - Raising Cane's															
Fast Food Restaurant w/Drive-Thru	3.66 KSF	#934	--	--	--	62	58	120	103	98	201	862	862	1,724	
<i>less pass-by trips (20%)</i>			--	--	--	-12	-12	-24	-20	-20	-40	-172	-172	-344	
<i>New Trips</i>			--	--	--	50	46	96	83	78	161	690	690	1,380	
Phase II - Guzman Y Gomez Mexican Kitchen (GYG) + Wendy's															
Fast Food Restaurant w/Drive-Thru (GYG)	2.83 KSF	#934	58	56	114	48	44	92	79	76	155	666	666	1,332	
<i>less pass-by trips (20%)</i>			-11	-11	-22	-9	-9	-18	-16	-16	-32	-133	-133	-266	
<i>New Trips</i>			47	45	92	39	35	74	63	60	123	533	533	1,066	
Fast Food Restaurant w/Drive-Thru (Wendy's)	2.55 KSF	#934	53	50	103	43	40	83	71	69	140	601	601	1,202	
<i>less pass-by trips (20%)</i>			-10	-10	-20	-8	-8	-16	-14	-14	-28	-120	-120	-240	
<i>New Trips</i>			43	40	83	35	32	67	57	55	112	481	481	962	
Total Development (Phases I & II)															
<i>New Trips</i>			90	85	175	124	113	237	203	193	396	1,704	1,704	3,408	
<i>Pass-By Trips</i>			-21	-21	-42	-29	-29	-58	-50	-50	-100	-425	-425	-850	

Sources: Institute of Transportation Engineers (ITE) Trip Generation Manual; 10th Edition, 2017.

ITE Trip Generation Handbook; 3rd Edition, 2017.

Notes: Raising Cane's Restaurant is not open during the morning.

Trip Distribution

Table 3 provides the anticipated distribution of *new* site traffic. This was based on existing site travel patterns, proposed access and the operational characteristics of the adjacent street system.

Table 3: Trip Distribution

Route & Direction	Percent Route To/From Site
US Route 14	
West of Three Oaks Recreation Area Dr	45%
East of Exchange Dr	40%
Three Oaks Recreation Area Dr	
South of US Route 14	--
Federal Dr	
North of US Route 14	5%
Exchange Dr	
North of US Route 14	10%
Totals =	100%

-- Less than 5%

Site and Total Traffic Assignments

Exhibits 5a and 5b illustrate the site Phase I (Raising Cane's) traffic assignment for the development's *new* and *pass-by* trips, respectively, which is based on the traffic characteristics summarized in *Tables 2 and 3* (traffic generation and trip distribution) and assigned to the area roadways. Similarly, *Exhibits 6a and 6b* illustrate the combined Phase I and Phase II (Raising Cane's and additional drive-through restaurants) *new* and *pass-by* trips, respectively.

The Phase I site traffic (*Exhibits 5a and 5b*) and 2027 No-Build traffic (*Exhibit 4*) were combined to produce the 2027 Total Phase I traffic, which is illustrated on *Exhibit 7*. Similarly, the combined Phase I and Phase II site traffic (*Exhibits 6a and 6b*) and 2027 No-Build traffic (*Exhibit 4*) were combined to produce the 2027 Total Phases I and II traffic, which is illustrated on *Exhibit 8*.

Drive-Through Queueing (Stacking) Analyses

Drive-through queueing analyses were conducted to determine whether the proposed stacking length is adequate to accommodate the drive-through vehicles for the proposed restaurants. Per City of Crystal Lake Ordinance 5 stacking spaces are required prior to the first point of service (order box or service window) and 4 spaces between the order box and the pickup window. For dual drive-through configurations, 5 spaces should be provided prior to each order board. **Table 6** summarizes the requirements and stacking provided for each of the proposed restaurants:

Table 6 Drive-Through Stacking Requirements

Use	City Requirements	Provided Prior to Order	Provided Order to Pick-up	Total Provided
Guzman Y Gomez	4+5*2 =14	9	6	15
Wendy's	4+5 =9	8	4	12

Based on the Design Studio 24, LLC Site Plan (see **Appendix E**), both *Wendy's* and *GYG* provide the required stacking.

Based upon recent surveys conducted by Kimley Horn for at similar Raising Canes fast-food restaurants with drive-through facilities (see **Appendix H**), the maximum stacking did not exceed 15 vehicles, with typical peak hour queues ranging between 8 to 9 vehicles. The sites surveyed were generally located on principal arterial roads with similar, and/or higher, Annual Average Daily Traffic (AADT) volumes as US Route 14.

In general, we believe that for all of the proposed uses that the provided stacking and queueing should be sufficient for the demand once the initial "newness" of the restaurants wears off.

Bicycle and Pedestrian Accommodations

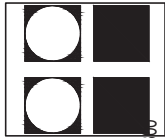
The proximity of the proposed development to the Three Oaks Recreation Area and the associated proposed boardwalk along the south end of the site require that additional attention in site planning and layout be given to pedestrian and bicycle accommodations. For traditionally auto-centric land uses, the proposed plan generally does a good job of providing accommodations for pedestrians and bicycles by providing connections and marked crossings to the south end of the lots as well as outdoor patio areas.

Some additional considerations could include:

- Enhanced warning signage for the pedestrian connections to the proposed sidewalk along US Route 14 (e.g., fluorescent yellow-green signs and markings).
- Provide Stop Signs and Stop Bars along exit drives in advance of the pedestrian crossings that parallel US Route 14.
- Provisions for bicycle parking / bike racks should be included on the site plans or in a common area adjacent to the future boardwalk easement.

- Consider supplemental signage instructing riders to dismount and walk bikes upon entering the various restaurant sites to help minimize conflicts with motorists.
- Ensure plantings and landscaping is kept to ground level in the vicinity of crossing locations to preserve sight lines for pedestrians.

Exhibit 10 identifies some of our recommendations on the current proposed site plan.

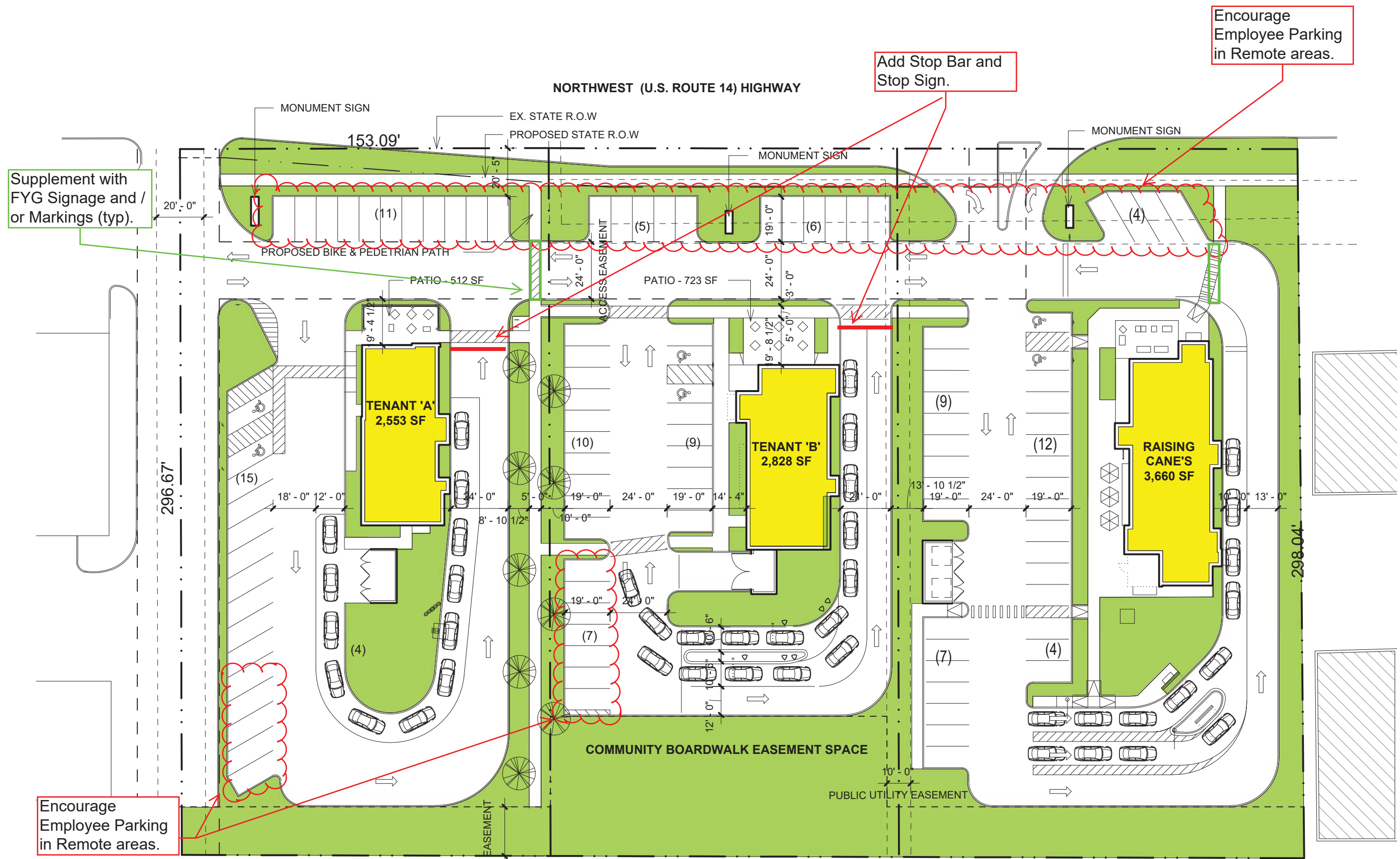


DESIGN STUDIO 24, LLC.
 ARCHITECTS - DESIGNERS - PLANNERS
 2211 N. Elston Ave. Suite 304, Chicago, Illinois 60614 Fax: 847.886.7751 Tel.: 847.886.8969

GW PROPERTIES
 PROPOSED SITE STUDY
 5417-5501 NORTHWEST HWY
 CRYSTAL LAKE, ILLINOIS

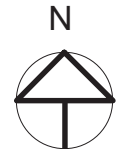
DATE: 07.28.2021
 MARK T. DIGANCI
 REG. ARCHITECT

ST.1



1 SITE PLAN - OVERALL
 1" = 40'-0"

PARKING SPACE:
 WENDY'S - 26 SPACES (2 ADA STALLS)
 GYG - 41 SPACES (2 ADA STALLS)
 RAISING CANES - 36 SPACES (2 ADA STALLS)



**Exhibit 10-
 Pedestrian Considerations**

Part IV. Recommendations and Conclusions

Analyses have been conducted under existing and future conditions to determine the impact from the proposed multi-restaurant development on the study area intersections. In addition to the pedestrian scale recommendations previously discussed, the capacity analysis results indicate that the increase in project site-generated traffic has little effect upon the operations of the area roadway network with the recommendations contained herein:

Under Construction – Part of Raising Canes approval

- A dedicated right-turn lane would be installed on US Route 14 at the proposed three-quarter (left-in/right-in/right-out) site access driveway.
- The painted median along the eastern site frontage on US Route 14 would be restriped as a two-way left-turn lane.

Future improvements with approval of additional Drive Throughs

- A Stop sign would be installed on the shared site access driveway, opposite Federal Drive. This site access should be widened slightly to the east to provide one inbound lane and two outbound lanes. Exiting lanes should be marked as a separate left-turn lane and a shared through/right-turn lane. The proposed three-quarter site access driveway on US Route 14 should provide one inbound lane and one outbound (right-turn) lane, operating under Stop sign control.
- The development should provide for a future connection to the parcel to the east, if/when it is redeveloped with a compatible use.
- To the extent feasible, deliveries should occur during off-peak hours.
- The entrance to the Wendy's site should provide a raised "truck apron" to give the appearance of a narrower entrance drive to help limit speed of inbound vehicles while still accommodating the larger delivery vehicles needed.
- Consideration should be given to signing and marking the intersection of the shared driveway with Cassidy Tire and the east west circulation roadway "DO NOT BLOCK INTERSECTION" to help maintain traffic flow given the potential for delays exiting at that location.
- Employees should be encouraged to park in remote spaces (furthest from the site entrance or along US Route 14) to provide convenient parking for customers.
- Appropriate wayfinding signing should be provided on site to direct traffic to the various drive-throughs.

Part V. Technical Addendum

The following Appendices were previously referenced. They provide technical support for our observations, findings and recommendations discussed in the text.

Appendices

- A. Photo Inventory
- B. Traffic Count Summaries
- C. Crash Summary Map
- D. CMAP Traffic Projections
- E. July 28, 2021 Site Plan (Phases I & II)
- F. ITE Trip Generation Excerpts
- G. Capacity Analysis Worksheets
- H. Traffic Characteristics Similar Facility



GW Crystal Lake

Stormwater Management Report

5417-5501 Northwest Highway
(Lot 1 of the Pauly Subdivision Doc. #2008R0052561)
Crystal Lake, IL

Prepared by:
Kimley-Horn and Associates, Inc.
4201 Winfield Road, Suite 600
Warrenville, IL 60555
Contact: Eric Tracey

Prepared on: 8/5/2021



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4. RUNOFF VOLUME REDUCTION..... 3

EXHIBITS

- Exhibit 1 – National Wetlands Inventory Map*
- Exhibit 2 – FEMA Firm Map*
- Exhibit 3 – NRCS Soils Map*
- Exhibit 4 – USGS Map*
- Exhibit 5 – Existing Drainage Plan*
- Exhibit 6 – Proposed Drainage Area Map and Design Calculations*

1. PROJECT NARRATIVE

Kimley-Horn and Associates, Inc., serves as the engineering consultant for GW Properties. They are proposing to construct two new building pads, Tenant 'A' building area is 2,553 SF and Tenant 'B' is 2,828 at 5417-5501 Northwest Highway in Crystal Lake, IL. The site is currently an abandoned used car dealership with an existing building and surface parking lot. The existing building and onsite features are to be demolished. The sitework for the building pads includes site access modifications, mass grading, building and pavement construction, and utility construction for new storm, sanitary and water services. The site is located in McHenry County and will be subject to the City of Crystal Lake and McHenry County Stormwater Management Ordinance.

1.1. Pre-Development Conditions

The proposed site for the new building pads is the western side (Lot 1) of the existing car dealership development. Lot 1 is approximately 2.06-acres in size and is a paved parking lot with 2 buildings in the existing condition. The existing site runoff mostly drains in three directions: north, east and southwest via sheet flow to the IL 14 State R.O.W., adjacent lot and to the lake as shown on the **Existing Drainage Plan** included in this report as **Exhibit 5**.

In the existing condition, the area of proposed improvements contains 1.85 acres of impervious surface and 0.21 acres of pervious surface.

Per the U.S. Fish and Wildlife Service National Wetlands Inventory, there are no wetlands or riparian environments located within the area of development or within 100 feet of the proposed development. The **National Wetlands Inventory map** is included in this report as **Exhibit 1**.

Per the FEMA Flood Insurance Rate Map, Map Number 17111C0335J, the site is located in Zone X which is defined by FEMA as an "Area determined to be outside the 0.2% annual chance floodplain". The **FEMA Flood Insurance Rate Map** is included in this report as **Exhibit 2**.

1.2. Post-Development Conditions

The proposed development will include two building pads, a drive aisle and parking island and utility services and eventually two drive through restaurants. Generally, the proposed drainage patterns match the existing drainage patterns with the flow being routed via sheet flow to proposed storm sewer inlets as well as sheet flowing to the State right-of-way, adjacent lot and lake. The portions existing storm sewer system will be removed and replaced with new storm sewer, additionally new storm sewers will tie into existing storm structures. Refer to the **Proposed Drainage Area Map** included in this report as **Exhibit 6** for additional details.

The area of proposed improvements will contain 1.43 acres of impervious surface and 0.63 acres of pervious surface. This is a decrease of impervious area by 0.42 acres from the existing condition. Refer to Section 2. Site Runoff and Section 3. Site Runoff Storage in this report for additional stormwater information

2. SITE RUNOFF

The proposed development will utilize new storm sewer pipes and inlets to route both minor and major storm events to the Route 14 right-of-way, adjacent lot and lake.

The Pre vs. Post runoff has been analyzed using HydroCAD Software Solutions LLC, which utilizes the SCS TR-20 method. 10 year and 100 year -24 hour storm Rainfall Data was obtained from the Illinois State Water Survey's Bulletin 75. A CN of 98 was assumed for impervious areas and 80 was assumed for pervious areas. A time of concentration of 10 minutes was assumed for the runoff calculations for all drainage areas. Refer to **Exhibit 6** for the **Proposed Drainage Area Map and Design Calculations**.

Drainage Area	Pre-Development 100-YR,24-HR (cfs)	Post-Development 100-YR,24-HR (cfs)
Outfall 1	0.91	0.90
Outfall 2	0.66	0.65
Outfall 3	0.37	0.35

3. SITE RUNOFF STORAGE

As noted above, the proposed building pads and future restaurants decrease the existing impervious area, so detention for the proposed improvements will not be required.

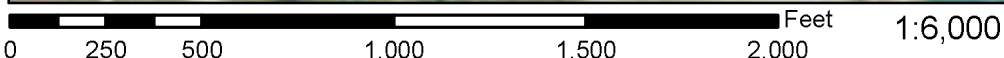
4. RUNOFF VOLUME REDUCTION

To achieve the runoff volume reduction requirement of the local stormwater ordinances, the proposed site plan minimizes impervious surfaces. The reduction in impervious surfaces compared to the existing conditions is previously discussed in this report. The proposed site plan maximizes green space around the building and patio areas as well as around the perimeter of the site.

National Flood Hazard Layer FIRMMette



88°18'32"W 42°13'46"N



88°17'55"W 42°13'19"N

Basemap: USGS National Map: Orthoimagery: Data refreshed October, 2020

Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) <i>Zone A, V, A99</i>
		With BFE or Depth <i>Zone AE, AO, AH, VE, AR</i>
		Regulatory Floodway
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile <i>Zone X</i>
		Future Conditions 1% Annual Chance Flood Hazard <i>Zone X</i>
		Area with Reduced Flood Risk due to Levee. See Notes. <i>Zone X</i>
		Area with Flood Risk due to Levee <i>Zone D</i>
OTHER AREAS		NO SCREEN Area of Minimal Flood Hazard <i>Zone X</i>
		Effective LOMRs
		Area of Undetermined Flood Hazard <i>Zone D</i>
GENERAL STRUCTURES		Channel, Culvert, or Storm Sewer
		Levee, Dike, or Floodwall
OTHER FEATURES		Cross Sections with 1% Annual Chance Water Surface Elevation
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary
		Coastal Transect Baseline
MAP PANELS		Digital Data Available
		No Digital Data Available
		Unmapped
		The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

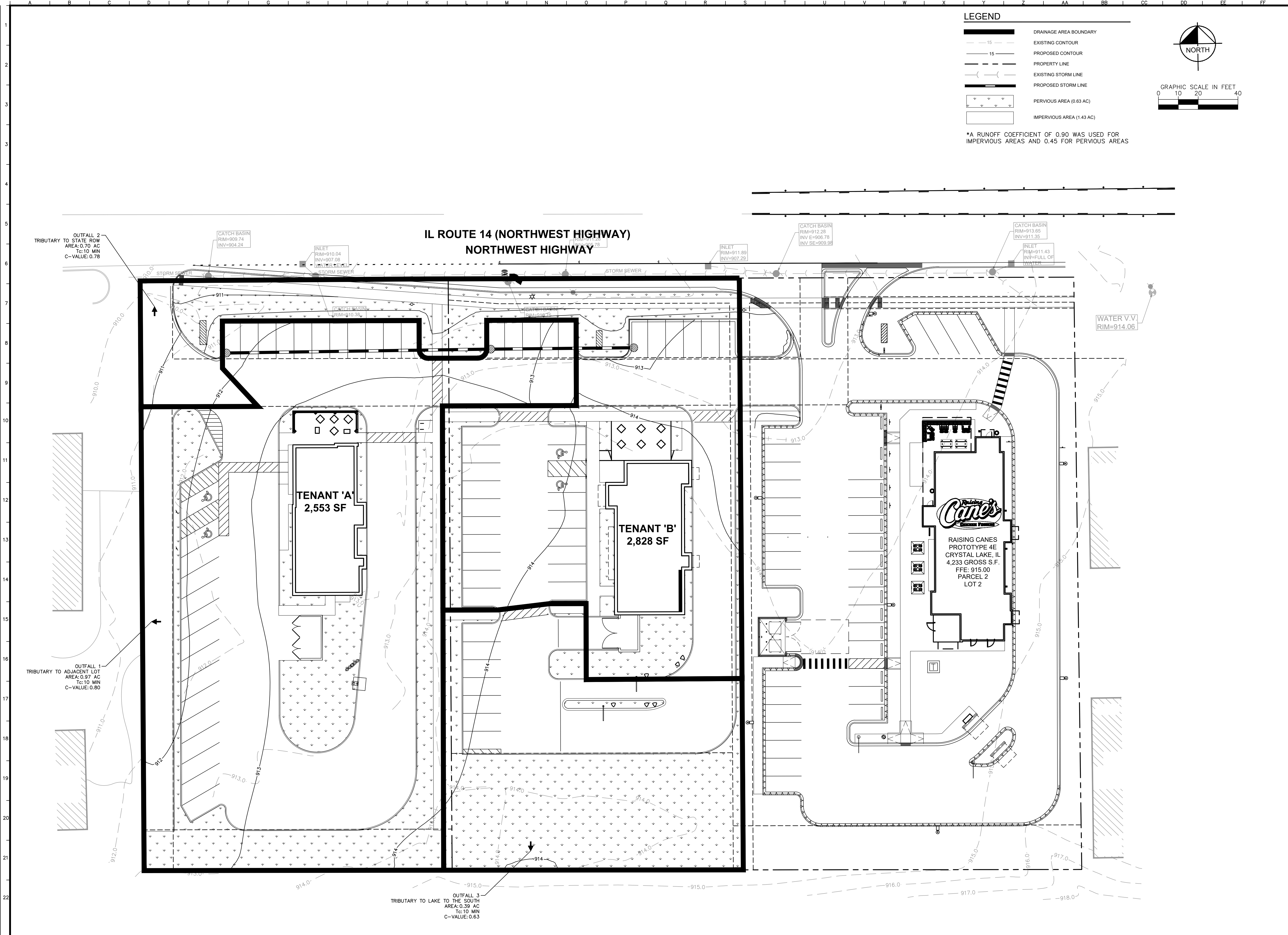


This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on **7/22/2021 at 1:47 PM** and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

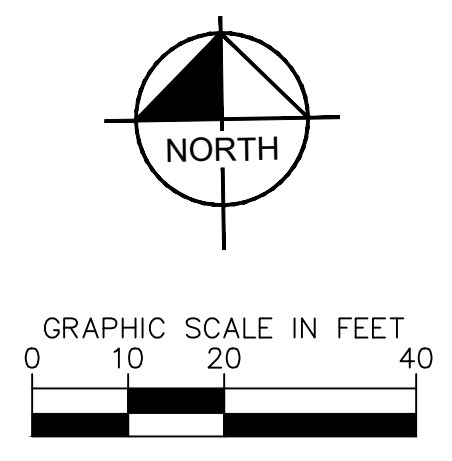
Drawing name: K:\GIS_DEV\168726001_GW Properties-Crystal Lake, IL\X Design\CAD\Exhibits\Proposed Drainage Area Map.dwg Layout: Jul 21, 2021 5:37pm By: Juliette.Strosser
 This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



LEGEND

- DRAINAGE AREA BOUNDARY
- EXISTING CONTOUR
- PROPOSED CONTOUR
- PROPERTY LINE
- EXISTING STORM LINE
- PROPOSED STORM LINE
- PERVIOUS AREA (0.63 AC)
- IMPERVIOUS AREA (1.43 AC)

*A RUNOFF COEFFICIENT OF 0.90 WAS USED FOR IMPERVIOUS AREAS AND 0.45 FOR PERVIOUS AREAS



**IL ROUTE 14 (NORTHWEST HIGHWAY)
NORTHWEST HIGHWAY**

TENANT 'A'
2,553 SF

TENANT 'B'
2,828 SF



RAISING CANES
PROTOTYPE 4E
CRYSTAL LAKE, IL
4,233 GROSS S.F.
FFE: 915.00
PARCEL 2
LOT 2

Kimley»Horn	© 2021 KIMLEY-HORN AND ASSOCIATES, INC. 4201 WINFIELD ROAD, SUITE 600 WARRENVILLE, IL 60555 WWW.KIMLEY-HORN.COM							
SCALE:	AS NOTED	DESIGNED BY: RBG	DRAWN BY: RDB	CHECKED BY: EJT	NO.			DATE
PROPOSED DRAINAGE AREA MAP				GW CRYSTAL LAKE				
				5417 NORTHWEST HWY CRYSTAL LAKE, IL 60014				
				ORIGINAL ISSUE: 7/21/21				
				KHA PROJECT NO. 168726001				
				SHEET NUMBER				
				EXH.				